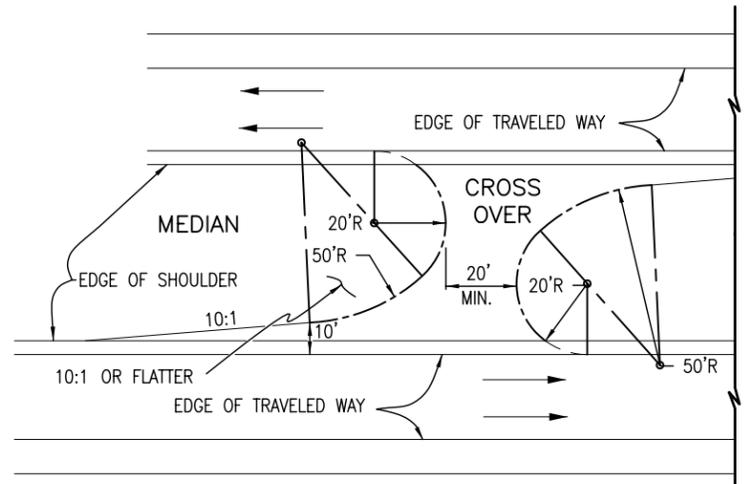


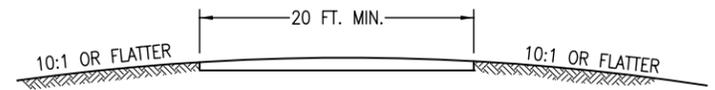
MEDIAN WIDTH LESS THAN 50 FT.



MEDIAN WIDTH GREATER THAN 50 FT.

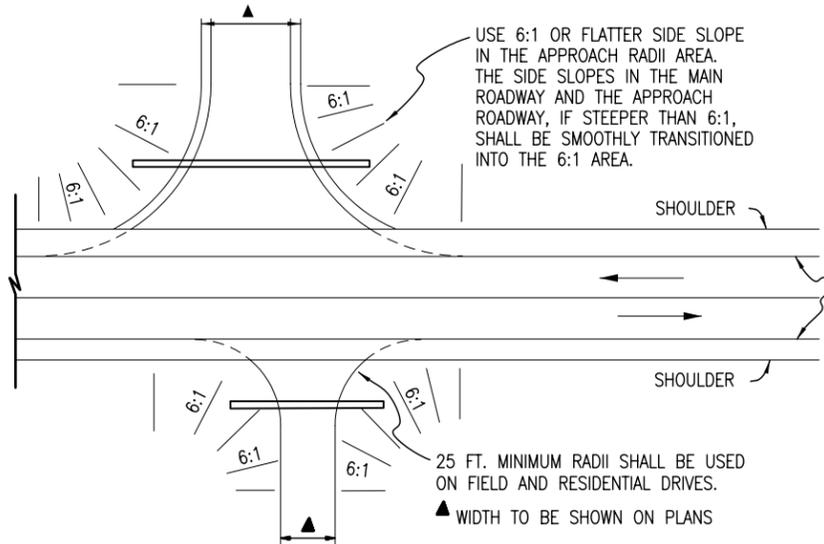
TYPICAL PLANS FOR EMERGENCY MEDIAN CROSS OVER

LOCATION OF RADIUS POINTS MAY BE ADJUSTED FOR BEST FIT



TYPICAL SECTION FOR MEDIAN CROSS OVER

ANY REQUIRED PIPE OR INLET FOR MEDIAN DRAINAGE SHALL HAVE A TRAVERSABLE DESIGN AS SPECIFIED ON THE PLANS

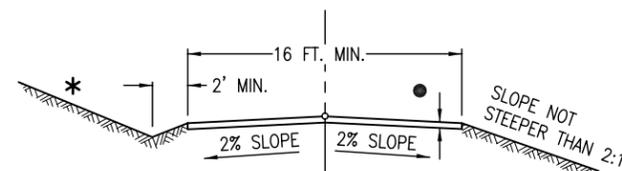


USE 6:1 OR FLATTER SIDE SLOPE IN THE APPROACH RADII AREA. THE SIDE SLOPES IN THE MAIN ROADWAY AND THE APPROACH ROADWAY, IF STEEPER THAN 6:1, SHALL BE SMOOTHLY TRANSITIONED INTO THE 6:1 AREA.

25 FT. MINIMUM RADII SHALL BE USED ON FIELD AND RESIDENTIAL DRIVES.
▲ WIDTH TO BE SHOWN ON PLANS

SIDE DRAINS SHALL BE LOCATED BEYOND THE CLEAR ZONE, OR WHEN WITHIN THE CLEAR ZONE, THEY SHALL BE INSTALLED WITH END SECTIONS CONFORMING TO A 6:1 SLOPE. FIFTY FT. RADII SHALL BE USED ON INTERSECTING ROADS, EXCEPT FOR FIELD AND RESIDENTIAL DRIVES OR UNLESS OTHERWISE SPECIFIED ON PLANS. RADII MAY BE VARIED TO SUIT FIELD CONDITIONS.

TYPICAL PLANS FOR SIDE APPROACH ROAD

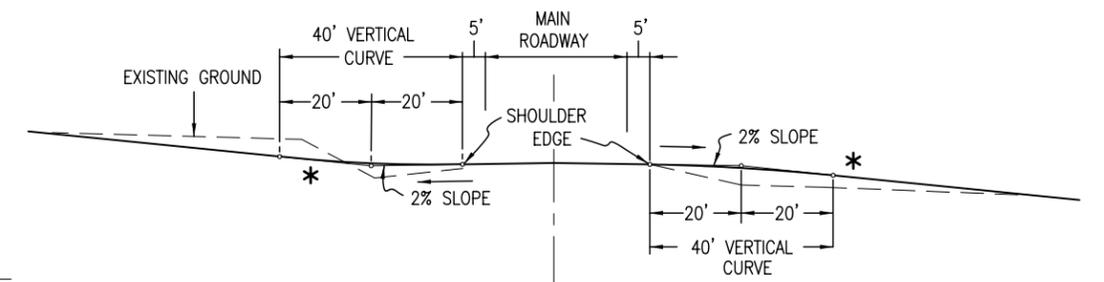


1/2 CUT SECTION 1/2 FILL SECTION

TYPICAL SECTION FOR APPROACH ROAD

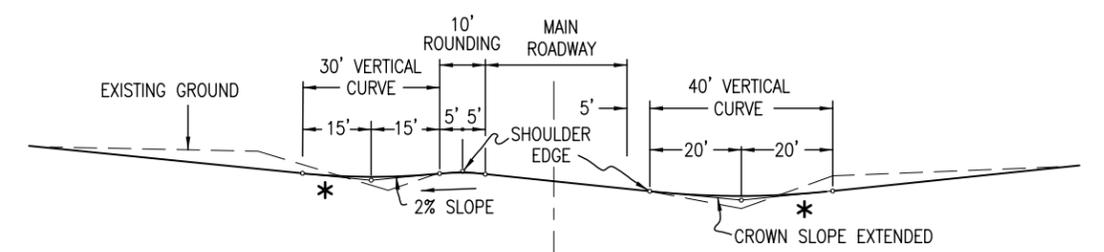
● ROAD APPROACHES WHICH REQUIRE HMA PAVEMENT, SHALL BE PLACED AS FOLLOWS:

PUBLIC APPROACHES AND ENTRANCES TO BUILDINGS OR RESIDENCES SHALL BE PAVED 50 FT. OUT FROM EDGE OF SHOULDER OR TO THE RIGHT OF WAY LINE, WHICHEVER IS LESS. FIELD ENTRANCES SHALL BE PAVED 4 FT. OUT FROM EDGE OF SHOULDER. STABILIZATION THICKNESSES SHALL BE AS SHOWN ON THE PLANS.

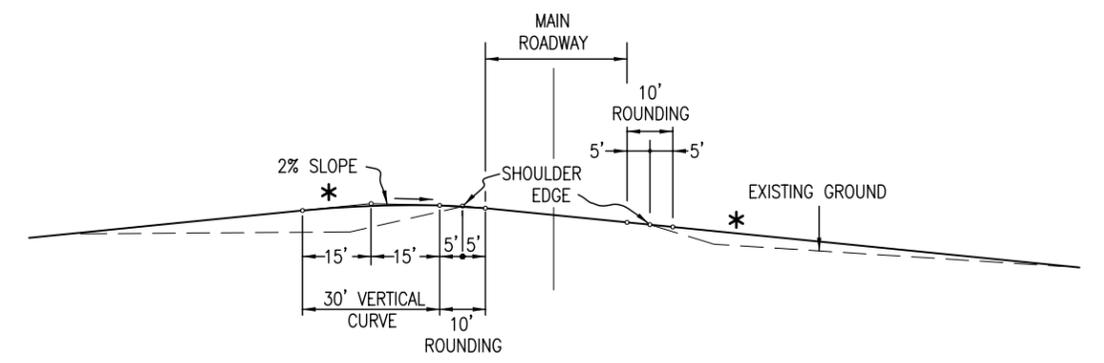


1/2 CUT SECTION 1/2 FILL SECTION

STANDARD CROWNED SECTION



SUPERELEVATED CUT SECTION



SUPERELEVATED FILL SECTION

VERTICAL ALIGNMENT SIDE APPROACH ROADS INTERSECTING MAIN ROADWAY

* TANGENT SLOPE NOT STEEPER THAN 8% BEYOND THE VERTICAL CURVE. THE SLOPE MAY BE STEEPER, IF REQUIRED, TO MEET EXISTING APPROACH SLOPE. HOWEVER, APPROACH ROAD SLOPE SHOULD NOT BE STEEPER THAN EXISTING SLOPE.

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Date:	Comments
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(R-X)	
(R-X)	

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APPROACH ROADS
 Issued By: Project Development Branch on July 04, 2006

STANDARD PLAN NO.
 M-203-1
 Sheet No. 1 of 1