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Colorado Department of Transportation

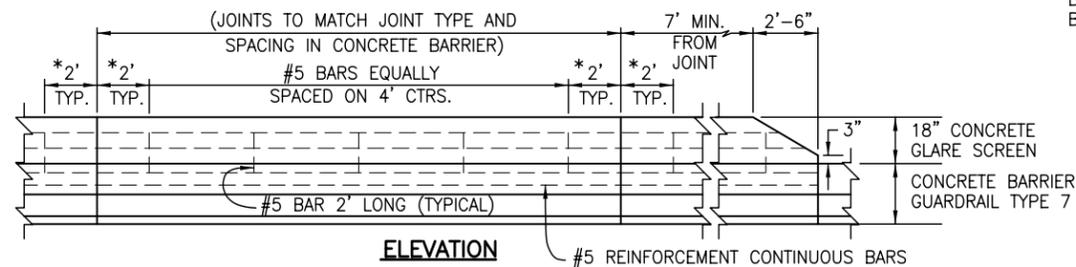
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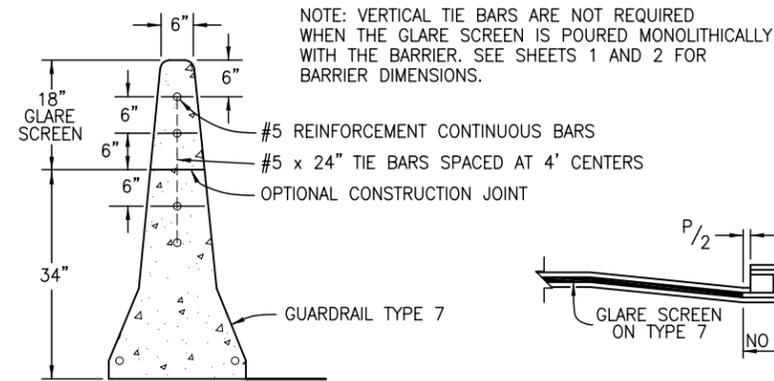
GUARDRAIL TYPE 7
F-SHAPE BARRIER

Issued By: Project Development Branch on July 04, 2006

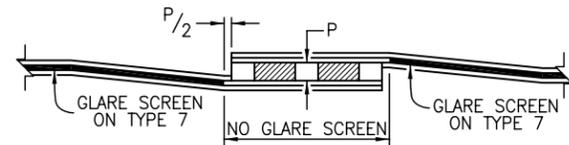
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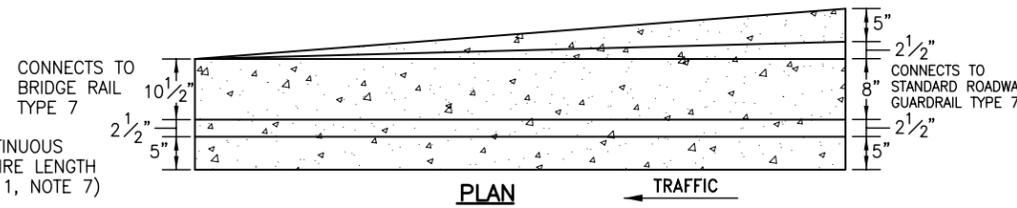
* 2 FT. IS TYPICAL FOR CAST-IN-PLACE BARRIERS.
1 FT. IS TYPICAL FOR PRECAST BARRIERS.
THE MINIMUM ACCEPTABLE DIMENSION IS 6 IN.



CONCRETE GLARE SCREEN

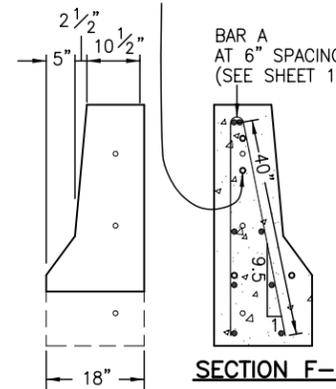


GLARE SCREEN AT MEDIAN OBSTRUCTIONS

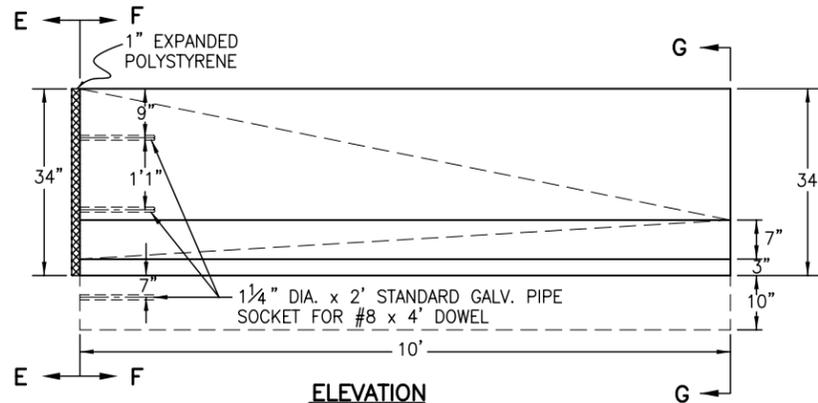


PLAN

4 #5 LONGITUDINAL CONTINUOUS REINFORCEMENT FOR ENTIRE LENGTH OF BARRIER (SEE SHEET 1, NOTE 7)



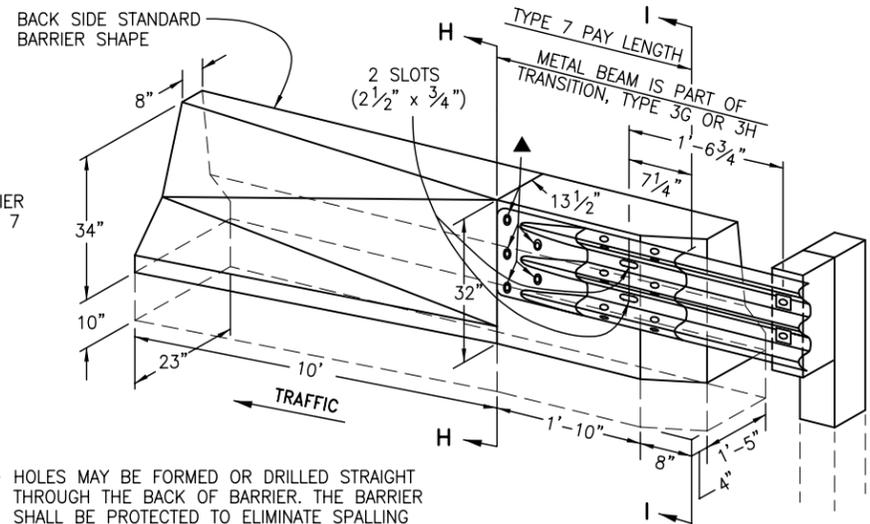
SECTION E-E



ELEVATION

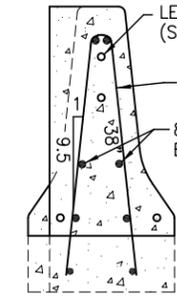
THIS SECTION PROVIDES A TRANSITION FOR THE SHAPE OF THE BRIDGE RAIL TYPE 7 TO THE ROADWAY GUARDRAIL TYPE 7. MEASURED AND PAID FOR AS GUARDRAIL TYPE 7. (SEE ANCHORAGE DETAIL ON SHEET 1 FOR REINFORCEMENT INFORMATION)

BRIDGE RAIL TYPE 7 TO ROADWAY SHOULDER TYPE 7 TRANSITION AND ANCHORAGE

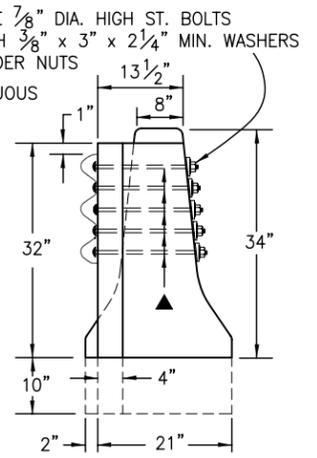


TYPE 7 TO SINGLE TYPE 3G TRANSITION AND ANCHORAGE

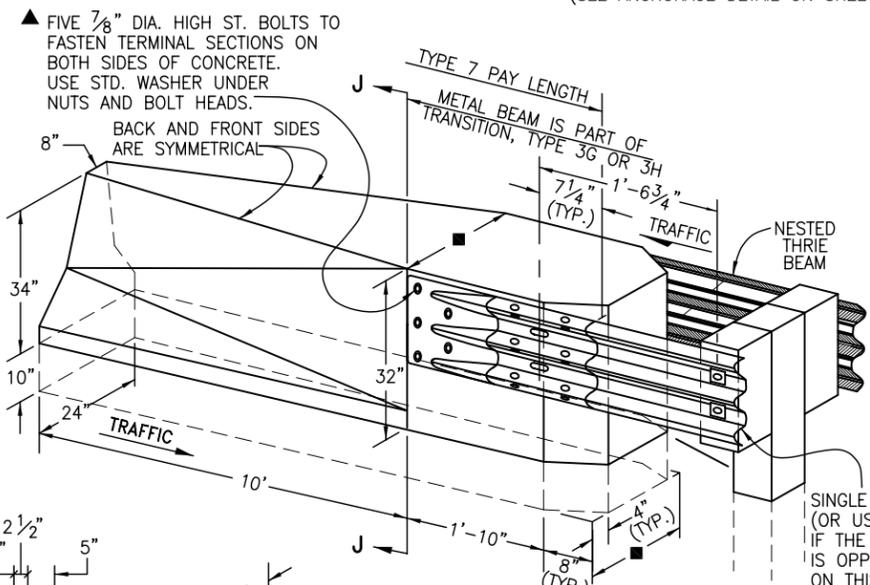
(SEE ANCHORAGE DETAIL ON SHEET 1 FOR REINFORCEMENT INFORMATION)



SECTION H-H

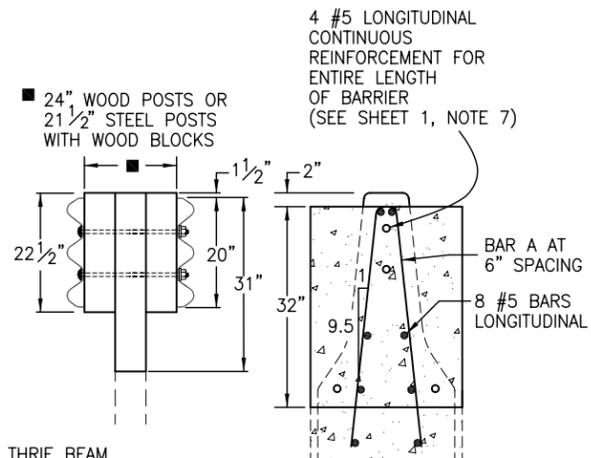


SECTION I-I



TYPE 7 TO DOUBLE TYPE 3G TRANSITION AND ANCHORAGE

(SEE ANCHORAGE DETAIL ON SHEET 1 FOR REINFORCEMENT INFORMATION)



SECTION J-J

▲ HOLES MAY BE FORMED OR DRILLED STRAIGHT THROUGH THE BACK OF BARRIER. THE BARRIER SHALL BE PROTECTED TO ELIMINATE SPALLING WHEN DRILLED.

▲ FIVE 7/8" DIA. HIGH ST. BOLTS TO FASTEN TERMINAL SECTIONS ON BOTH SIDES OF CONCRETE. USE STD. WASHER UNDER NUTS AND BOLT HEADS.

SINGLE THRIE BEAM (OR USE DOUBLE THRIE BEAM IF THE DIRECTION OF TRAFFIC IS OPPOSITE OF THAT SHOWN ON THIS SIDE)

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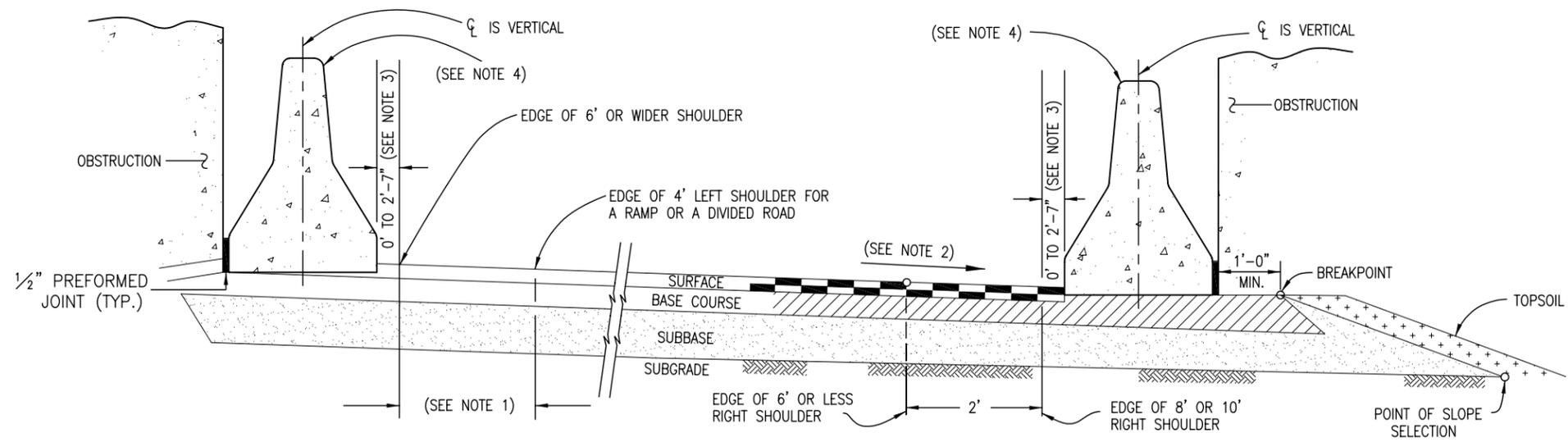
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**GUARDRAIL TYPE 7
F-SHAPE BARRIER**

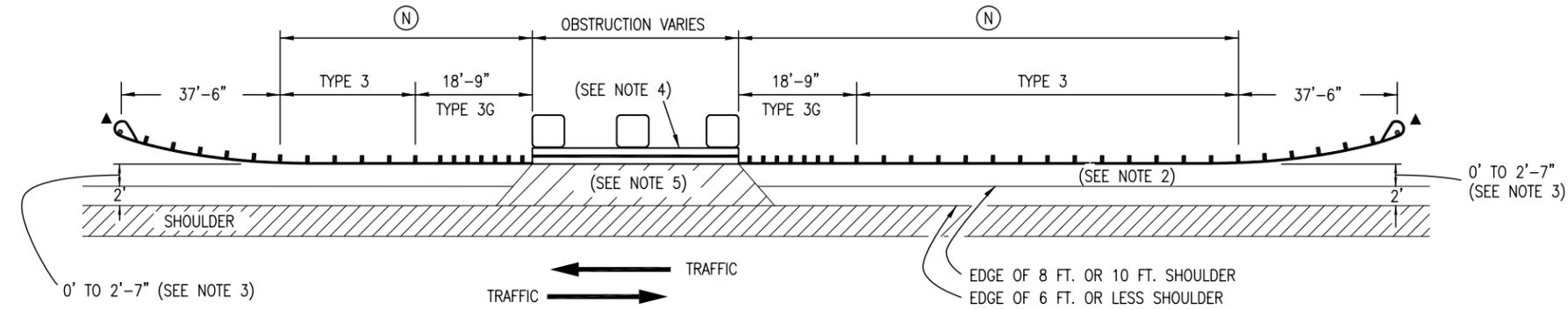
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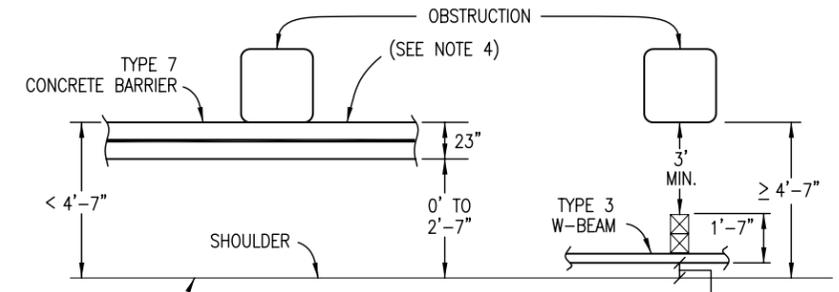


TYPE 7 ON LEFT AND RIGHT SHOULDERS AT OBSTRUCTIONS

- NOTES**
- TWO FT. IS DESIRABLE FOR THIS DIMENSION WITH A 4 FT. LEFT SHOULDER. THE MINIMUM IS 0 FT., WHICH IS ACCEPTABLE FOR 6 FT. OR WIDER SHOULDERS.
 - RATE OF SLOPE DEPENDS ON GUARDRAIL LOCATION:
 - FOR GUARDRAIL FACE 2 FT. OR LESS FROM THE NORMAL EDGE OF PAVED SHOULDER, CONTINUE THE RATE OF SLOPE OF THE NORMAL PAVED SHOULDER TO THE BREAKPOINT.
 - FOR GUARDRAIL FACE MORE THAN 2 FT. FROM THE NORMAL EDGE OF THE PAVED SHOULDER, THE SLOPE SHALL BE 10:1 OR FLATTER.
 - IF THE DISTANCE FROM THE EDGE OF SHOULDER TO THE OBSTRUCTION EXCEEDS 4 FT.-7 IN., TYPE 3-W BEAM GUARDRAIL MAY BE SPECIFIED ON THE PLANS INSTEAD OF TYPE 7 (SEE PLANS, AND DETAIL BELOW).
 - STYLE CA BARRIERS ARE SHOWN. STYLE CD MAY BE USED AS APPROPRIATE. SEE SHEET 2 FOR TYPE 7 TO SINGLE TYPE 3G TRANSITION.
 - THE AREA BETWEEN SHOULDER AND THE TYPE 7 SHALL BE PAVED. PAYMENT FOR THE PAVED SURFACE WILL BE MADE UNDER A PAVEMENT PAY ITEM, HMA OR CONCRETE, WITH QUANTITIES SHOWN ON THE PLANS.
- (N) THE GUARDRAIL LENGTH DIMENSION "N" IS THE LENGTH AS DETERMINED BY THE LENGTH OF NEED COMPUTATION AND AS SHOWN ON THE PLANS. MINIMUM SHALL BE 12 FT.-6 IN. WHERE SITE CONDITIONS ALLOW.

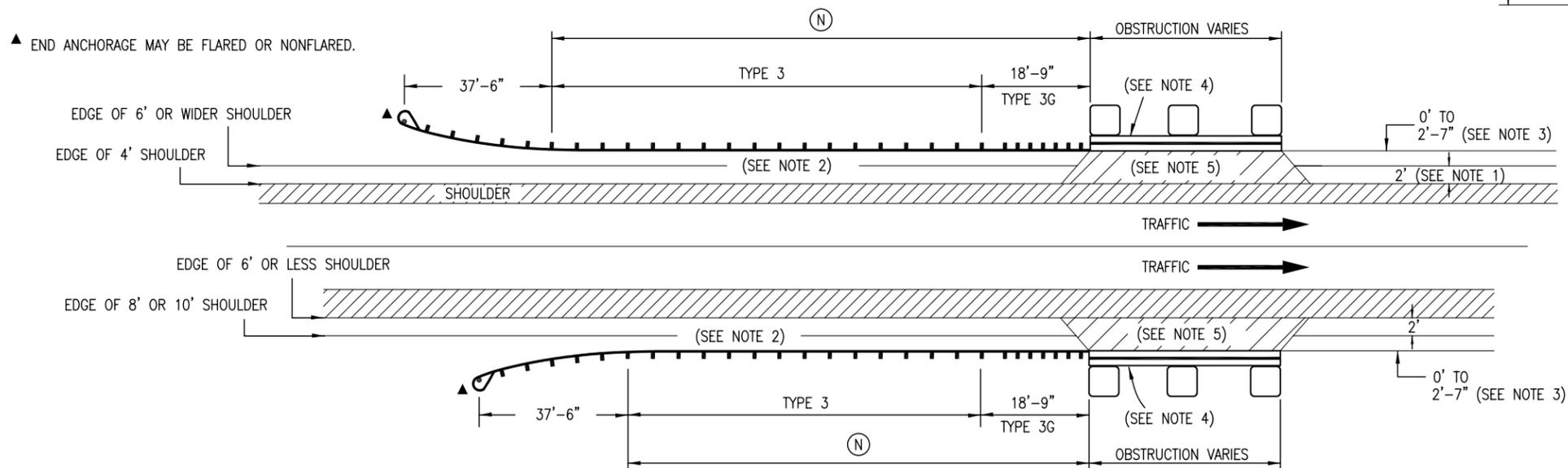


2-LANE 2-WAY ROADS



LIMIT OF GUARDRAIL INSTALLATION IN RESTRICTED CLEARANCE SITUATIONS. SEE THE DETAIL TYPE 7 ON LEFT AND RIGHT SHOULDERS AT OBSTRUCTIONS, AND NOTE 3.

SEE M-606-1, SHEET 1, NOTE 3



DIRECTIONAL ROADWAYS AND RAMP

HAZARDS ON ROADSIDES

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