

Oversight / NHS

FHWA REGION VIII OVERSIGHT? NO YES

NATIONAL HIGHWAY SYSTEM? NO YES

PLENARY ROADS DENVER, LLC

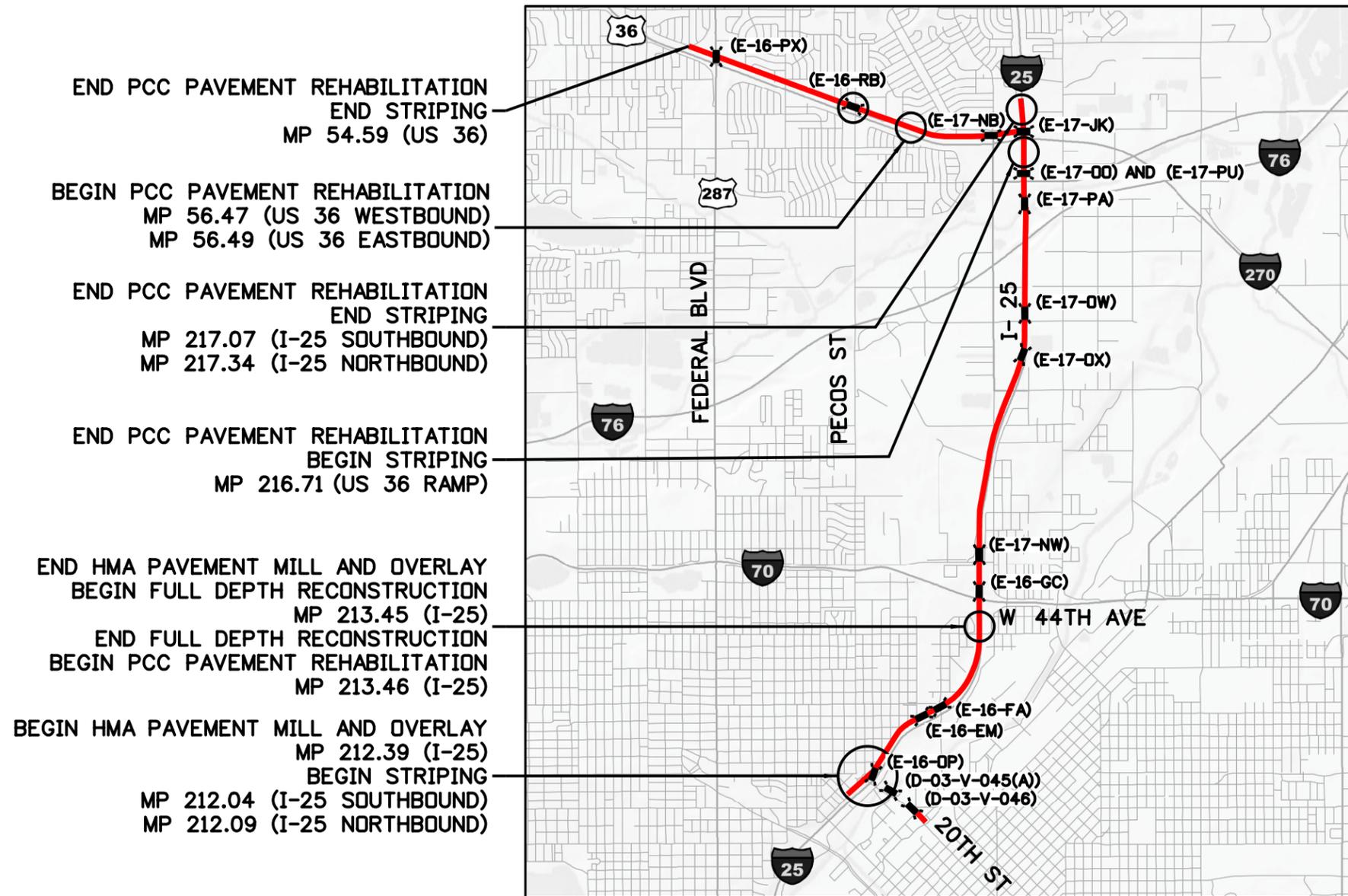
HIGHWAY CONSTRUCTION BID PLANS OF PROPOSED

PROJECT NO. 2015-001

INTERSTATE HIGHWAY I-25 AND STATE HIGHWAY NO. 36

DENVER COUNTY AND ADAMS COUNTY

I-25 ML INITIAL WORKS - PAVEMENTS



END PCC PAVEMENT REHABILITATION
END STRIPING
MP 54.59 (US 36)

BEGIN PCC PAVEMENT REHABILITATION
MP 56.47 (US 36 WESTBOUND)
MP 56.49 (US 36 EASTBOUND)

END PCC PAVEMENT REHABILITATION
END STRIPING
MP 217.07 (I-25 SOUTHBOUND)
MP 217.34 (I-25 NORTHBOUND)

END PCC PAVEMENT REHABILITATION
BEGIN STRIPING
MP 216.71 (US 36 RAMP)

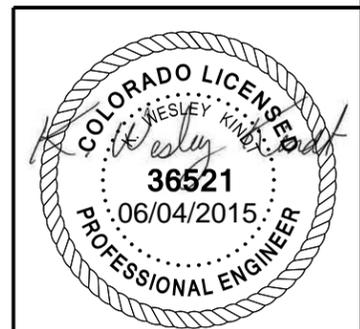
END HMA PAVEMENT MILL AND OVERLAY
BEGIN FULL DEPTH RECONSTRUCTION
MP 213.45 (I-25)
END FULL DEPTH RECONSTRUCTION
BEGIN PCC PAVEMENT REHABILITATION
MP 213.46 (I-25)

BEGIN HMA PAVEMENT MILL AND OVERLAY
MP 212.39 (I-25)
BEGIN STRIPING
MP 212.04 (I-25 SOUTHBOUND)
MP 212.09 (I-25 NORTHBOUND)

| SHEET NO. | INDEX OF SHEETS |
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| ML-2 | STANDARD PLANS LIST |
| ML-3 | GENERAL NOTES |
| ML-4 to ML-5 | TYPICAL SECTIONS |
| ML-6 to ML-7 | SUMMARY OF APPROXIMATE QUANTITIES |
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| AD ALTERNATIVE 1 SHEETS | |
|-------------------------|-----------------------------|
| BR1401 | GENERAL PLAN (1 of 2) |
| BR1402 | GENERAL PLAN (2 of 2) |
| BR1403 | MILL AND TRANSITION DETAILS |

PROJECT LOCATION MAP



Print Date: 5/12/2015
File Name: 20062DES_I-25_HMA_TitleSht
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Phone: 303-803-9452
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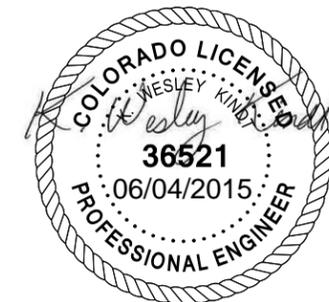
I-25 ML INITIAL WORKS
HMA AND PCC PAVEMENT
TITLE SHEET
Designer: E. MARSH
Detailer: T. HAUGH
Sheet Subset: TITLE
Subset Sheets: 1 of 1

Project No./Code
2014-001
Sheet: ML-1

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| INDEX | | | ITEM NO. | CONTRACT ITEM | UNITS | ROADWAY | | PROJECT TOTALS | |
|-------|------|-------|-----------|---|-------|---------|-----------|----------------|------------|
| BOOK | PAGE | SHEET | | | | PLAN | AS CONST. | PLAN | AS. CONST. |
| | | | 630-00007 | TRAFFIC CONTROL INSPECTION | DAY | 55 | | 55 | |
| | | | 630-00012 | TRAFFIC CONTROL MANAGEMENT | DAY | 120 | | 120 | |
| | | | 630-80001 | FLASHING BEACON (PORTABLE) | EACH | 8 | | 8 | |
| | | | 630-80335 | BARRICADE (TYPE 3 M-A) (TEMPORARY) | EACH | 3 | | 3 | |
| | | | 630-80341 | CONSTRUCTION TRAFFIC SIGN (PANEL SIZE A) | EACH | 14 | | 14 | |
| | | | 630-80342 | CONSTRUCTION TRAFFIC SIGN (PANEL SIZE B) | EACH | 30 | | 30 | |
| | | | 630-80343 | CONSTRUCTION TRAFFIC SIGN (PANEL SIZE C) | EACH | 18 | | 18 | |
| | | | 630-80355 | PORTABLE MESSAGE SIGN PANEL | EACH | 1 | | 1 | |
| | | | 630-80358 | ADVANCE WARNING FLASHING OR SEQUENCING ARROW PANEL (C TYPE) | EACH | 2 | | 2 | |
| | | | 630-80360 | DRUM CHANNELIZING DEVICE | EACH | 120 | | 120 | |
| | | | 630-80363 | DRUM CHANNELIZING DEVICE (WITH LIGHT) (FLASHING) | EACH | 4 | | 4 | |
| | | | 630-80380 | TRAFFIC CONE | EACH | 400 | | 400 | |
| | | | | <u>FORCE ACCOUNT ITEMS</u> | | | | | |
| | | | 700-70010 | F/A MINOR CONTRACT REVISIONS | F A | 1 | | 1 | |
| | | | 700-70012 | F/A ASPHALT PAVEMENT INCENTIVE | F A | 1 | | 1 | |
| | | | 700-70013 | F/A CONCRETE PAVEMENT INCENTIVE | F A | 1 | | 1 | |
| | | | 700-70016 | F/A FUEL COST ADJUSTMENT | F A | 1 | | 1 | |
| | | | 700-70018 | F/A ROADWAY SMOOTHNESS INCENTIVE | F A | 1 | | 1 | |
| | | | 700-70019 | F/A ASPHALT CEMENT COST ADJUSTMENT FOR HMA AND SMA | F A | 1 | | 1 | |
| | | | 700-70025 | F/A QUALITY INCENTIVE PAYMENT | F A | 1 | | 1 | |
| | | | 700-70380 | F/A EROSION CONTROL | F A | 1 | | 1 | |



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I-25 ML INITIAL WORKS
 HMA AND PCC PAVEMENT
 SUMMARY OF APPROX. QUANTITIES
 Designer: E. MARSH Structure Numbers
 Detailer: E. MARSH
 Sheet Subset: SDAQ Subset Sheets: 2 of 2

Project No./Code
 2014-001
 Sheet: ML-7

SCHEDULE OF CONSTRUCTION TRAFFIC CONTROL SIGNS

| SIGN CODE | LEGEND | DIMENSIONS | CONSTRUCTION TRAFFIC SIGN (EACH) [I-25] | | | CONSTRUCTION TRAFFIC SIGN (EACH) [US 36] | | |
|--------------------|---|------------|---|----------|----------|--|-----------|-----------|
| | | | PANEL SIZE | | | PANEL SIZE | | |
| | | | A | B | C | A | B | C |
| R2-1(35) | "SPEED/LIMIT/35" | 48" x 60" | | | | | | |
| R2-1(45) | "SPEED/LIMIT/45" | 48" x 60" | | | | | | |
| R2-1(55) | "SPEED/LIMIT/55" | 48" x 60" | | | | | 2 | |
| R2-6P | "FINES/DOUBLE" | 36" x 36" | | | | 6 | | |
| R4-1 | "DO/NOT/PASS" | 48" x 60" | | | | | | |
| R4-2 | "PASS/WITH/CARE" | 48" x 60" | | | | | | |
| R52-6a | "BEGIN/FINES/DOUBLE/IN WORK/ZONE" | 48" x 60" | | | 4 | | | 4 |
| R52-6b | "END/FINES/DOUBLE/IN WORK/ZONE" | 48" x 60" | | | 4 | | | 4 |
| W1-4L | LANE CURVE SYMBOL (LEFT) | 48" x 48" | | | | | | |
| W1-4R | LANE CURVE SYMBOL (RIGHT) | 48" x 48" | | | | | | |
| W1-6L | LARGE ARROW (LEFT) | 48" x 12" | | | | | | |
| W1-6R | LARGE ARROW (RIGHT) | 48" x 12" | | | | | | |
| W4-2L | LEFT LANE CLOSED SYMBOL | 48" x 48" | | | | | 4 | |
| W4-2R | RIGHT LANE CLOSED SYMBOL | 48" x 48" | | | | | 4 | |
| W5-1 | "ROAD/NARROWS" | 48" x 48" | | | | | | |
| W20-1 | "ROAD/WORK/(DIST.)" | 48" x 48" | | 4 | | | | 2 |
| W20-5L | "LEFT/LANE/CLOSED/AHEAD" | 48" x 48" | | | | | | 2 |
| W20-5R | "RIGHT/LANE/CLOSED/AHEAD" | 48" x 48" | | | | | | 2 |
| W20-5aL | "LEFT/TWO LANES/CLOSED/1/2 MILE" | 48" x 48" | | | | | | 2 |
| W20-5aR | "RIGHT/TWO LANES/CLOSED/1/2 MILE" | 48" x 48" | | | | | | 2 |
| W21-5aL | "LEFT/SHOULDER/CLOSED" | 48" x 48" | | | | | | |
| W21-5aR | "RIGHT/SHOULDER/CLOSED" | 48" x 48" | | | | | | |
| G20-5P | "WORK/ZONE" | 36" x 30" | | | | | 8 | |
| G20-10 | "XYZ/CONSTRUCTORS/THANK YOU/..." | 48" x 48" | | 2 | | | | 2 |
| G20-11 | "ROAD WORK/DATE-DATE/FOR INFORMATION/..." | 48" x 48" | | 2 | | | | 2 |
| SIGN TOTALS | | | 0 | 8 | 8 | 14 | 22 | 10 |

CONSTRUCTION TRAFFIC CONTROL DEVICES

| ITEM NUMBER | DESCRIPTION | SIZE | UNIT | QUANTITY | |
|-------------|---|------|------|----------|-------|
| | | | | I-25 | US 36 |
| 630-00000 | FLAGGING | | HOUR | 30 | 300 |
| 630-00007 | TRAFFIC CONTROL INSPECTION | | DAY | 15 | 40 |
| 630-00012 | TRAFFIC CONTROL MANAGEMENT | | DAY | 40 | 80 |
| 630-80001 | FLASHING BEACON (PORTABLE) | | EACH | 0 | 8 |
| 630-80335 | BARRICADE (TYPE 3M-A) (TEMP) | | EACH | 0 | 3 |
| 630-80355 | PORTABLE MESSAGE SIGN PANEL | | EACH | 0 | 1 |
| 630-80341 | CONSTRUCTION TRAFFIC SIGN (PANEL SIZE A) | | EACH | 0 | 14 |
| 630-80342 | CONSTRUCTION TRAFFIC SIGN (PANEL SIZE B) | | EACH | 8 | 22 |
| 630-80343 | CONSTRUCTION TRAFFIC SIGN (PANEL SIZE C) | | EACH | 8 | 10 |
| 630-80358 | ADVANCE WARNING FLASHING OR SEQUENCING ARROW PANEL (C TYPE) | | EACH | 0 | 2 |
| 630-80360 | DRUM CHANNELIZING DEVICE | | EACH | 0 | 120 |
| 630-80363 | DRUM CHANNELIZING DEVICE (WITH LIGHT) (FLASHING) | | EACH | 0 | 4 |
| 630-80380 | TRAFFIC CONE | 36" | EACH | 200 | 200 |

NOTES:

- I-25 MANAGED LANE CONSTRUCTION PHASING HAS BEEN LAID OUT AS FOLLOWS:
 - HMA SECTIONS: THE TWO MILES OF HMA SHALL HAVE THE TOP 2" MILLED AND OVERLAID, EXCEPT FOR THE ~50-FOOT SECTION ON THE SOUTH END.
 - PCCP SECTIONS: THE FOUR MILES OF PCCP SHALL HAVE PANELS REPLACED AS DIRECTED BY THE ENGINEER.
- US 36 CONSTRUCTION PHASING HAS BEEN LAID OUT AS FOLLOWS:
 - EB: THE TWO MILES OF EB LANES WILL BE CONSTRUCTED ONE HALF AT A TIME
 - WB: THE TWO MILES OF WB LANES WILL BE CONSTRUCTED ONE HALF AT A TIME
- PAY ITEMS WERE DETERMINED BASED ON CDOT M&S STANDARDS S-630-1 AS FOLLOWS:

US 36:

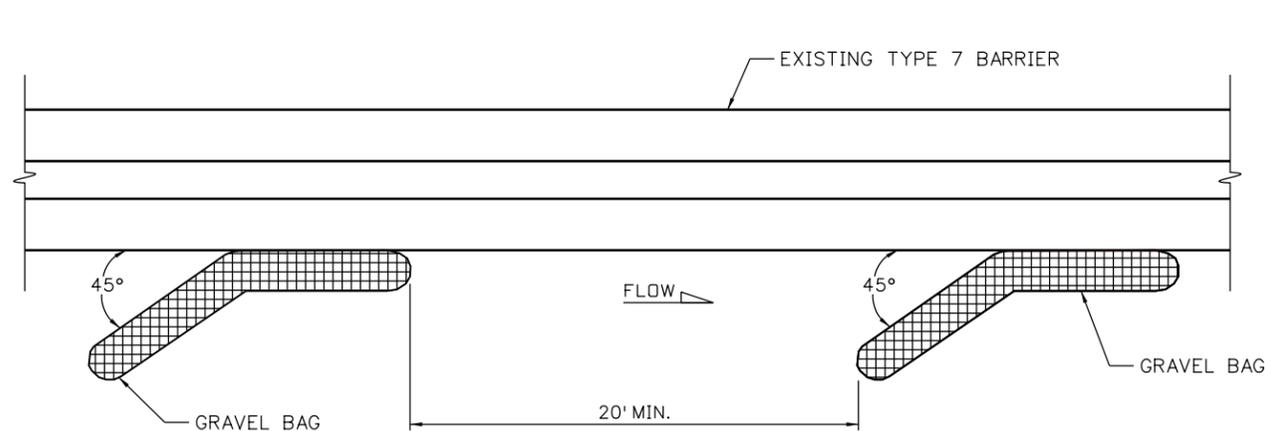
 - FOR EB US 36, CASE NO. 5 WAS USED FOR LEFT-LANE CLOSURE AND CASE NO. 8 WAS USED FOR RIGHT-LANE CLOSURE
 - FOR WB US 36, CASE NO. 6 WAS USED FOR LEFT-LANE CLOSURES AND CASE NO. 7 WAS USED FOR RIGHT-LANE CLOSURES
- FOR US 36, QUANTITIES WERE ESTIMATED BASED ON THE ANTICIPATED DURATIONS OF 50 WORKING DAYS.
- FOR US 36, QUANTITIES FOR DRUMS WERE ESTIMATED ASSUMING THAT EB WOULD BE DONE IN ONE PHASE AND WB IN ANOTHER PHASE.



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| Unit: | | | Void: | Designer: J. SEYER Detailer: E. MARSH Sheet Subset: TABS | | | | | | |
| 1670 BROADWAY, SUITE 3400 DENVER, COLORADO 80202 Phone: 303-764-1520 FAX: 303-860-7139 | | | | Structure Numbers Subset Sheets: 3 of 3 | Sheet: ML-10 | | | | | |

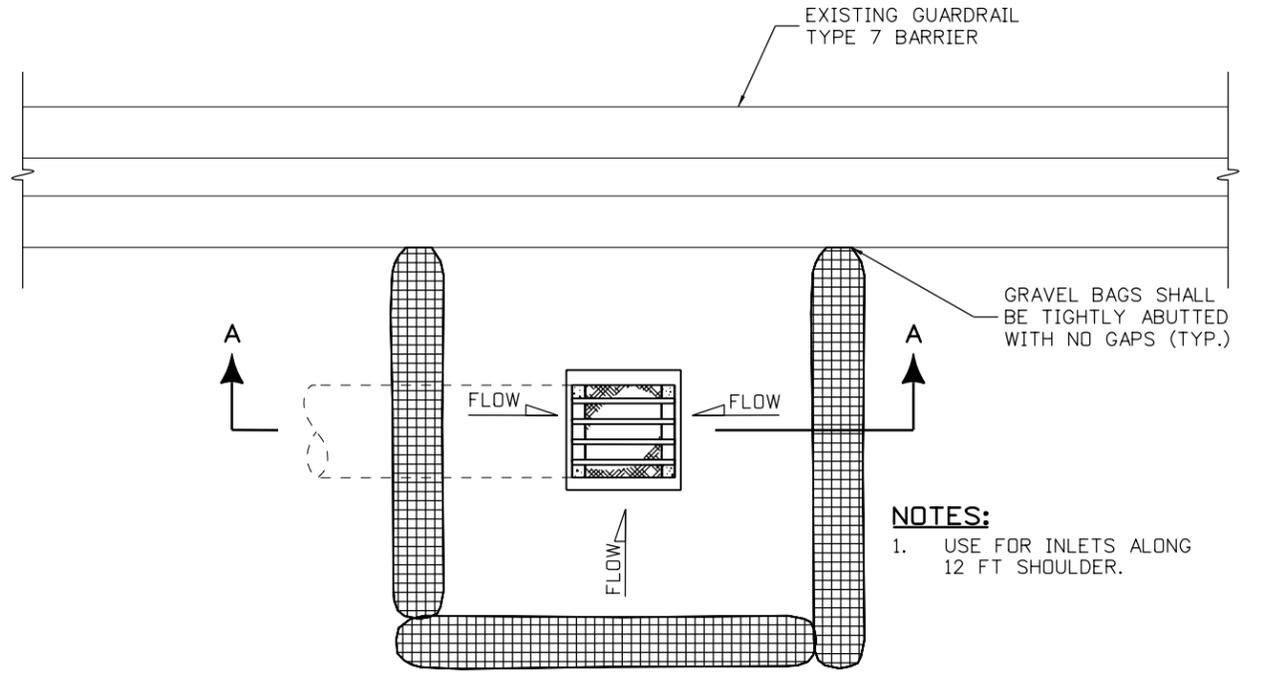
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PLAN VIEW FOR MULTIPLE GRAVEL BAGS
GRAVEL BAG CHECK
 N.T.S.

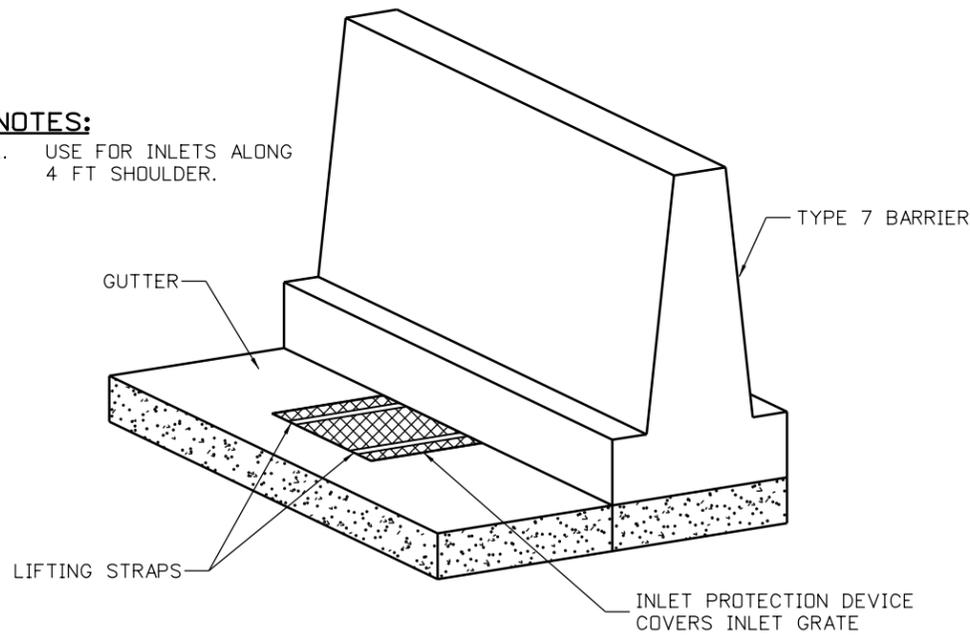
- NOTES:**
- PLACE THREE GRAVEL BAGS AT DOWNSTREAM END OF PROJECT



PLAN VIEW
 N.T.S.

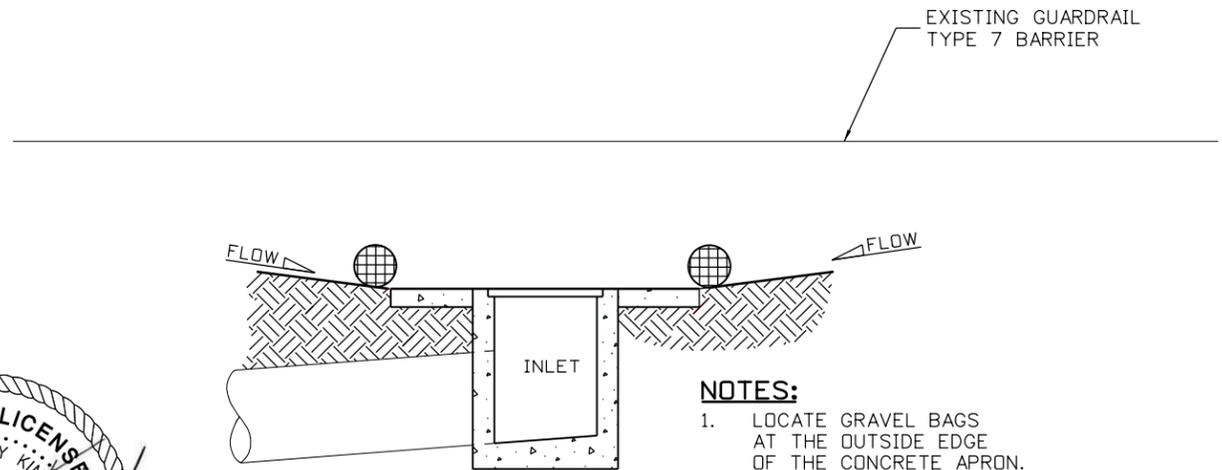
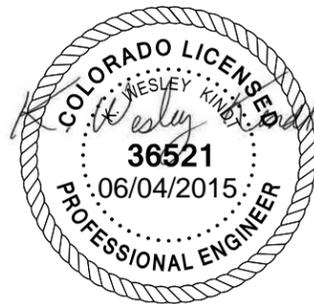
- NOTES:**
- USE FOR INLETS ALONG 12 FT SHOULDER.

- NOTES:**
- USE FOR INLETS ALONG 4 FT SHOULDER.



ISOMETRIC VIEW
STORM DRAIN INLET PROTECTION (TYPE II)
OPTION B
 N.T.S.

- NOTES:**
- CONTRACTOR TO INSPECT DURING RAIN EVENTS TO REDUCE FLOODING ON TRAVELED WAY.



SECTION A-A
GRAVEL BAG FILTER AT DROP INLET
 N.T.S.

- NOTES:**
- LOCATE GRAVEL BAGS AT THE OUTSIDE EDGE OF THE CONCRETE APRON.

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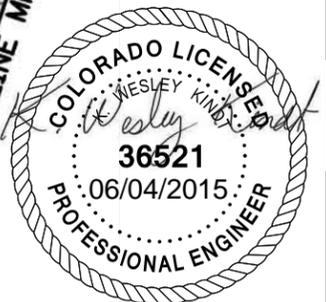
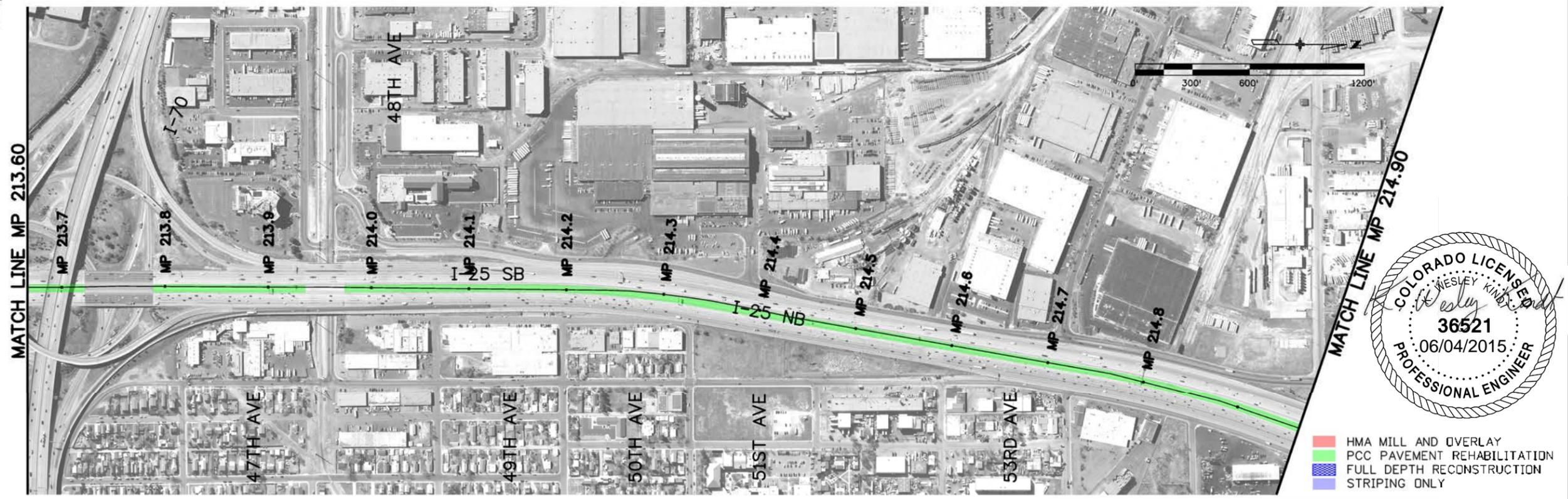
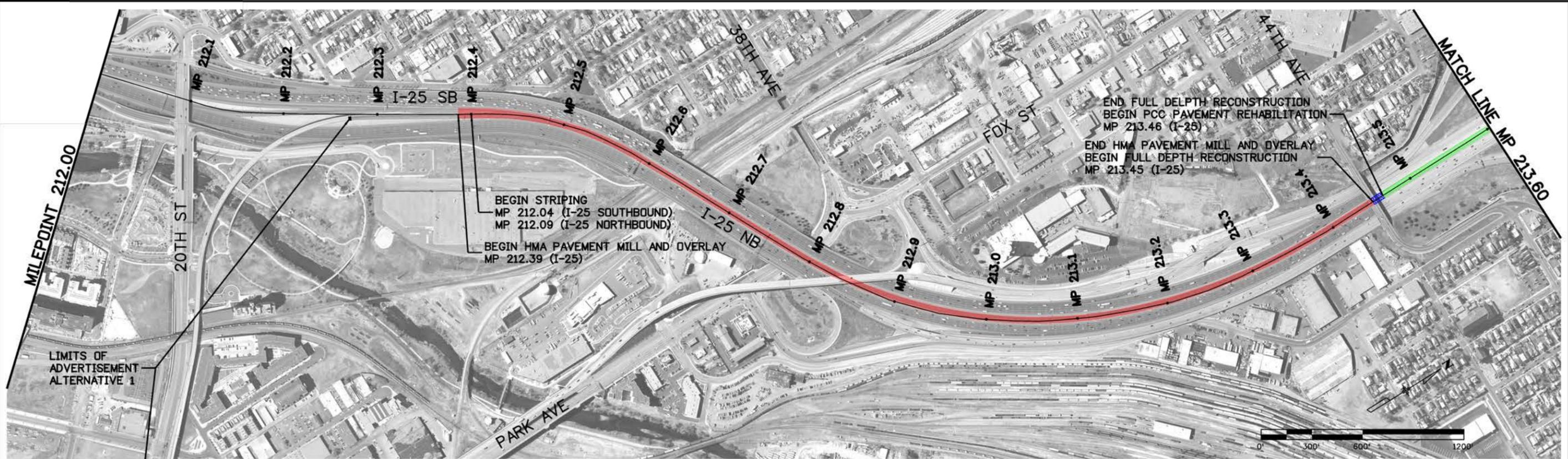
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| I-25 ML INITIAL WORKS | |
| HMA AND PCC PAVEMENT | |
| EROSION CONTROL DETAIL | |
| Designer: E. MARSH | Structure Numbers: |
| Detailer: T. HAUGH | |
| Sheet Subset: EROSION | Subset Sheets: 1 of 1 |

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| Project No./Code |
| 2014-001 |
| Sheet: ML-13 |

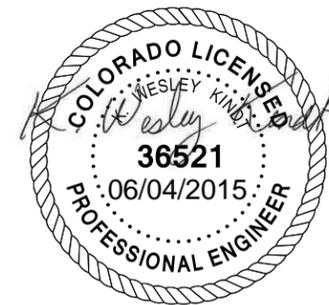
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- HMA MILL AND OVERLAY
- PCC PAVEMENT REHABILITATION
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- STRIPING ONLY

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- HMA MILL AND OVERLAY
- PCC PAVEMENT REHABILITATION
- FULL DEPTH RECONSTRUCTION
- STRIPING ONLY

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Plenary Roads

1700 Lincoln Street, Suite 3000
 Denver, Colorado 80203
 Phone: 303-803-9452
 FAX: 303-803-9453

| As Constructed |
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| No Revisions: |
| Revised: |
| Void: |

| I-25 ML INITIAL WORKS HMA AND PCC PAVEMENT US 36 PROJECT AREA MAP | | | |
|---|-----------|-------------------|--------|
| Designer: | E. MARSH | Structure Numbers | |
| Detailer: | T. HAUGH | Subset Sheets: | 4 of 4 |
| Sheet Subset: | PRJCT MAP | | |

| Project No./Code |
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| 2014-001 |
| Sheet: ML-16B |

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NOTE:

- ALL MANHOLES AND DRAINAGE INLETS WITHIN HMA MILL AND OVERLAY SECTION TO BE ADJUSTED UNLESS OTHERWISE NOTED.

EXISTING INLET
 EXISTING MANHOLE

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| Print Date: 5/11/2015 |
| File Name: 20062DES_I-25_HMA_Plan02 |
| Horiz. Scale: 1:100 Vert. Scale: As Noted |
| Unit: |
| HDR 1670 BROADWAY, SUITE 3400 DENVER, COLORADO 80202 Phone: 303-764-1520 FAX: 303-860-7139 |

| Sheet Revisions | | |
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| Date: | Comments | Init. |
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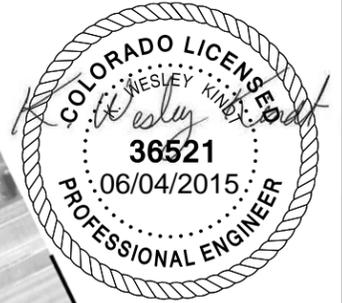
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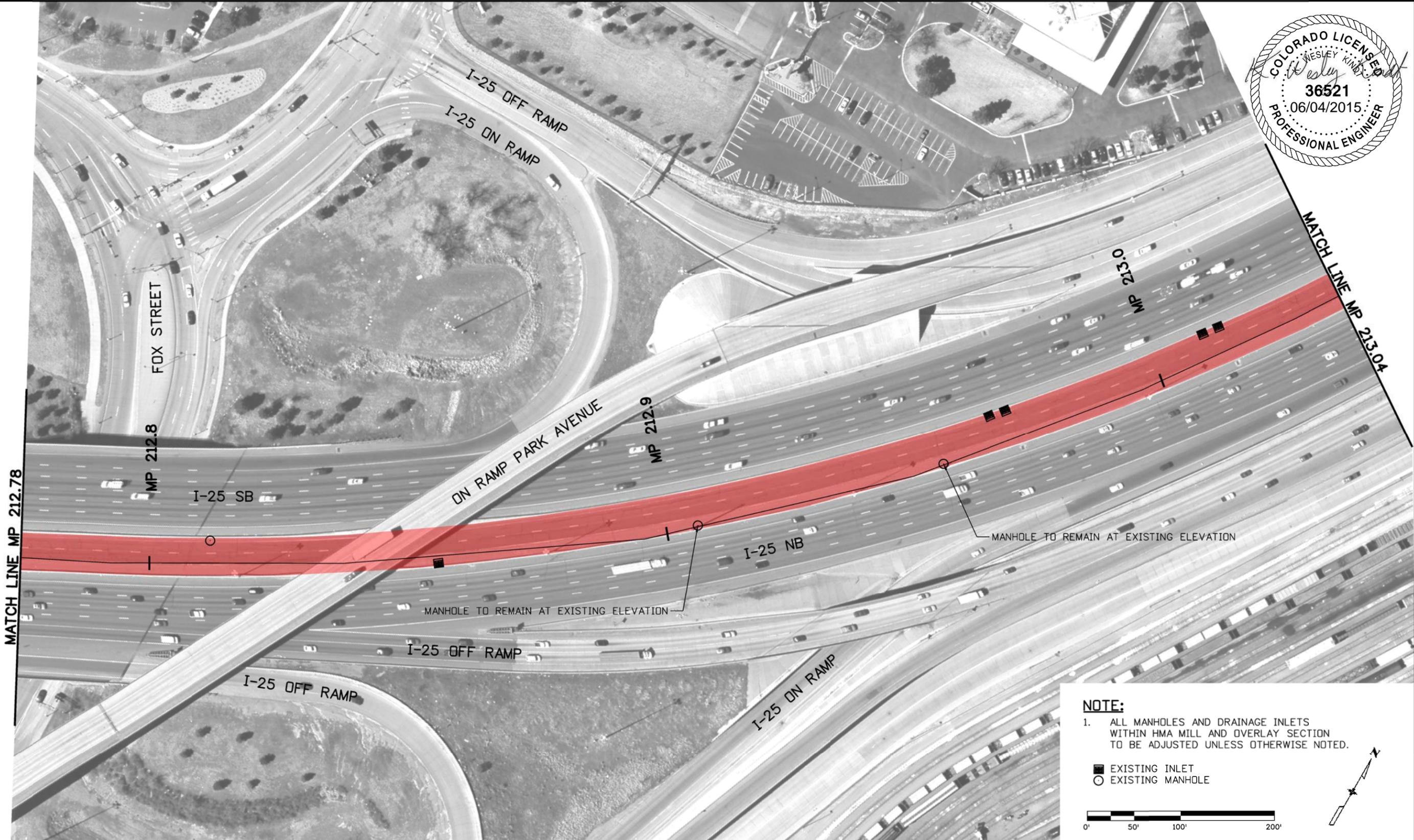
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| As Constructed |
| No Revisions: |
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| I-25 ML INITIAL WORKS HMA AND PCC PAVEMENT PLAN SHEET MP 212.51 - MP 212.78 | | | |
| Designer: | E. MARSH | Structure Numbers | |
| Detailer: | T. HAUGH | | |
| Sheet Subset: | PLAN | Subset Sheets: | 2 of 5 |

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| Project No./Code | 2014-001 |
| Sheet: | ML-18 |



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NOTE:
 1. ALL MANHOLES AND DRAINAGE INLETS WITHIN HMA MILL AND OVERLAY SECTION TO BE ADJUSTED UNLESS OTHERWISE NOTED.

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 ○ EXISTING MANHOLE



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| Print Date: 5/11/2015 |
| File Name: 20062DES_I-25_HMA_Plan03 |
| Horiz. Scale: 1:100 Vert. Scale: As Noted |
| Unit: |
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| Sheet Revisions | | | |
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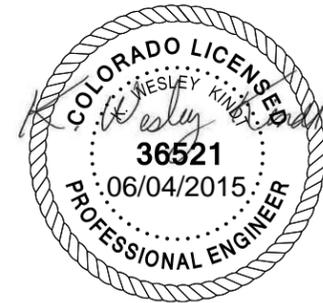
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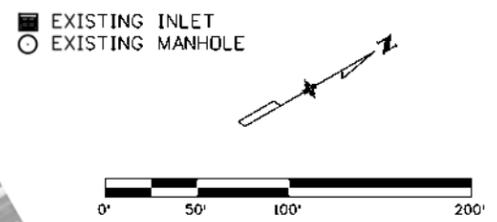
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| I-25 ML INITIAL WORKS HMA AND PCC PAVEMENT PLAN SHEET MP 212.78 - MP 213.04 | | | |
| Designer: | E. MARSH | Structure Numbers: | |
| Detailer: | T. HAUGH | Structure Numbers: | |
| Sheet Subset: | PLAN | Subset Sheets: | 3 of 5 |

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| Project No./Code | 2014-001 |
| Sheet: | ML-19 |



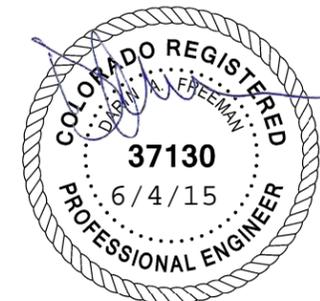
NOTE:
 1. ALL MANHOLES AND DRAINAGE INLETS WITHIN HMA MILL AND OVERLAY SECTION TO BE ADJUSTED UNLESS OTHERWISE NOTED.



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| Print Date: 5/11/2015 | | Sheet Revisions | | | Plenary Roads Denver, LLC 1700 Lincoln Street, Suite 3000 Denver, Colorado 80203 Phone: 303-803-9452 FAX: 303-803-9453 | | As Constructed | | I-25 ML INITIAL WORKS HMA AND PCC PAVEMENT | | Project No./Code | |
| File Name: 20062DES_I-25_HMA_Plan05 | | Date: | Comments | Init. | | | No Revisions: | | PLAN SHEET MP 213.31 - MP 213.50 | | 2014-001 | |
| Horiz. Scale: 1:100 Vert. Scale: As Noted | | | | | Revised: | | Designer: E. MARSH | Structure Numbers: | | | | |
| Unit: | | | | | Void: | | Detailer: T. HAUGH | Sheet Subset: PLAN | Subset Sheets: 5 of 5 | Sheet: ML-21 | | |
| 1670 BROADWAY, SUITE 3400 DENVER, COLORADO 80202 Phone: 303-764-1520 FAX: 303-860-7139 | | | | | | | | | | | | |

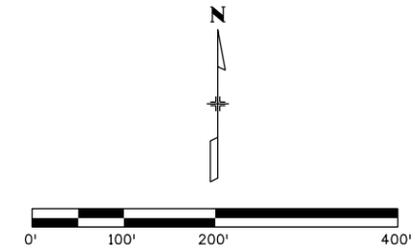
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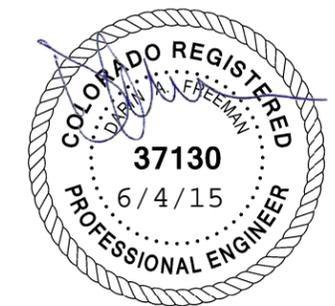
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| Print Date: 4/2/2015 | | Sheet Revisions | | | Plenary Roads Denver, LLC 1700 Lincoln Street, Suite 3000 Denver, Colorado 80203 Phone: 303-803-9452 FAX: 303-803-9453 | As Constructed | I-25 ML INITIAL WORKS - BRIDGE | | Project No./Code |
| File Name: 20062DES_I-25_HMA_AdAltSht | | Date: | Comments | Init. | | No Revisions: | 20TH STREET VIADUCT DECK MILL | | 2014-001 |
| Horiz. Scale: NTS Vert. Scale: As Noted | | | | | | Revised: | Designer: E. MARSH | Structure | |
| Unit: | | | | | | Void: | Detailer: T. HAUGH | Numbers | |
| 1670 BROADWAY, SUITE 3400 DENVER, COLORADO 80202 Phone: 303-764-1520 FAX: 303-860-7139 | | | | | Sheet Subset: TITLE | Subset Sheets: 1 of 4 | Sheet: BR1400 | | |

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LEGEND:

- LIMITS OF 2" SILICA FUME REMOVAL
- LIMITS OF 35' LONG HMA TRANSITION.



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| Print Date: 6/2/2015 |
| File Name: 20062BRDG_I-25_BR1401.dgn |
| Horiz. Scale: 1:200 Vert. Scale: As Noted |
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| I-25 ML INITIAL WORKS - BRIDGE 20TH ST. VIADUCT DECK MILL GENERAL PLAN (SHEET 1 OF 2) | | | |
| Designer: | D. FREEMAN | Structure Numbers | |
| Detailer: | V. LUERA | | |
| Sheet Subset: | BRIDGE | Subset Sheets: | 2 of 4 |

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| Project No./Code |
| 2014-001 |
| Sheet: BR1401 |

