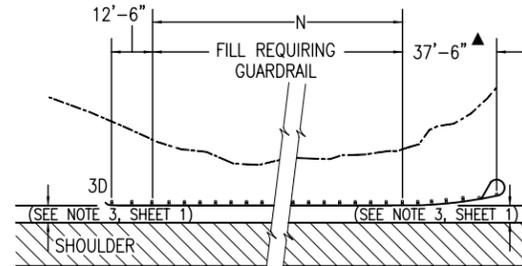


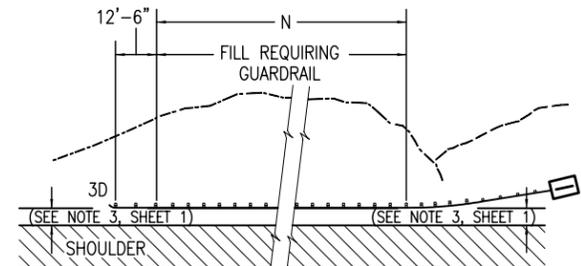
ONE-WAY

TRAFFIC



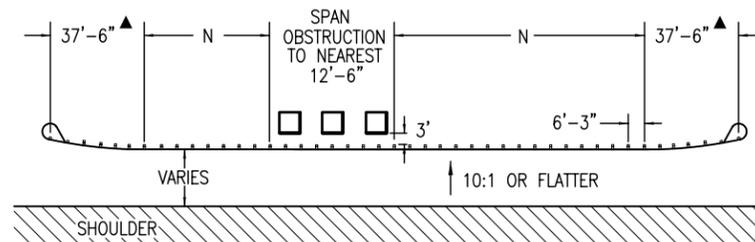
ONE-WAY

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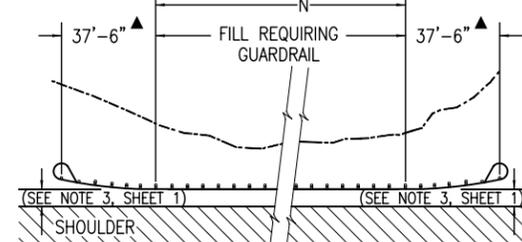
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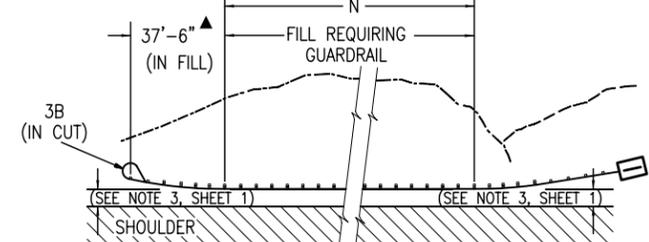
TWO-WAY

TRAFFIC



TWO-WAY

TRAFFIC



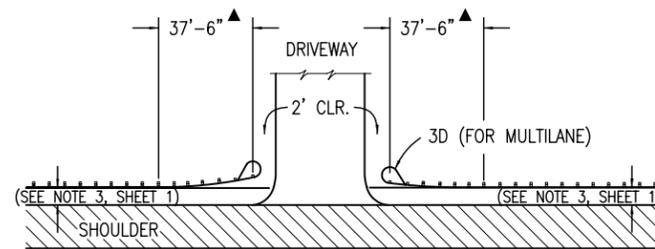
TWO-WAY

TRAFFIC

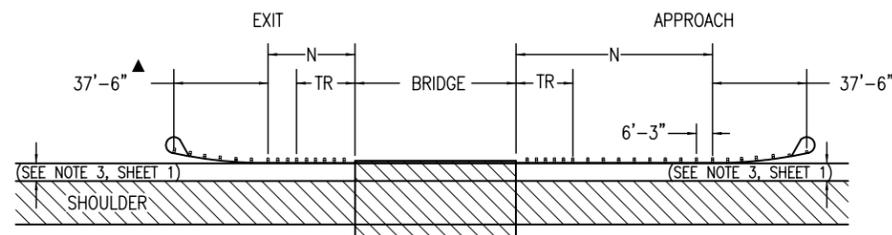
GUARDRAIL FOR ROADSIDE OBSTRUCTIONS

GUARDRAIL FOR ROADSIDE FILL CONSTRUCTION

GUARDRAIL FOR ROADSIDE CUT-TO-FILL CONDITION



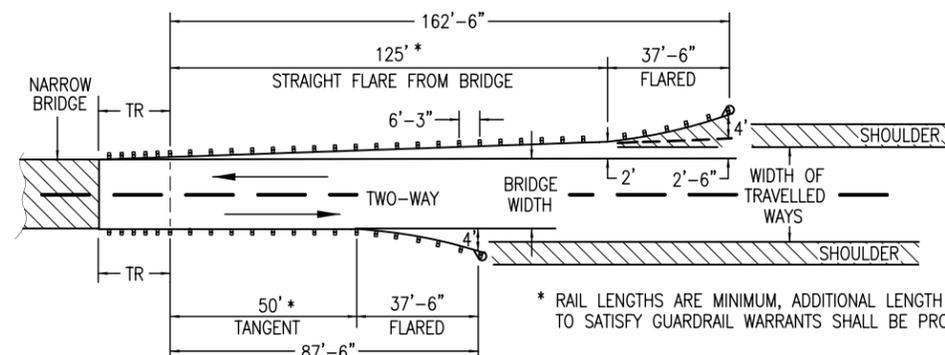
LAYOUT FOR DRIVEWAY APPROACH



APPROACH

EXIT

2-WAY NORMAL BRIDGE APPLICATION



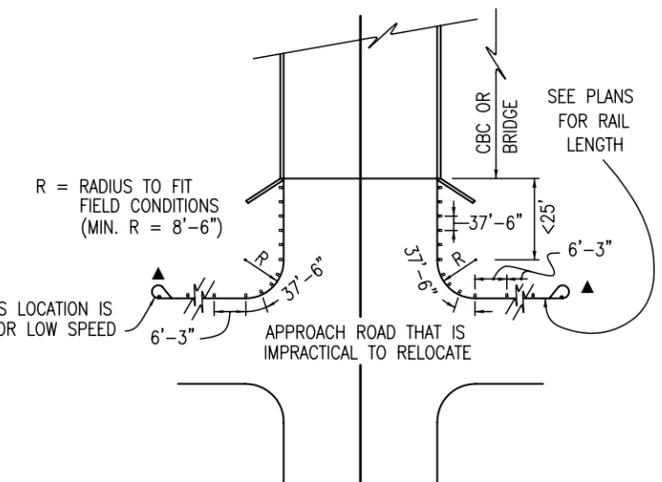
2-WAY NARROW APPLICATION

* RAIL LENGTHS ARE MINIMUM, ADDITIONAL LENGTH TO SATISFY GUARDRAIL WARRANTS SHALL BE PROVIDED

NOTES

1. THE TYPE 3G OR 3H TRANSITIONS (SEE SHEET 10) SHALL BE USED TO CONNECT A TYPE 3 W-BEAM TO TYPE 7 CONCRETE BARRIER OR TO A TYPE 7, 8, OR 10 BRIDGE RAIL. FOR A TRANSITION FROM A ROADWAY TYPE 3 W-BEAM TO A BRIDGE RAIL TYPE 3 WITH BACKING TUBES, THE TRANSITION TYPE 3L SHOWN ON SHEET 16 SHALL BE USED.
2. "TR" WILL BE 18 FT.-9 IN. FOR THE TRANSITIONS TYPE 3G AND 3H, AND 25 FT. FOR THE TRANSITION TYPE 3L.
3. THE GUARDRAIL LENGTH DIMENSION "N" IS THE LENGTH AS DETERMINED BY THE LENGTH OF NEED COMPUTATION AND IS SHOWN ON THE PLANS. THE MINIMUM IS 12 FT.-6 IN. WHERE SITE CONDITIONS ALLOW. THE OVERALL REQUIRED LENGTH OF NEED CAN INCLUDE THE LENGTH OF TRANSITION, THE LENGTH OF RAIL (N), AND ANY REDIRECTIVE LENGTH IN THE RAIL END TREATMENT. A TRAVERSABLE SLOPE SHALL BE PROVIDED BEHIND THE TERMINAL TO DIMENSION "N" PRIOR TO THE OBSTRUCTION UNLESS OTHERWISE APPROVED BY THE ENGINEER.

▲ END ANCHORAGE CAN BE FLARED OR NONFLARED



USE 3K IF THIS LOCATION IS ON DRIVEWAY OR LOW SPEED SERVICE ROAD.

APPROACH ROAD THAT IS IMPRACTICAL TO RELOCATE

GUARDRAIL TYPE 3 WITH BLOCKED OUT POSTS SPACED AT 3'-1 1/2" FROM STRUCTURE AROUND CURVE.

INTERRUPTED STRUCTURE APPROACH

(USE TYPE 3J ON SHEET 11 WHEN PRACTICAL)

Computer File Information

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 Last Modification Date: 07/04/06 Initials: LTA
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 CAD Ver.: MicroStation V8 Scale: Not to Scale Units: English

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(R-X)	

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GUARDRAIL TYPE 3 W-BEAM

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STANDARD PLAN NO.

M-606-1

Sheet No. 14 of 16