

US 6/Wadsworth



Environmental
Assessment

FINAL

Determination of Effects to Historic Properties

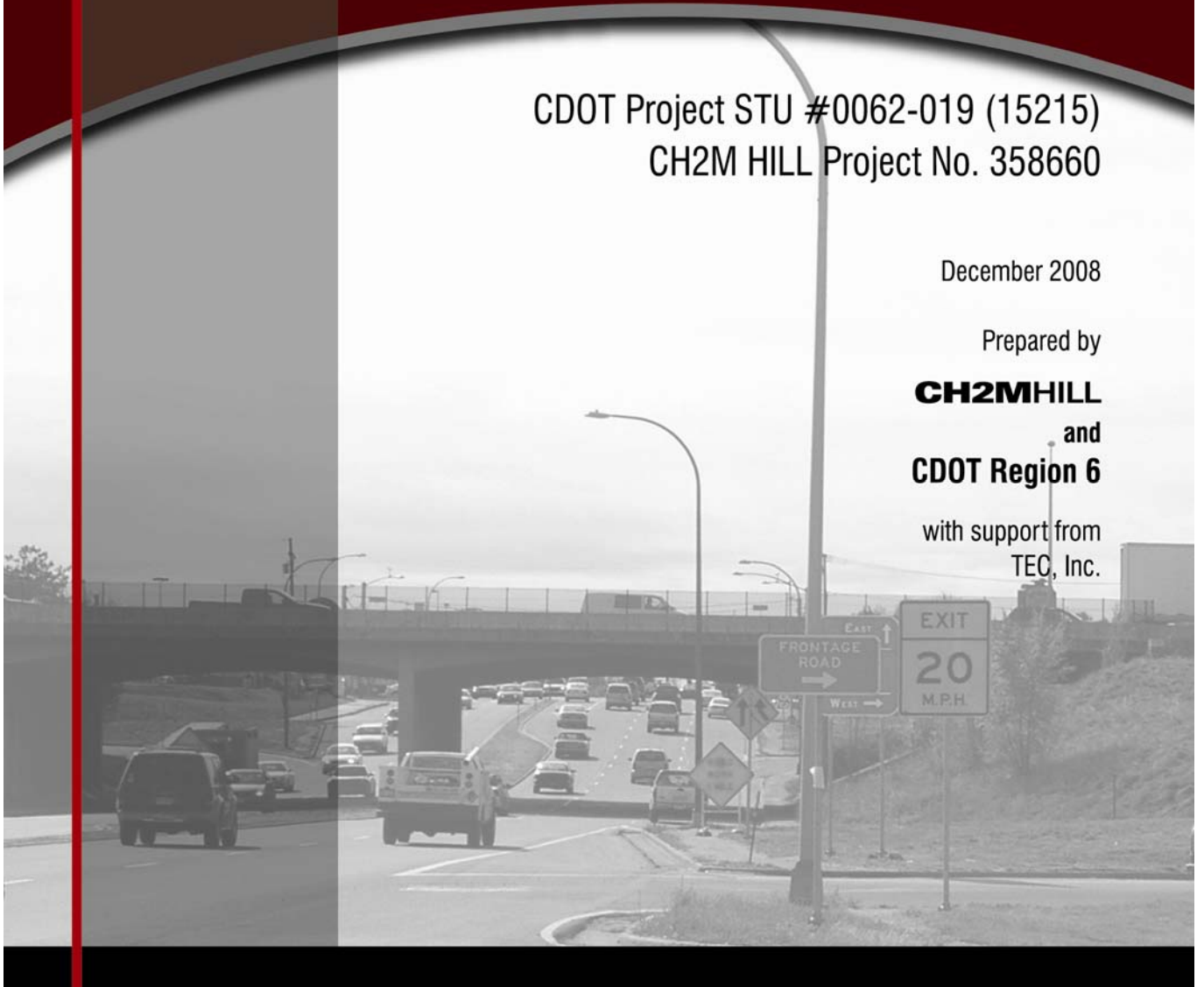
CDOT Project STU #0062-019 (15215)
CH2M HILL Project No. 358660

December 2008

Prepared by

CH2MHILL
and
CDOT Region 6

with support from
TEC, Inc.





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Acronyms and Abbreviations

APE	Area of Potential Effects
CDOT	Colorado Department of Transportation
CFR	Code of Federal Regulations
dB	decibels
dBA	decibel (A-weighted scale)
EA	Environmental Assessment
FHWA	Federal Highway Administration
HABS	Historic American Buildings Survey
MOA	Memorandum of Agreement
NHPA	National Historic Preservation Act
NRHP	National Register of Historic Places
ROW	right-of-way
SHPO	State Historic Preservation Office



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1.0 Introduction

The Colorado Department of Transportation (CDOT) proposes to reconstruct the interchange of US 6 and Wadsworth Boulevard and widen Wadsworth Boulevard between 4th and 14th Avenues in Jefferson County, Colorado. The purpose of the US 6 and Wadsworth Boulevard project is to improve traffic flow and safety for motorists, pedestrians, and bicyclists; accommodate high traffic volumes; and increase multi-modal travel options and connections at the US 6 and Wadsworth Boulevard interchange and along Wadsworth Boulevard between 4th Avenue and 14th Avenue. The existing design and configuration of the interchange and roadway within the project limits have not kept pace with traffic and multi-modal travel demands. CDOT, the Federal Highway Administration (FHWA), City of Lakewood, area residents, businesses, and commuters have prioritized making improvements to correct the transportation problems in the project area through previous planning efforts.

1.1 Project Study Area and Area of Potential Effects (APE)

The proposed US 6 and Wadsworth project is located in Lakewood, Colorado, and is defined as the area following the roadway corridors US 6 (also designated as 6th Avenue) and Wadsworth Boulevard (also designated as State Highway 121). The east-west limits along US 6 are from the eastern interchange ramps with Wadsworth Boulevard west to Garrison Street. On Wadsworth Boulevard, the project limits are 4th Avenue to 14th Avenue.

In accordance with the National Environmental Policy Act of 1969 and its implementing regulations, CDOT is preparing an Environmental Assessment (EA) to assess potential impacts to environmental resources within the project area. As part of the EA, and in compliance with Section 106 of the National Historic Preservation Act of 1966 (NHPA), CDOT conducted a Class III cultural resources inventory of the Area of Potential Effects (APE) for the proposed project (TEC, 2008) to identify historic properties within the APE. This report assesses effects to historic properties.

The APE for historic properties is illustrated in Exhibit 1. The APE generally includes the first row of properties fronting Wadsworth Boulevard and the US 6 frontage roads. Because the parcels are deep (mostly 300 feet), the first row encompasses properties that could be directly affected or indirectly affected by noise or visual changes. In areas where parcels are less deep, the first two rows of properties were included in the APE.

The entire Jefferson County Open School property between 10th Avenue and 12th Avenue at the west side of Wadsworth Boulevard was included in the APE in the interest of adding the property to the historical public record.

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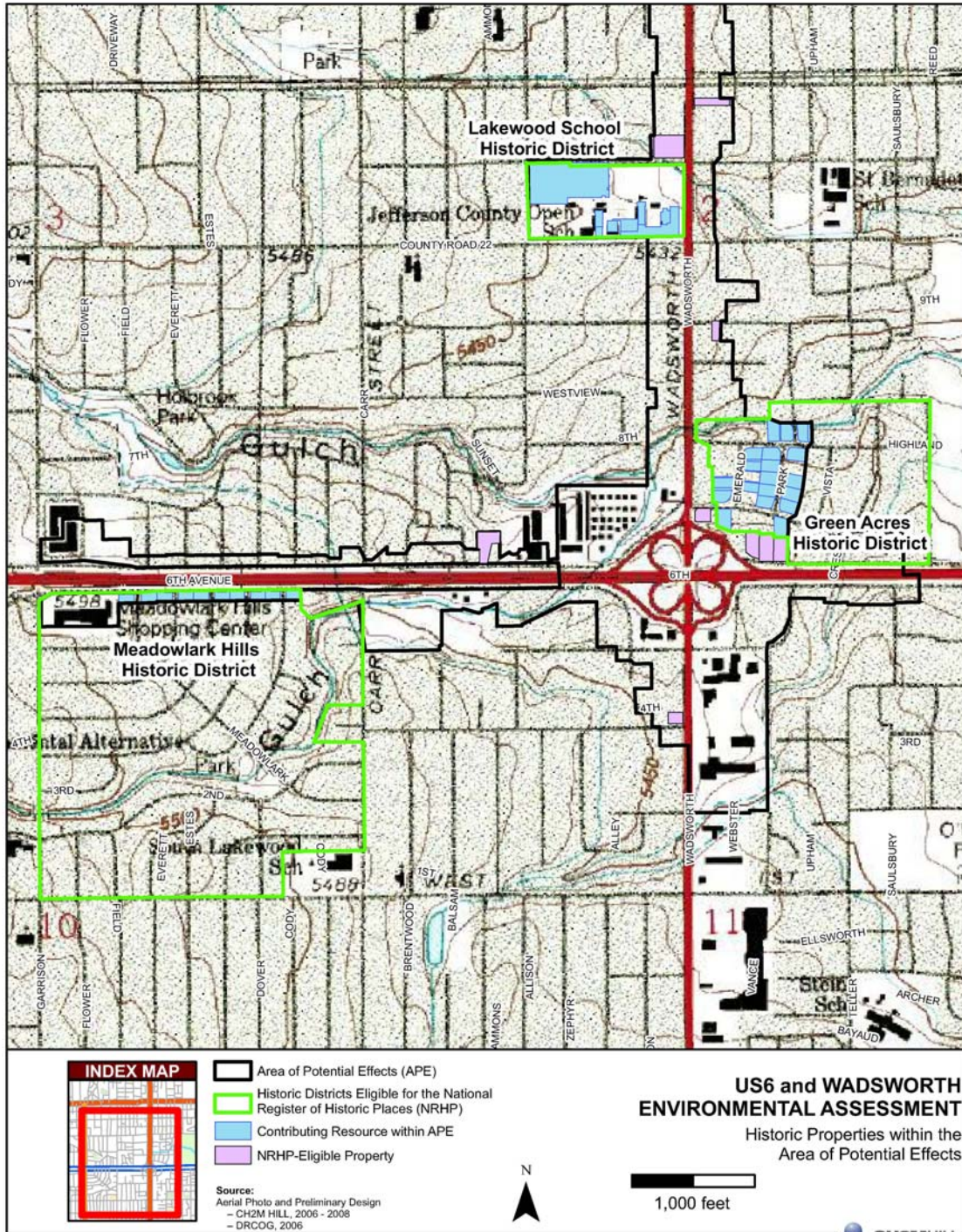


EXHIBIT 1
 Historic Properties within Area of Potential Effects

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1.2 Historic Properties within the APE

The cultural resources inventory of the APE (TEC, 2008) assessed eligibility for 151 historic resources within the APE. Of the 151 properties surveyed, nine architectural resources are individually eligible for nomination to the National Register of Historic Places (NRHP) and 39 architectural resources are contributing elements to historic districts. Three NRHP-eligible historic districts have been identified within or partially within the APE.

- Lakewood School Historic District was completely surveyed: seven buildings and two landscape features within this historic district were determined to be contributing resources, and 10 buildings and five landscape features were determined to be noncontributing to the historic significance of the historic district.
- Green Acres Historic District includes 107 buildings within its recommended boundary, of which 30 are within the APE and were surveyed. Twenty of those buildings were determined to be contributing to the Green Acres Historic District and 10 were considered to be noncontributing elements to the historic district.
- Meadowlark Hills Historic District consists of 413 buildings within its boundaries, 11 of which are included in the APE. Of those, 10 residential buildings were determined to be contributing resources and one commercial building was determined to be a noncontributing element to the Meadowlark Hills Historic District.

Exhibit 1 shows the location of NRHP-eligible or contributing resources within the APE. The Colorado State Historic Preservation Office (SHPO) has concurred with these eligibility determinations.

Two resources were recommended as “needs data” because full access to these properties was not available during the survey and these properties were not affected by the Proposed Action. If the Proposed Action were to change so that these properties were potentially affected, an access request will be prepared to fully evaluate these properties for their eligibility to the NRHP. None of the linear resource segments within the APE or the interchange overpass at US 6 and Wadsworth Boulevard is a historic resource. All other surveyed resources were recommended as not eligible for the NRHP. The Colorado SHPO has also concurred with these eligibility determinations.

1.3 Criteria for Effects to Historic Properties

Section 106 of the NHPA and implementation regulations require that federal agencies, such as FHWA, or state or other agencies that received federal assistance, such as CDOT, take into account the effects a proposed undertaking may have on historic properties. NHPA’s Section 106 regulations, Title 36 of the Code of Federal Regulations (CFR) Part 800.5 (36 CFR 800.5) (“Protection of Historic Properties”), include specific criteria for adverse effects that must be applied to federal undertakings with the potential to impact historic properties.

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When considering the potential for adverse effects, all reasonable, foreseeable impacts must be taken into account, including direct, indirect, and cumulative impacts.

An adverse effect is found when an undertaking may alter, directly or indirectly, any characteristic of a historic property that qualifies the property for inclusion in the NRHP. This includes diminishing the integrity of the property's location, design, setting, materials, workmanship, feeling, or association. Examples of adverse effects to historic properties outlined in 36 CFR 800.5 include, but are not limited to:

- i. Physical destruction of, or damage to, all or part of the property;
- ii. Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous materials remediation, and provision of handicapped access, that is not consistent with the Secretary of Interior's standards for the treatment of historic properties (36 CFR 68) and applicable guidelines;
- iii. Removal of the property from its historic location;
- iv. Change of the character of the property's use or of physical features within the property's setting that contribute to its historic significance;
- v. Introduction of visual, atmospheric, or audible elements that diminish the integrity of the property's significant historic features;
- vi. Neglect of a property which causes deterioration, except where such neglect and deterioration are recognized qualities of a property of religious and cultural significance to a [Native American] or native Hawaiian organization; and
- vii. Transfer, lease, or sale of property out of federal ownership or control without adequate and legally enforceable restrictions or conditions to ensure long-term preservation of the property's historic significance.

Effects to cultural resources are categorized as No Historic Properties Affected, No Adverse Effect, and Adverse Effect (36 CFR 800.5), as follows:

- **No Historic Properties Affected:** Either no historic properties are present, or there are historic properties present, but there is no effect of any kind, harmful or beneficial, on the historic properties.
- **No Adverse Effect:** There is an effect, but the effect is not harmful to those characteristics that qualify the property for inclusion in the NRHP.
- **Adverse Effect:** There is an effect, and that effect diminishes the qualities of significance that qualify the property for inclusion in the NRHP.

Effects to historic properties may be direct or indirect. Direct effects include, but may not be limited to, the physical destruction or modification of all or part of a resource. Indirect effects

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can include a variety of factors, such as the introduction of, or an increase in, noise and visual intrusions that alter the qualities that make a site eligible to the NRHP. Most effects to historic properties are permanent, but there may be only short-term, temporary effects related to the project construction process. Adverse effects may include reasonably foreseeable effects caused by the undertaking that may occur later in time, be farther removed in distance, or cumulative.

1.4 Consultation with the Colorado State Historic Preservation Office and Other Interested Parties

In the fall of 2007, CDOT initiated consultation with the Colorado SHPO and other interested parties to discuss the APE, survey methodology, and gather information on specific concerns. In addition to the Colorado SHPO, the invited participants were the Jefferson County Historical Commission, Lakewood Historical Society, Lakewood Heritage Center, and Colorado Preservation, Inc. Of these, the Jefferson County Historical Commission and Lakewood Historical Society indicated interest in participating in the evaluation of historic properties for this project. The Lakewood Heritage Center and Colorado Preservation, Inc. did not indicate an interest in participating as consulting parties for this project. All correspondence related to Section 106 consultations is included in the Appendix, Section 106 Correspondence.

In August 2007, the SHPO was provided background information on the project, invited to participate in scoping meetings, and asked to provide comments on the scope for the EA. The SHPO did not participate in scoping meetings but did provide a written letter noting the requirements of the Section 106 process. A meeting was held with the SHPO Section 106 coordinator in November 2007. During that meeting, she provided comments on the APE, survey methodology, and potential historic properties and districts in the project area. She specifically asked the team to assess the potential that post-World War II subdivisions in the area had historical significance and integrity. Based on field survey of the APE (TEC, 2008), CDOT submitted eligibility determinations for historic properties within the APE to SHPO in July 2008. The SHPO concurred with most of the determinations and requested additional information about several resources. After CDOT provided additional information, the SHPO concurred with all eligibility determinations in October 2008. In December 2008, CDOT submitted this report assessing effects to historic properties to SHPO. In a letter dated December 21, 2008, the SHPO concurred with all of the effect determinations outlined in this report.

The Jefferson County Historical Commission did not provide any specific comments about the survey, information about historic properties within the APE, or effects to historic properties documented in this report.

The Lakewood Historical Society provided specific information or questions about potential historic properties within the APE. The Lakewood Historical Society recommended historical research for the building at 1296 Wadsworth Blvd. and the mobile diner located behind 785 Wadsworth Blvd. as possible historic resources. The *Historic Resources Survey: US 6 and*



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Wadsworth Boulevard, Lakewood, Colorado (TEC, 2008) evaluated these properties and concluded that neither is eligible for the NRHP. The Lakewood Historical Society recommended the Lakewood School World War II Memorial and the gas station at 401 Wadsworth Blvd. also be evaluated. Both resources were determined to be eligible for the NRHP (TEC, 2008) as part of this project. The World War II Memorial was determined to be a contributing feature in the NRHP-eligible historic district, and 401 Wadsworth Blvd. was determined individually eligible for the NRHP. The Lakewood Historical Society did not provide formal comments on either the eligibility or effect determinations but did provide verbal acknowledgement of the findings.



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2.0 Proposed Action and Alternatives

The Proposed Action includes changes to Wadsworth Boulevard between 4th and 14th Avenues and to the Wadsworth Boulevard interchange with US 6. The Proposed Action would widen Wadsworth between 4th and 14th Avenues and would replace the existing US 6/Wadsworth interchange with a new diamond interchange and a new loop ramp in the northwest quadrant of the interchange. These changes would improve traffic flow and safety for motorists, pedestrians, and bicyclists; accommodate high traffic volumes; and increase multi-modal travel options and connections at the interchange and on Wadsworth.

2.1 Wadsworth Boulevard Proposed Action

The Proposed Action would widen Wadsworth Boulevard from four lanes with two center turn lanes and limited sidewalks to six travel lanes, a raised median, and detached sidewalks. The extra travel lane in each direction would provide adequate capacity to meet both current and projected future traffic volumes. The median would prevent left turns at mid-block locations and would allow left turns only at intersections with cross streets, thereby controlling access and improving safety on Wadsworth. Detached multi-use sidewalks would provide a continuous facility for pedestrians and bicyclists, improving safety and connections between different modes of transportation.

The existing roadway width varies along the Wadsworth corridor, ranging from 80 to 95 feet. The Proposed Action would have a roadway width (curb to curb) between 91 and 122 feet. Additional width would be required for sidewalks along both sides of Wadsworth. This widening has the potential to directly affect historic properties located along Wadsworth Boulevard between 4th and 14th Avenues due to right-of-way acquisition or encroachment of the roadway (reducing the distance between historic buildings and surrounding roadways). Historic properties could also be indirectly affected by changes in noise or visual characteristics or changes to accesses or traffic patterns that affect the integrity of historic settings. Exhibit 2 displays the locations of historic properties along Wadsworth in proximity to proposed roadway improvements. This exhibit is provided as reference to the detailed discussion of impacts to these properties that follows in Section 3.0.



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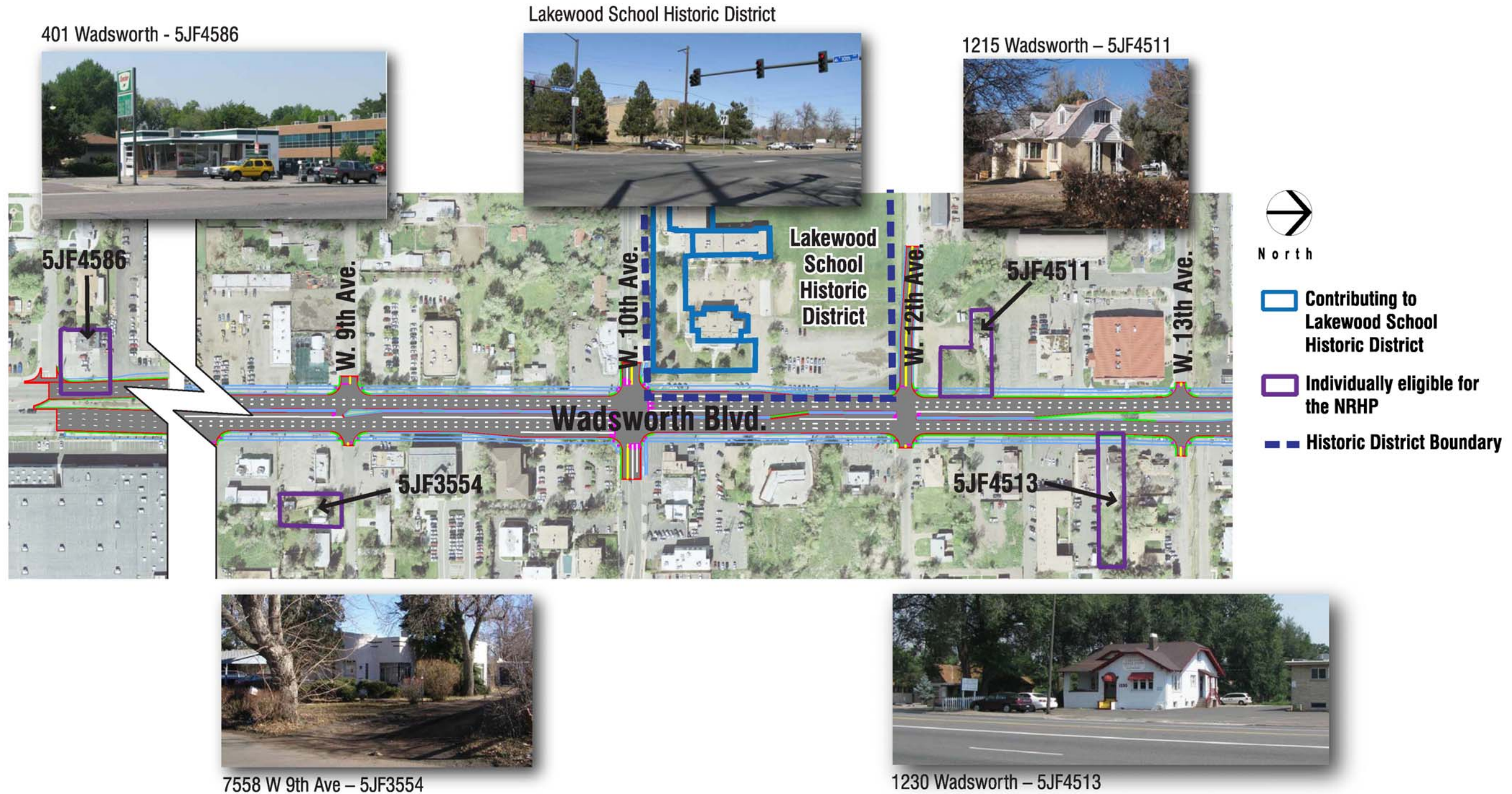


EXHIBIT 2
Historic Properties along Wadsworth Boulevard

2.2 US 6/Wadsworth Interchange Proposed Action

The Proposed Action would replace the current interchange between US 6 and Wadsworth Boulevard with a new diamond interchange and a new loop ramp in the northwest quadrant of the interchange. This type of interchange is called a Tight Diamond with Loop.

The loop ramp would accommodate peak evening traffic moving from westbound US 6 to southbound Wadsworth. The loop would be reconstructed with a larger radius to accommodate a 25-mph design speed for the off-ramp. This configuration would allow vehicles to maintain a higher speed while exiting US 6, reducing the amount of deceleration required on US 6 in high-speed traffic. In addition, maintaining the higher speed on the loop allows an easier merge with Wadsworth Boulevard traffic because the difference in vehicle speeds is reduced, improving safety. The remaining ramps would be constructed in a diamond configuration to minimize impacts to properties surrounding the interchange.

All of the entrance and exit ramps in the interchange would be lengthened to provide safer acceleration and deceleration distances for cars merging on and off US 6. The longer ramps and expanded loop in the northwest quadrant of the interchange would require more right-of-way and result in a larger footprint than the existing cloverleaf interchange. Historic properties located in proximity to these ramps have the potential to be directly affected by right-of-way acquisition or encroachment of the roadway (reducing the distance between historic buildings and surrounding roadways). Historic properties could also be indirectly affected by changes in noise, visual characteristics, or changes to accesses or traffic patterns that affect the integrity of historic settings.

Of the 49 eligible or contributing resources, there are seven NRHP-eligible historic properties adjacent to roadway improvements in the northeast quadrant of the interchange. These include four individually eligible buildings and three contributing features of the Green Acres Historic District. Exhibit 3 displays the locations of these historic properties in proximity to proposed roadway improvements. This exhibit is provided as reference to the detailed discussion of impacts to these properties and efforts of the project team to avoid or minimize those impacts that follows in Section 3.0.

Impacts to some of these properties were unavoidable. The Proposed Action is the interchange design option that best minimizes effects to historic properties. The project team made additional refinements to the Proposed Action interchange design to further minimize effects to historic properties. Because of the proximity of the historic properties to proposed roadway improvements, the needs to address safety issues due to the joined frontage road and off-ramp and minimize neighborhood cut-through traffic, and the density of historic properties in this location, CDOT had no other option that met safety, traffic, and community needs without impacting historic properties. The following sections describe the numerous options that CDOT considered to minimize effects to historic properties in the interchange area and explain the reasons why these options were or were not incorporated into the Proposed Action



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Properties Contributing to Green Acres Historic District

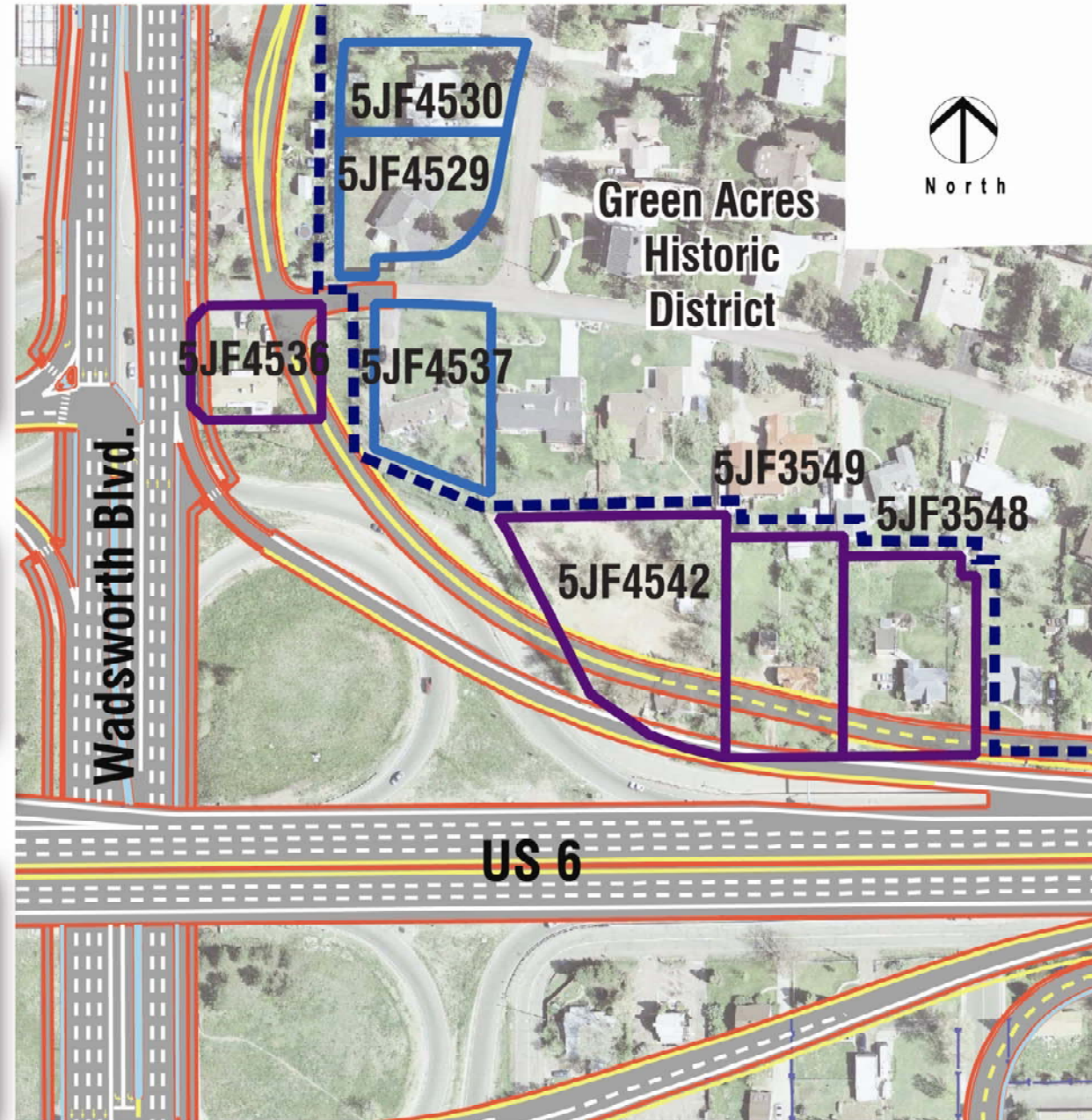
715 Emerald Lane - 5JF4530



705 Emerald Lane - 5JF4529



7500 Broadview Drive - 5JF4537



Properties Individually Eligible for the NHRP

700 Wadsworth Boulevard - 5JF4536



7433 W 6th Ave - 5JF4542



7423 W 6th Ave - 5JF3549



7395 W 6th Ave - 5JF3548



■ Historic District Boundary

EXHIBIT 3
Historic Properties Immediately Adjacent to the Northeast Quadrant of the Interchange

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2.2.1 Efforts to Minimize Effects of the Proposed Action on Historic Properties in the Northeast Quadrant of the Interchange

Exhibit 4 illustrates the interchange design elements that were altered to minimize impacts to historic properties to the greatest extent possible, while still meeting design standards and capacity requirements. The location and width of the following design elements were considered carefully to minimize property impacts:

- Location of the gore area for the westbound US 6 off-ramp;
- Taper of the off-ramp;
- Frontage road proximity to the off-ramp; and
- Length of the deceleration lane for the loop ramp.

The interaction of these elements with each other was also considered. The design team also considered removing the acceleration lane on Wadsworth associated with the northeast off-ramp, but this would not be feasible given the traffic volumes on US 6 and Wadsworth. The resulting design of the Proposed Action represents the minimal effect to historic properties that was possible while still maintaining a design that meets minimum standards.



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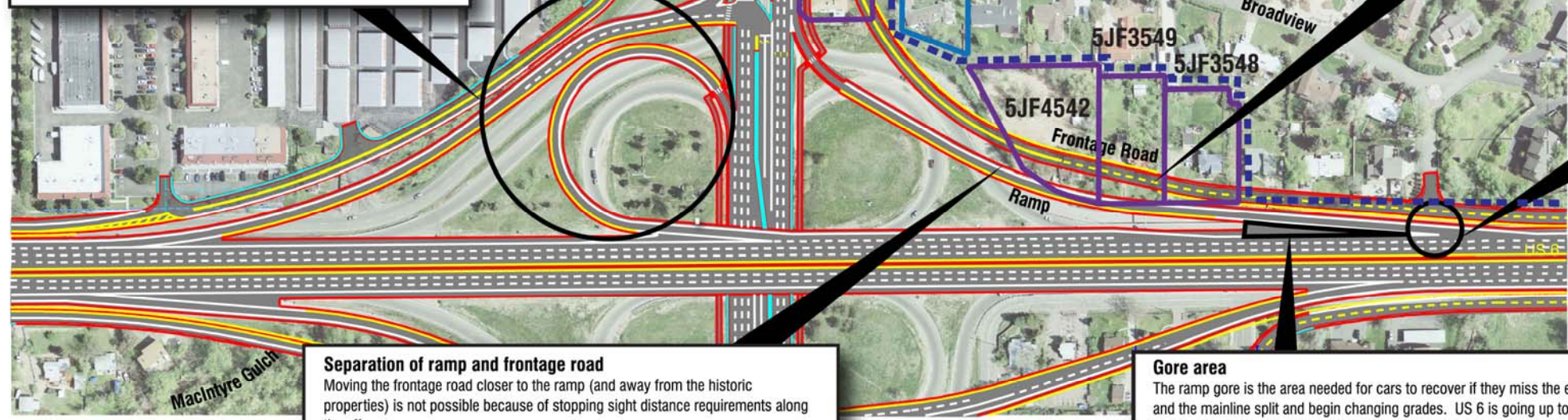
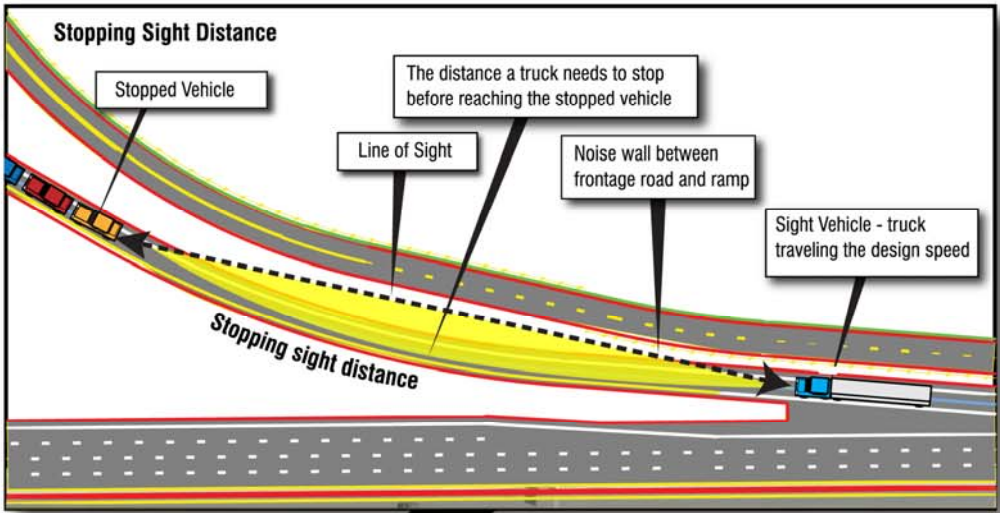
Fourth lane add for Wadsworth between off ramp and Highland Drive
 The fourth northbound lane on Wadsworth is needed to receive the dedicated right turn lane from the westbound to northbound exit ramp to ensure safety and avoid traffic operation issues on US 6 and Wadsworth.

Without a fourth northbound receiving lane:

- A single lane yield would create a queue on the ramp that extends to US 6 mainline. It would increase the probability of rear end collisions because of reduced stopping sight distance. Vehicles traveling at high speed on US 6 would have to stop in the through lanes.
- A signalized two-lane right turn would solve the queuing to US 6 and rear end collision problems but would adversely affect Wadsworth traffic, increasing congestion along Wadsworth as northbound through traffic would stop at an additional signal.

Radius of Loop Ramp
 Increasing the design speed of the loop ramp to 30 mph would reduce the required deceleration length on US 6 from 550 to 520 feet allowing the gore nose to move closer to Wadsworth to reduce impacts to historic properties. (However, the gore nose is controlled more by the grade issues than by the deceleration length of the loop ramp.)

- Radius of loop would increase from 150 to 231 feet
- Loop ramp is designed at 25 mph; 30 mph was desirable but resulted in significant property impacts in the northwest quadrant, including relocation of a large public storage area and office park, and McIntyre and Lakewood gulches. This resulted in significant impacts to waters and wetlands regulated by the Clean Water Act.



Taper for off-ramp
 The length of the taper is controlled by deceleration length requirements and alignment constraints.

- Deceleration length is based on the mainline US 6 design speed of 70 mph to ramp design speed of 50 mph.
- The angle of deflection and location of taper is based on horizontal geometric constraints created by US 6 increasing in elevation to go over Wadsworth. Because of the need to elevate US 6 bridge over Wadsworth, moving the Wadsworth offramp departure west, shortens the distance for the ramp to meet Wadsworth grade and results in either too steep a grade or extending the ramp into northbound Wadsworth traffic lanes (essentially eliminating any fourth add lane).

Separation of ramp and frontage road
 Moving the frontage road closer to the ramp (and away from the historic properties) is not possible because of stopping sight distance requirements along the off-ramp.

- If the frontage road were closer to the ramp, the roadway would create an obstruction blocking the line of sight for drivers exiting US 6 (see stopping sight distance illustration).
- Ramp could be moved closer to US 6 at the west end but not enough to save the historic properties, the gore nose location still controls the eastern end of the ramp (where 5JF 3548 is located).

Gore area
 The ramp gore is the area needed for cars to recover if they miss the exit. The end of the gore, or gore nose, is the point where the ramp and the mainline split and begin changing grades. US 6 is going up to get over Wadsworth, and the ramp is going down to meet Wadsworth.

- The off-ramp has been designed to minimize impacts to 5JF3548. Moving the gore west would raise its elevation and require a longer ramp to get back down to Wadsworth and move the merging intersection with Wadsworth north, decreasing the merge distance of the fourth lane.
- Moving the gore nose to the west also would impact the deceleration length for the westbound US 6 to southbound 25 mph Wadsworth loop ramp. Shortening the deceleration length would be possible if the loop was a higher design speed but the higher speed requires a larger loop creating significant additional property impacts (see Radius of Ramp Loop discussion).

EXHIBIT 4
 Design Features of the Tight Diamond with Loop Interchange

2.3 Interchange Concepts and Design Options Considered but Dismissed

Other interchange concepts were considered during the alternatives development and screening process; the concepts' impacts to historic properties are described below. After the Proposed Action was selected, additional design options were considered in an attempt to avoid or minimize impacts to historic resources in the vicinity of the interchange; those options are also described below.

2.3.1 Other Interchange Concepts

Eight interchange design concepts were studied during the Level 1 screening process. Four of these concepts were screened out because they did not meet the purpose and need for the project. CDOT evaluated four remaining interchange design options that met the safety and mobility needs of the project: Tight Diamond, Tight Diamond with Loop (preferred), Single Point Urban Interchange, and Partial Cloverleaf. These four interchange designs were evaluated in detail before selection of a preferred alternative, and effects to historic properties were key considerations in the evaluation of the preferred alternative.

Each of the four interchange design concepts would directly impact five historic resources northeast of the interchange. The impacts to the historic properties would result from two primary features shared by all of the interchange options: the need for a longer deceleration lane for the westbound off-ramp on US 6 and the need for an improved frontage road connection to Wadsworth in the northeast quadrant of the interchange.

The longer deceleration lane required for the westbound US 6 off-ramp would widen US 6 to the north on the east side of Wadsworth Boulevard. Widening US 6 in this location would require the existing frontage road to also shift farther north. The three historic properties currently located on the frontage road in this area would be affected by each of the four options, due to the requirements for the off-ramp design. Two other historic properties near the frontage road and Wadsworth would be affected due to the location of the intersection of the frontage road and Wadsworth Boulevard.

After detailed evaluation of the four interchange concepts, the Tight Diamond with Loop was determined to best balance transportation needs with environmental and community impacts and was selected as the preferred alternative. After the Tight Diamond with Loop concept was chosen, further refinement of the design specific to the northeast quadrant of the interchange was considered in an attempt to avoid or minimize impacts to the historic properties in this location.

2.3.2 Other Design Considerations to Avoid or Minimize Impacts to Historic Resources

The design team looked at creative solutions that could be implemented with the Tight Diamond with Loop alternative to avoid or minimize impacts to historic properties in the northeast quadrant of the interchange. During a brainstorming session, four additional design options were developed, two of which could avoid five historic properties, and two of which could avoid two historic properties. Although the options avoid historic properties, further analysis revealed that each of the options has significant shortcomings and doesn't satisfy the purpose and need for the project for one or more of the following reasons: they increase congestion; do not reduce congestion; are not safe; do not meet driver expectations; increase neighborhood cut through traffic; or do not meet design standards. These design options are illustrated in Exhibits 5 and 6.

Option 1 - Merge Frontage Road with Off-Ramp could successfully avoid impacts to two of the five historic properties. However, several existing issues would not be addressed. Cut through traffic in the neighborhood would remain. The frontage road would remain tied into the off-ramp, mixing two different types and speeds of traffic flow and perpetuating an unsafe condition. Finally, the off-ramp would extend farther east than in the Proposed Action to allow US 6 traffic to decelerate before reaching the frontage road merge, increasing impacts east of the interchange.

Option 2 - Folded Diamond Interchange could successfully avoid impacts to all five historic properties. However, the resulting impacts to properties northwest of the interchange and to the US 6 bridge over Wadsworth would substantially increase project cost; queuing traffic on the loop would impact traffic on US 6; and traffic operations on Wadsworth Boulevard within the interchange area would break down.

Option 3 - Single Intersection Frontage Road and Off-Ramp could successfully avoid impacts to two of the five historic properties, but would create safety concerns at the intersection of the frontage road/off-ramp with Wadsworth Boulevard and would cause a break down in traffic operations on Wadsworth Boulevard within the interchange area.

Option 4 - Folded Diamond Interchange with Frontage Road through Neighborhood could successfully avoid impacts to all five historic properties. However, the impacts discussed for Option 2 would apply. Additionally, routing the frontage road through the neighborhood would increase traffic through the historic Green Acres neighborhood. Reducing neighborhood cut-through traffic is an identified and community supported need for the project.

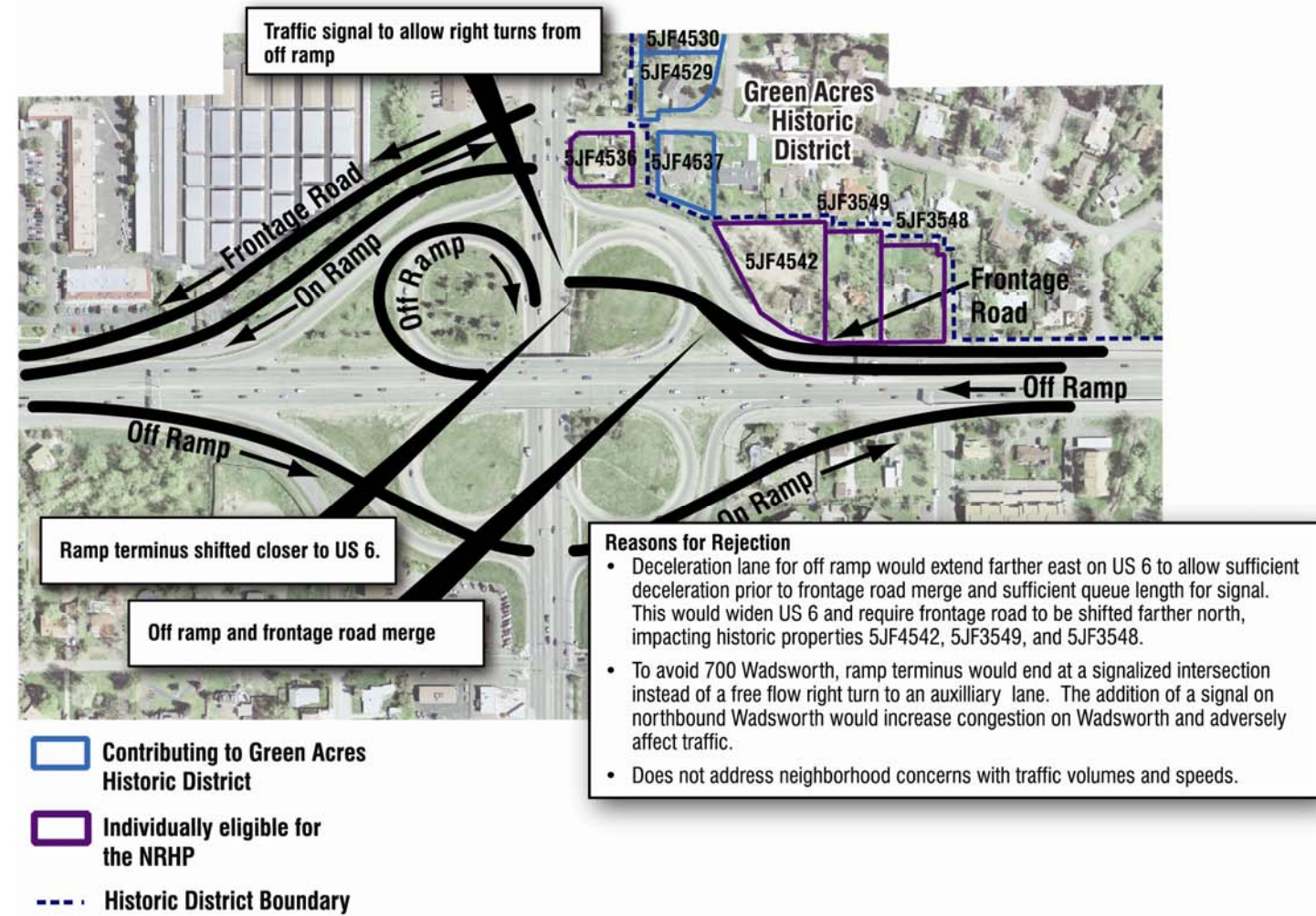
After considering these avoidance options, the project team determined that the negative impacts to safety, traffic operations, project cost, neighborhoods, and other properties made these options infeasible.

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Avoidance Option 1 - Merge Frontage Road with Off Ramp

Would merge frontage road with off ramp and would shift terminus of off ramp closer to US 6 to avoid historic properties 5JF4536 and 5JF4537. Would not avoid historic properties 5JF4542, 5JF3549, or 5JF3548.



Avoidance Option 2 - Folded Diamond Interchange

Would exit all traffic from westbound US 6 onto loop ramp, removing need for off ramp northeast of interchange. Would thereby allow frontage road to remain in existing location, avoiding impacts to historic properties 5JF4542, 5JF3549, and 5JF3548. Frontage road would intersect Wadsworth at same location as loop ramp terminus, close to US 6, avoiding impacts to historic properties 5JF4536 and 5JF4537.

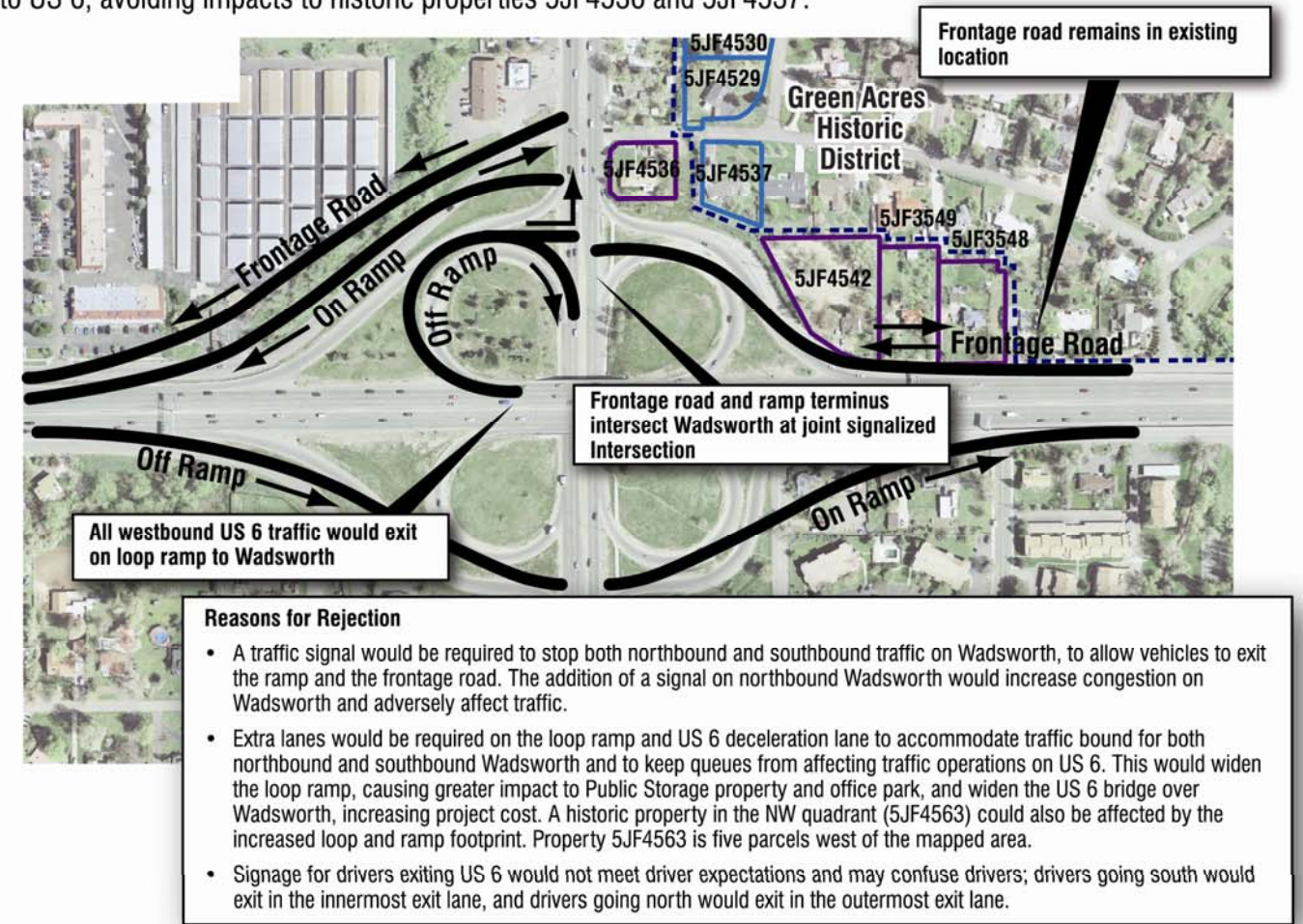


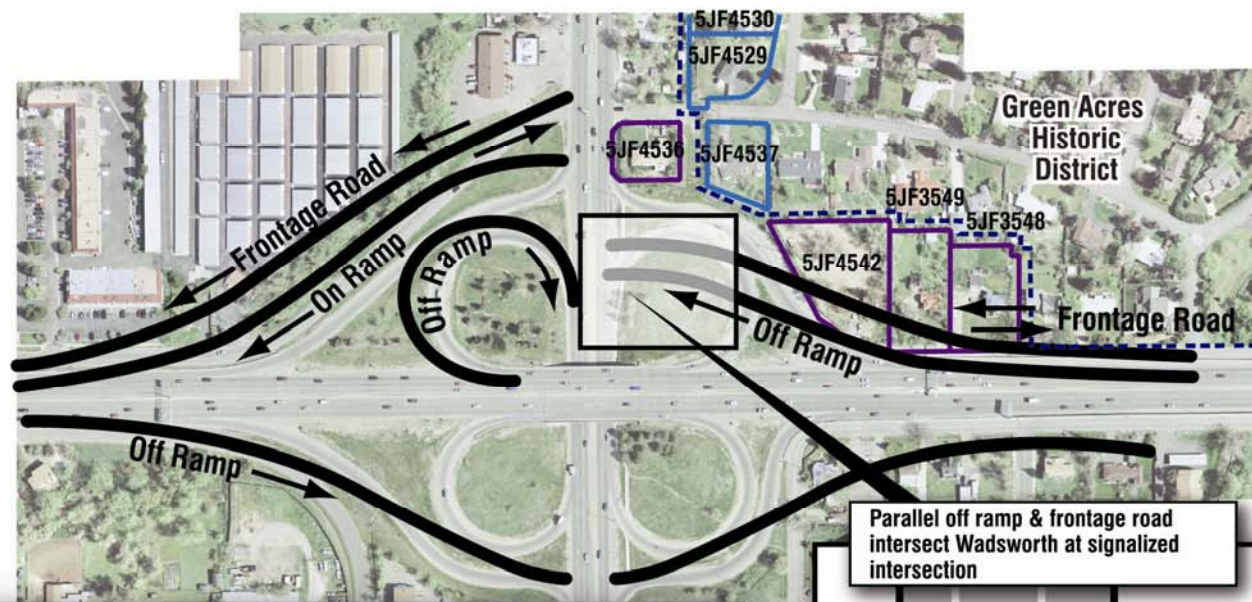
EXHIBIT 5
Rejected Avoidance Options 1 and 2

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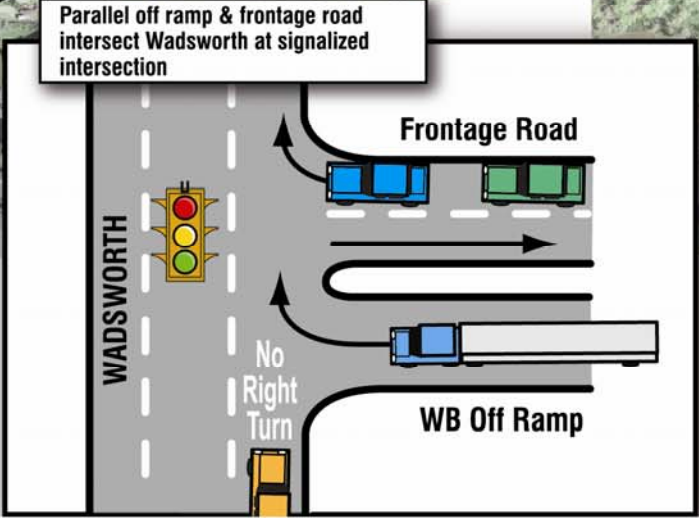
Avoidance Option 3 - Single Intersection Frontage Road and Off Ramp

Would place frontage road and off ramp parallel to each other and would provide signalized intersection for both at Wadsworth. Would shift intersection closer to US 6 to avoid historic properties 5JF4536 and 5JF4537. Would not avoid historic properties 5JF4542, 5JF3549, or 5JF3548.



Reasons for Rejection

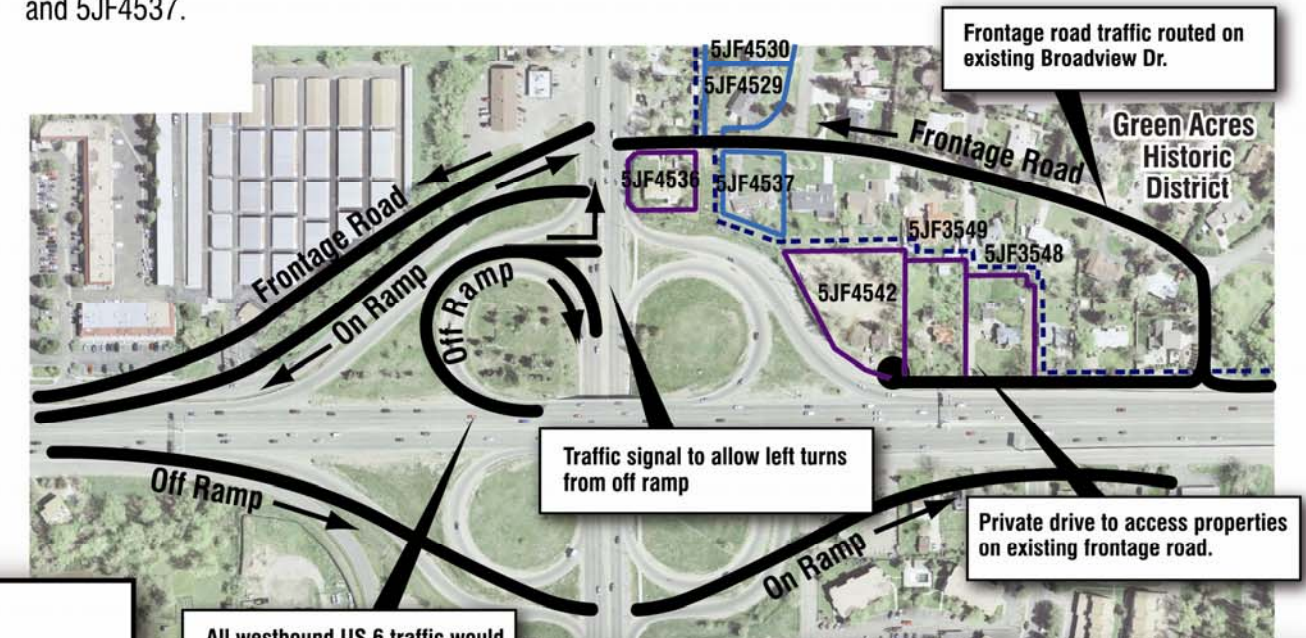
- A traffic signal would be required to stop northbound traffic on Wadsworth, to allow vehicles to exit the ramp and the frontage road. The addition of a signal on northbound Wadsworth would increase congestion on Wadsworth and adversely affect traffic. Signal operations would be difficult because of the side-by-side right turns from the ramp and frontage road to Wadsworth.
- The intersection would present significant safety concerns. Vehicles bound for the eastbound frontage road could accidentally turn right onto the one-way off ramp, creating the potential for head-on collisions. This type of intersection also does not meet driver expectations and could cause confusion.
- Deceleration lane for off ramp would need to extend farther east on US 6 than it does currently, to allow sufficient deceleration prior to stopping at ramp terminus, and to keep queues from affecting traffic operations on US 6. This would widen US 6 and require frontage road to be shifted farther north, impacting historic properties 5JF4542, 5JF3549, and 5JF3548.



- Contributing to Green Acres Historic District
- Individually eligible for the NRHP
- Historic District Boundary

Avoidance Option 4 - Folded Diamond Interchange with Frontage Road through Neighborhood

Would exit all traffic from westbound US 6 onto loop ramp, removing need for off ramp northeast of interchange. Would thereby allow frontage road to remain in existing location, avoiding impacts to historic properties 5JF4542, 5JF3549, and 5JF3548. Would change the frontage road to a private drive and route frontage road traffic on existing Broadview Drive, avoiding impacts to historic properties 5JF4536 and 5JF4537.



All westbound US 6 traffic would exit on loop ramp to Wadsworth

Reasons for Rejection

- A traffic signal would be required to stop both northbound and southbound traffic on Wadsworth, to allow vehicles to exit the ramp to northbound Wadsworth. The addition of a signal on northbound Wadsworth would increase congestion on Wadsworth and adversely affect traffic.
- Extra lanes would be required on the loop ramp and US 6 deceleration lane to accommodate traffic bound for both northbound and southbound Wadsworth and to keep queues from affecting traffic operations on US 6. This would widen the loop ramp, causing greater impact to Public Storage property and office park, and widen the US 6 bridge over Wadsworth, increasing project cost. A historic property in the NW quadrant (5JF4563) could also be affected by the increased loop and ramp footprint. Property 5JF4563 is five parcels west of the mapped area.
- Signage for drivers exiting US 6 would not meet driver expectations and may confuse drivers and cause accidents; drivers going south would exit in the innermost exit lane, and drivers going north would exit in the outermost exit lane.
- Would increase traffic through historic Green Acres neighborhood. Reducing neighborhood cut-through traffic is an identified and community supported need.

EXHIBIT 6
Rejected Avoidance Options 3 and 4

3.0 Effects to Historic Properties

3.1 Impacts of the No Action Alternative to Historic Properties

Under the No Action Alternative, the US 6/Wadsworth Boulevard interchange would remain in its current configuration and Wadsworth Boulevard would not be widened. Because the interchange would not be reconstructed and Wadsworth Boulevard would not be widened, there would be no direct effect to historic properties in the APE. The residential setting of historic properties in the vicinity of the interchange would continue to be indirectly affected by the noise and dust emissions generated in proximity of the major highways. Although noise walls would continue to be present east of Wadsworth Boulevard, these walls have a negligible visual effect on the historic setting of properties in this area, primarily because the walls are located next to the highway (between the frontage road and the highway) and away from the residences. The walls also serve to reduce traffic noise and, therefore, represent a beneficial impact to the residential settings of these properties adjacent to the US 6 frontage roads. No sound walls would be provided west of Wadsworth Boulevard along US 6, and the beneficial effects to the residential character of historic properties located in these neighborhoods west of US 6, such as the Meadowlark Hills subdivision, would not be realized.

3.2 Impacts of the Proposed Action to Historic Properties

The Proposed Action includes changes to Wadsworth Boulevard between 4th and 14th Avenues and to the Wadsworth Boulevard interchange with US 6 as described in Section 2.0.

Exhibits 7 through 10 summarize the effects to NRHP-eligible historic properties within the APE. Exhibit 7 provides a list of historic resources within the APE that are individually eligible for listing in the NRHP. Exhibit 8 provides a list of the historic resources located within the NRHP-eligible Lakewood School Historic District. Exhibits 9 and 10, meanwhile, provide lists of the historic resources located within the APE in the NRHP-eligible Green Acres and Meadowlark Hills Historic Districts, respectively.



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EXHIBIT 7
Effects to Individually Eligible Historic Resources in the APE

Site No.	Address	Description	Date	NRHP Eligibility	Proposed Action Impact	Permanent Effect Finding	Criteria of Adverse Effect
5JF3548	7395 W. 6th Ave. Frontage Rd.	English Norman Cottage single-family residence	1946	Officially Eligible; Criterion C	Demolition of structure (full acquisition)	Adverse Effect	i. Physical destruction of property
5JF3549	7423 W. 6th Ave. Frontage Rd.	Mediterranean Revival single-family residence	1939	Officially Eligible; Criterion C	Demolition of structure (full acquisition)	Adverse Effect	i. Physical destruction of property
5JF3554	7558 W. 9th Ave.	Art Deco single-family residence	1939	Officially Eligible; Criterion C	No direct or indirect impact (no change to setting)	No Historic Properties Affected	n/a
5JF4511	1215 Wadsworth Blvd.	Dutch Colonial Revival single-family residence	1918, 1948-1949	Officially Eligible; Criterion A	Partial acquisition of historic property frontage	No Adverse Effect	n/a
5JF4513	1230 Wadsworth Blvd.	Craftsman Bungalow residence converted into a business	1928	Officially Eligible; Criterion C	Acquisition of portion of property that does not contribute to historic significance	No Adverse Effect	n/a
5JF4536	700 Wadsworth Blvd.	Ranch residence converted into a business	1947	Officially Eligible; Criterion C	Demolition of structure (full acquisition)	Adverse Effect	i. Physical destruction of property
5JF4542	7433 W. 6th Ave. Frontage Rd.	Minimal Traditional single-family residence	1940	Officially Eligible; Criterion C	Demolition of structure (full acquisition)	Adverse Effect	i. Physical destruction of property
5JF4563	8125 W. 6th Ave. Frontage Rd.	Craftsman single-family residence	1918	Officially Eligible; Criterion C	No direct or indirect impact (no adverse change to setting); beneficial noise reduction	No Adverse Effect	n/a
5JF4586	401 Wadsworth Blvd.	Gas Station	1958	Officially Eligible; Criterion C	Roadway project ties into existing condition north of property boundary	No Adverse Effect	n/a



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EXHIBIT 8

Effects to Lakewood School Historic District, Site 5JF4512

Building	Description	Date	NRHP Eligibility	Proposed Action Impact	Permanent Effect Finding
Lakewood School Historic District	School complex comprising the New America School and Jefferson County Open School	1927-1977	Officially Eligible Historic District, Criteria A and C	Acquisition of portion of parking lot along eastern edge of the historic district; parking area is noncontributing to the significance of the historic district	No Adverse Effect
Building 1: New America School	Renaissance Revival-style, two-story, brick building	1932-1934	Contributing Resource	Acquisition of portion of parking lot in front of building that does not contribute to historic significance	No Adverse Effect
Building 2: Elementary School	Art Deco, two-story, brick building	1927	Contributing Resource	No Impact	No Adverse Effect
Building 3: Gymnasium	Art Deco, one-story, brick building	1927	Contributing Resource	No Impact	No Adverse Effect
Building 5: Arts and Administration	Formalist, two-story, brick building	Ca. 1968-1970	Contributing Resource	No Impact	No Adverse Effect
Building 7: Junior High	Art Deco, two-story, brick building	1931	Contributing Resource	No Impact	No Adverse Effect
Structure 10: World War II Memorial	Stone plaque and flag pole	Ca. 1950-1951	Contributing Resource	No Impact	No Adverse Effect
Structure 12: Storage Outbuilding	One-story, concrete-block outbuilding	Ca. 1950-1951	Contributing Resource	No Impact	No Adverse Effect
Memorial Field	Soccer field with metal, arched entrance	1950-1951	Contributing Resource	No Impact	No Adverse Effect
Landscaped areas adjacent to Buildings 1, 2, 3, 7	Concrete walkways, stairs, and flanking grass-covered landscaped areas and plantings in the vicinity of Buildings 1, 2, 3, 7	Ca. 1920-1950s	Contributing Resource	No Impact	No Adverse Effect



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EXHIBIT 9
Effects to Green Acres Historic District

Site No.	Address	Description	Date	NRHP Eligibility	Proposed Action Impact	Permanent Effect Finding
Green Acres Historic District	Bounded by Emerald Lane and Reed Street from US 6 to 9th Place	Post World War II residential subdivision	late 1940s to early 1960s	Officially Eligible Historic District, Criteria A and C	Construction of sound wall near south and west boundaries of the district; minor property acquisition from corner of one contributing property; beneficial effects of restoration of neighborhood roads and reduction in traffic noise	No Adverse Effect
5JF3610	700 Park Lane	Ranch clad in red brick	1950	Contributing Resource	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4526	7445 Highland Drive	Split-Level clad in brick	1957	Contributing Resource	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4527	7415 Highland Drive	Two-story Colonial Revival clad in brick	1957	Contributing Resource	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4528	7375 Highland Drive	Split-Level-style house clad in red brick	1958	Contributing Resource	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4529	705 Emerald Lane	Split-Level clad in vertical wood siding	1961	Contributing Resource	Indirect visual impact from construction of a sound barrier wall along rear/side of property	No Adverse Effect
5JF4530	715 Emerald Lane	Ranch clad in blonde brick	1953	Contributing Resource	Indirect visual impact from construction of a sound barrier wall along rear of property	No Adverse Effect
5JF4537	7500 Broadview Drive	Ranch clad in horizontal siding	1947	Contributing Resource	Partial acquisition (approximately 180 square feet) required in southwest corner of property. Indirect visual impact from construction of a sound barrier wall along rear/side of property	No Adverse Effect
5JF4541	7370 Broadview Drive	Ranch clad in horizontal vinyl siding	1947	Contributing Resource	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4543	740 Emerald Lane	Ranch clad in blonde brick	1953	Contributing Resource	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4544	730 Emerald Lane	Ranch clad in red brick	1953	Contributing Resource	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4545	701 Park Lane	Split-Level clad in brick	1955	Contributing Resource	Beneficial indirect effect from reduced noise	No Adverse Effect



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EXHIBIT 9 Effects to Green Acres Historic District

Site No.	Address	Description	Date	NRHP Eligibility	Proposed Action Impact	Permanent Effect Finding
5JF4546	721 Park Lane	Split-Level clad in brick	1955	Contributing Resource	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4547	741 Park Lane	Split-Level clad in brick	1955	Contributing Resource	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4548	761 Park Lane	Ranch clad in brick	1955	Contributing Resource	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4549	781 Park Lane	Ranch clad in brick	1957	Contributing Resource	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4550	790 Park Lane	Ranch clad in vertical siding	1955	Contributing Resource	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4551	660 Park Lane	Ranch clad in red brick	1951	Contributing Resource	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4552	680 Park Lane	Ranch clad in blonde brick	1951	Contributing Resource	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4553	720 Park Lane	Ranch clad in red brick	1951	Contributing Resource	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4636	710 Emerald Lane	Ranch clad in brick	1953	Contributing Resource	Beneficial indirect effect from reduced noise	No Adverse Effect

EXHIBIT 10 Effects to Meadowlark Hills Historic District

Site No.	Address	Description	Date	NRHP Eligibility	Proposed Action Impact	Permanent Effect Finding
Meadowlark Hills Historic District	Bounded by West 6th Avenue/Frontage Road to the north, Carr Street to the east, West 1st Avenue to the south, and Garrison Street to the west	Post World War II residential subdivision	1953 to 1956	Officially Eligible Historic District, Criteria A and C	Construction of sound wall across frontage road near districts northern boundary	No Adverse Effect
5JF4590	545 Meadowlark Drive	Ranch clad in blonde brick	1955	Contributing Resource	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4591	540 Meadowlark Drive	Ranch clad in blonde brick	1954	Contributing Resource	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4592	555 Everett St.	Ranch clad in red brick	1954	Contributing Resource	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4593	560 Everett St.	Ranch clad in brick	1954	Contributing Resource	Beneficial indirect effect from reduced noise	No Adverse Effect



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EXHIBIT 10
Effects to Meadowlark Hills Historic District

Site No.	Address	Description	Date	NRHP Eligibility	Proposed Action Impact	Permanent Effect Finding
5JF4594	585 Estes St.	Ranch clad in blonde brick	1954	Contributing Resource	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4595	580 Estes St.	Ranch clad in blonde brick	1954	Contributing Resource	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4596	585 Dudley St.	Ranch clad in red brick	1954	Contributing Resource	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4597	580 Dudley St.	Ranch clad in blonde brick	1954	Contributing Resource	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4598	585 Dover St.	Ranch clad in blonde brick	1954	Contributing Resource	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4599	590 Dover St.	Split-Level clad in brick	1955	Contributing Resource	Beneficial indirect effect from reduced noise	No Adverse Effect

The following sections detail the effects to each historic property identified in the above exhibits. Properties near the interchange in the northeast quadrant are described first, as these properties are affected by the same aspects of the Proposed Action (the ramp and frontage road configurations) and, therefore, effects are better understood when discussed as a group. The discussion of effects to historic properties in the northeast quadrant is supplemented by Sections 2.2.1 and 2.3 of this report that detail efforts to avoid impacts to historic properties from the interchange design and other design considerations that were considered but did not meet the purpose and need for the project. The site description and NRHP eligibility summary are provided for context to the effects determination to assess whether alterations affect the characteristics that qualify the historic property for inclusion in or eligibility for the NRHP. Full descriptions of the sites and description of their historic importance can be found in *Historic Resources Survey: US 6 and Wadsworth Boulevard, Lakewood, Colorado* (TEC, 2008).

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3.2.1 7395 West 6th Avenue Frontage Road (5JF3548)

Site Description

The building at 7395 W. 6th Ave. Frontage Road is an English Norman Cottage-style, one-story, single-family house that is clad in blonde brick and was built in 1946 (Exhibit 11). The property is located east of Wadsworth Boulevard and north of US 6. The house's principal façade and entrance face West 6th Avenue Frontage Road, and are located approximately 60 feet from the existing edge of the roadway.



EXHIBIT 11

7395 W. 6th Ave. Frontage Road, view to the northeast from West 6th Avenue Frontage Road

Eligibility Determination

The house at 7395 W. 6th Ave. Frontage Road is eligible for listing in the NRHP under Criterion C because the house is representative of the English Norman Cottage architectural style in Lakewood. Its steeply pitched, cross-gabled entrance, brick masonry exterior, and arched door and window at its principal façade are all character-defining features of this style. The detached, two-car brick garage located northwest of the house contributes to the house's historical setting and is a contributing historic feature of the property.

Effect Determination

CDOT would acquire the house and its detached garage under the Proposed Action. As illustrated in Exhibit 12, the roadway slope for the ramp (shown by the green line) would nearly touch the front entrance. Even if retaining walls were used at the edge of the ramp pavement (essentially moving the green line of the slope closer to the red line indicating the pavement edge), CDOT would need to acquire additional right-of-way along the frontage of this property to conduct maintenance activities, and the house would still need to be removed to allow for roadway maintenance.

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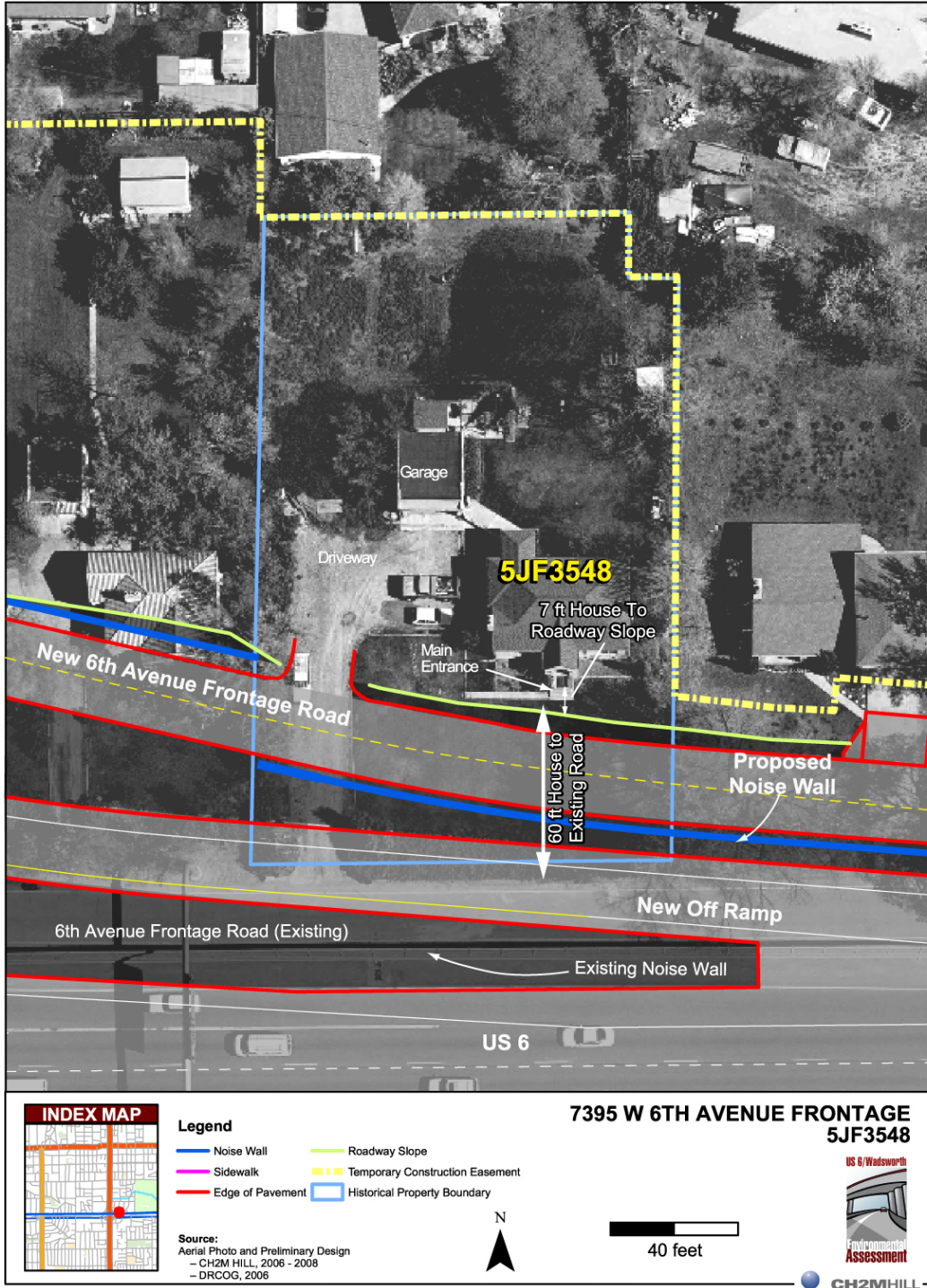


EXHIBIT 12
Effects of Proposed Action to 7395 W. 6th Ave. Frontage Road

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Although the overall property is relatively large at approximately 25,700 square feet, the house is located on the front portion of the property toward the frontage road. CDOT would need to acquire nearly all (93 percent) of the front yard area. CDOT determined that the entire property would need to be acquired for the following reasons: this property would be left with no front yard under the Proposed Action; there would not be sufficient room for CDOT to conduct roadway maintenance with the house in place; and the house would have marginal, if any, viability as a residence so close to the frontage road. Even without removal of the house, the loss of the front yard and setback from the roadway would be a significant **Adverse Effect** to the property's historic setting.

The garage would not need to be removed to support the Proposed Action, but without the house, the garage would not retain sufficient historical integrity to convey its NRHP eligibility. In addition, the garage would have no economical use or function without the house and would, therefore, also be acquired by CDOT. The removal of the house and garage would result in a **direct impact** and an **Adverse Effect** to this historic property under 36 CFR 800.5(i) Criteria of Adverse Effect, the physical destruction of, or damage to, all or part of the property.

Avoidance Options

As detailed in Section 2.2.1, CDOT minimized the footprint of the Proposed Action interchange design to minimize effects to historic properties. There were no design modifications to the ramp or frontage road configurations that could be implemented to avoid impacts to the historic building and meet safety or capacity requirements for the Proposed Action. Impacts to 5JF3548 are primarily tied to the constraints of the gore area, where grade elevations and the lengths of deceleration lanes have been minimized as much as possible but still require removal of the house. Exhibit 4 (in Section 2.2.1) summarizes the constraints that affect the historic properties and why the effects cannot be avoided.

In Section 2.3, several alternative interchange designs to avoid effects to this and other historic properties in the northeast quadrant of the interchange are described. These designs did not meet the safety and mobility requirements for the project. Most were also not effective at saving the historic properties, as illustrated in Exhibits 5 and 6 (in Section 2.3.2). Of the alternative designs that did meet the purpose and need, the Tight Diamond with Loop (the Proposed Action) has the least impact to historic properties in the northeast quadrant because it has the narrowest footprint. Because of the loop ramp in the northwest quadrant, the off-ramp from westbound US 6 to northbound Wadsworth can be a single lane, allowing traffic to merge onto Wadsworth Boulevard without stopping at a signal. Under a traditional diamond interchange (as implemented on the eastbound to southbound and southbound to eastbound ramps on the south side of US 6) or the Single Point Urban Interchange, vehicles must stop at a signal before entering onto Wadsworth (to allow left-hand turns, which are handled by the loop ramp in the Proposed Action). To accommodate queues of waiting vehicles, additional lanes must be provided to avoid cars backing up onto US 6. The additional width required for a multi-lane ramp design would have required more right-of-way in the northeast quadrant. The Partial Cloverleaf has a similar footprint to the Tight Diamond with Loop in the northeast quadrant

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because it maintains a loop in the northwest quadrant and adds a loop in the southeast quadrant.

Another avoidance option CDOT considered was moving the house back on its existing lot and maintaining the property in residential use rather than demolishing the building. While the remaining lot would be approximately 30 percent smaller than others in the neighborhood, it would be a lot size comparable to smaller lots in other areas of metro Denver, and a site plan could be developed to meet requirements for its R1 zoning.

The project team contacted Professional Building Movers Inc., a house moving company, to assess whether the house could be feasibly moved. The company determined that the house could be moved without significant damage, but the detached garage could not be moved because it is located too close to the house and would not be economical to move. (If the house were removed, the garage could stay in place but in order to make room to move the house, the garage would need to be removed or relocated.) The cost to move the house would be approximately \$46,000. This cost does not include a new foundation (or basement), utility disconnect and reconnect, garage demolition, tree removal and site preparation, site cleanup, security fencing, or permitting costs, which could add another \$50,000 or more to the cost. Excavating and finishing a new basement also would add to the cost.

The project team contacted the City of Lakewood Code Enforcement, Planning, Environmental Services, and Heritage, Culture & the Arts departments to determine whether the moved house could meet current City codes and to ascertain their preferences for this property. Generally, there were no restrictions that would prevent CDOT from moving the house and retaining the property in residential use. Lakewood staff did not express strong preferences for either moving or demolishing the properties and preferred to defer to the property owner's wishes. The Heritage, Culture & the Arts (HCA) Manager expressed the desire to consider alternatives to demolishing the house. The Lakewood Historical Society and the Jefferson County Historical Commission (Section 106 consulting parties) will be asked to provide comments or questions based on the conclusions in this report and CDOT and SHPO will consult with them as part of the Section 106 process.

The property owner has been in active contact with CDOT and is anxious to be relocated as soon as possible. Although the property owner does not wish to move her house on the lot, CDOT does have a process where it could acquire the property from the current property owner, move the house, and resell the property. In this scenario, CDOT would need to sell the property with a conservation easement (under 36 CFR 800.5(vii) Criteria of Adverse Effect, Transfer, lease, or sale of the property) placing restrictions or conditions on the house to ensure the long-term preservation of the property's historic significance. CDOT has implemented this type of transfer in the past, but it is not common due to the cost, complexity and administration of an agreement and the difficulty in finding an interested buyer familiar with and accepting of easements and their terms. CDOT would also need the help of a preservation entity, such as Colorado Historical Foundation, to value the easement and enforce its conditions. These costs

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add to the project cost of moving the properties and also represent ongoing maintenance costs to CDOT that are difficult to fund after the construction project is completed.

CDOT is also concerned that the house may be difficult to market because of the restrictions of the conservation easement and the location of the house along the frontage road. Although CDOT is confident that the house would have some value, it is likely to have a significantly reduced value as compared to others in the neighborhood. The potentially long process of selling the property also presents problems protecting the property from vandalism or criminal activity and in maintaining the property in good condition.

The historic integrity of the property would be significantly diminished if the house were relocated to the rear portion of its parcel. The detached garage, a contributing architectural feature, has to be moved before the house is moved because it is in the way. The moving company did not recommend moving the garage due to cost. The removal of the garage would result in loss of an important contributing element of the property's historic significance. The acquisition of 30 percent of the property would cause the property's residential setting to be diminished by the loss of land, the loss of mature trees and vegetation that screen the house along West 6th Avenue Frontage Road, and the reduced setback distance between the front of the house and the new Frontage Road, all of which are characteristics of its setting. Moreover, trees at the rear of the lot would need to be removed to accommodate the relocation of the house. Moving the house would also detach the building from its original, raised basement. The reconstruction of a new basement underneath the house would introduce replacement materials, and result in additional integrity loss to the house.

After careful consideration, CDOT has determined that moving the house is not a feasible avoidance option. Moving the property would diminish the property's historical integrity, and the property owner does not want to move the house. In addition, the cost, complexity, and administration of a conservation easement and difficulty in finding a new owner familiar with and willing to accept the restrictions of this agreement would create a significant challenge to executing this action in a timely manner.

3.2.2 7423 West 6th Avenue Frontage Road (5JF3549)

Site Description

The building at 7423 W. 6th Ave. Frontage Road is a Mediterranean Revival-style, one-story, single-family residence clad in stucco that was built in 1939 (Exhibit 13). The property is located east of Wadsworth Boulevard and north of US 6. The house's principal façade and entrance face West 6th Avenue Frontage Road, and are located approximately 69 feet from the existing edge of the roadway.

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EXHIBIT 13

7423 W. 6th Ave. Frontage Road, view to the north from West 6th Avenue Frontage Road

Eligibility Determination

The house at 7423 W. 6th Ave. Frontage Road is eligible for listing for the NRHP under Criterion C because it is representative of the Mediterranean Revival architectural style in Lakewood. Its low-pitched, cross-gabled roof, terra cotta roofing materials, stucco exterior, arched entrance with glazed decorative tiles, and casement windows are all character-defining features of this style. The house's detached, stucco-clad garage located northwest of the house is also clad in stucco, and is a contributing historic feature of the property.

Effect Determination

CDOT would need to acquire the house and its detached garage under the Proposed Action. As illustrated in Exhibit 14, the proposed off-ramps for westbound US 6 to northbound Wadsworth Boulevard (the northern set of red lines) and roadway slope would run through the house. As with 5JF3548, the garage would not be directly affected but would not retain historic integrity or residential function if disconnected from the residence. Further, CDOT would need to acquire the entire parcel because it would no longer be useful as a residence. The removal of the house and garage would result in a **direct impact** and an **Adverse Effect** to this historic property under 36 CFR 800.5(i), the physical destruction of, or damage to, all or part of the property.

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EXHIBIT 14
Effects to 7423 W. 6th Ave. Frontage Road

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Avoidance Options

As with 5JF3548, effects to 5JF3549 were unavoidable because of the constraints of the interchange design. Other interchange types had equal or greater impacts, and modifications to the Proposed Action that could have saved the property were not possible because those modifications would have resulted in unsafe conditions or severely congested traffic conditions. See discussion and illustrations in Section 2.0.

As with 5JF3548, CDOT also considered moving the house back on the lot and maintaining the property in residential use as an avoidance option to demolishing the building and garage. Like 5JF3548, after acquisition of land for the Proposed Action, the remaining lot would be approximately 30 percent smaller than others in the neighborhood but would be a lot size comparable to smaller lots in other areas of metro Denver. A site plan could be developed to meet requirements for its R1 zoning.

The project team contacted Professional Building Movers Inc. to assess whether the house could be feasibly moved. The company determined that the house could be moved without significant damage, but the detached garage could not because it is located too close to the house and would not be economical to move. (If the house were removed, the garage could stay in place but in order to make room to move the house, the garage would need to be removed or relocated.) The cost of moving the house would be approximately \$45,000. This cost does not include a new foundation (or basement), utility disconnect and reconnect, garage demolition, tree removal and site preparation, site cleanup, security fencing, or permitting costs, which could add another \$50,000 or more to the cost. Excavating a new basement also would add to the cost.

The same administrative and maintenance constraints that made moving 5JF3548 infeasible also apply to 5JF3549. The current property owner is elderly; although she has been in contact with CDOT right-of-way staff regarding the acquisition process, the option of moving her house has not been discussed so her preferences are not known. Depending on the timing of acquisition, even if the current property owner wished to live in a moved property, it is possible that a change in ownership could occur. Regardless of the current ownership, attaching a conservation easement to the property would present long-term challenges to future real estate transactions and would represent a long-term maintenance cost that CDOT would have difficulty funding.

The historic integrity of the property would be significantly diminished if the house were relocated to the rear portion of its parcel. The detached garage, a contributing architectural feature, has to be moved before the house is moved because it is in the way. The moving company did not recommend moving the garage due to cost. The removal of the garage would result in loss of an important contributing element of the property's historic significance. The acquisition of 30 percent of the property would cause the property's residential setting to be diminished by the loss of land, the loss of mature trees and vegetation that screen the house along West 6th Avenue Frontage Road, and the reduced setback distance between the front of the house and the new Frontage Road under the Proposed Action, all of which are characteristics of its setting. Moreover, trees at the rear of the lot would need to be removed to

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accommodate the relocation of the house. Moving the house would also detach the building from its original, raised basement. The reconstruction of a new basement underneath the house would introduce replacement materials, and result in additional integrity loss to the house.

After careful consideration, CDOT has determined that moving the house is not a feasible avoidance option. Moving the house would diminish the property's historical integrity. In addition, the cost, complexity, and administration of a conservation easement and difficulty in finding a new owner familiar with and willing to accept the restrictions of this agreement would create a significant challenge to executing this action in a timely manner.

3.2.3 7433 West 6th Avenue Frontage Road (5JF4542)

Site Description

The building at 7433 W. 6th Ave. Frontage Road is a one-story, Minimal Traditional-style, single-family house that was built in 1940 (Exhibit 15). It is located east of Wadsworth Boulevard and north of US 6. The house's principal façade and entrance face West 6th Avenue Frontage Road and are located approximately 42 feet from the existing edge of the roadway.



EXHIBIT 15
7433 W. 6th Ave. Frontage Road, view to the north from West 6th Avenue Frontage Road

Eligibility Determination

The house at 7433 W. 6th Ave. Frontage Road is eligible for listing on the NRHP under Criterion C because it is representative of the Minimal Traditional architectural style in Lakewood. Its low-pitched, cross-gabled roof, horizontal siding, boxy footprint, and multi-light



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windows are all character-defining features of this style, and all of these features appear to be original and unaltered.

Effect Determination

As illustrated in Exhibit 16, the proposed ramp for westbound US 6 to northbound Wadsworth Boulevard would directly affect the house at 7433 W. 6th Ave. Frontage Road. Because the building would be removed, CDOT would acquire the entire property to implement the Proposed Action. The removal of the house would result in a **direct impact** and an **Adverse Effect** to this historic property under 36 CFR 800.5(i), the physical destruction of, or damage to, all or part of the property.

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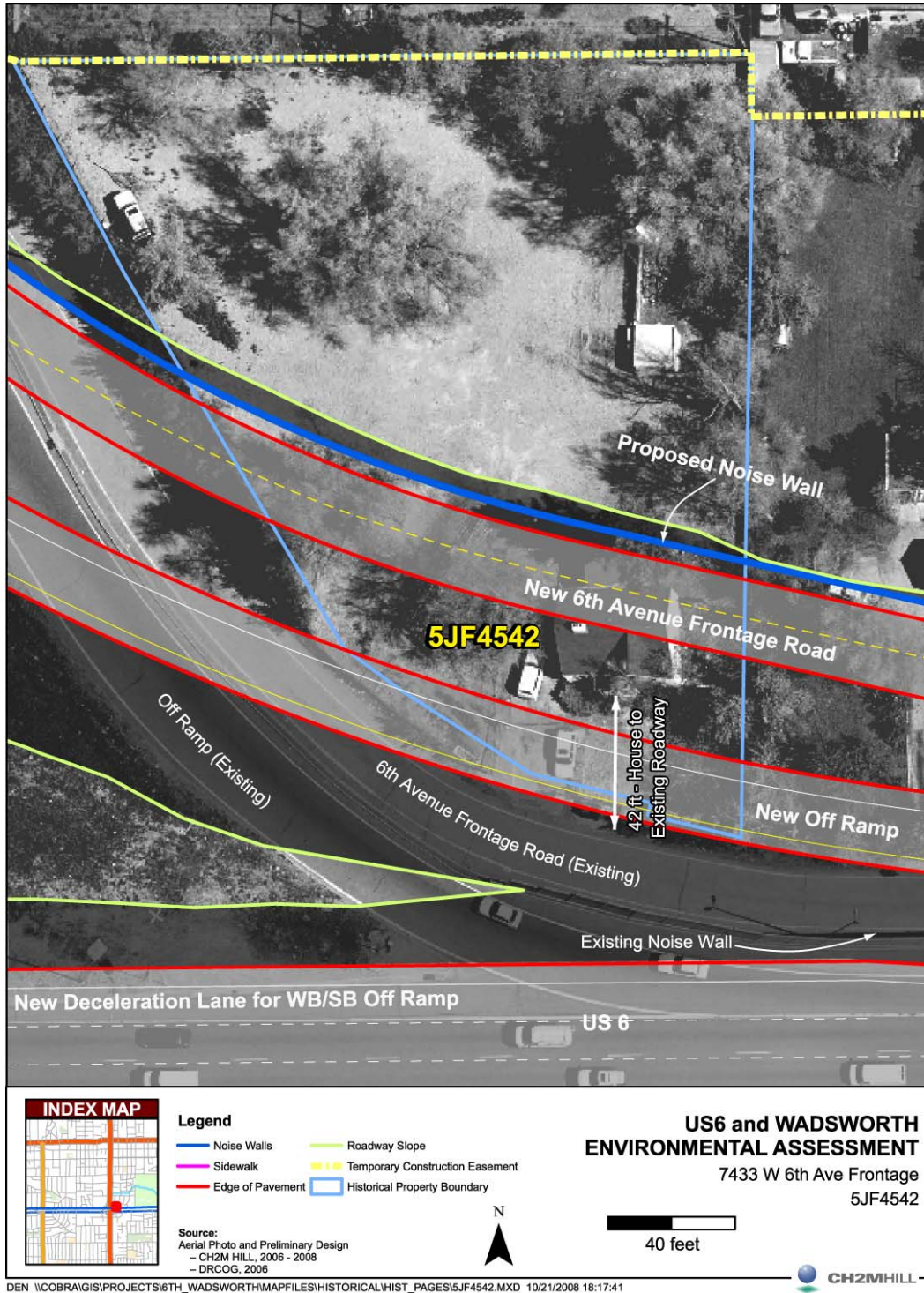


EXHIBIT 16
Effects to 7433 W. 6th Ave. Frontage Road

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Avoidance Options

As with 5JF3548 and 5JF3549, effects to 5JF4542 were unavoidable because of the constraints of the interchange design. Other interchange types had equal or greater impacts, and modifications to the Proposed Action that could have saved the property were not possible because those modifications would have resulted in unsafe conditions or severely congested traffic conditions. See discussion and illustrations in Section 2.0.

As with 5JF3548 and 5JF3549, CDOT also considered moving the house back on the lot and maintaining the property in residential use as an avoidance option to demolishing the house. Like 5JF3548 and 5JF2549, after acquisition of land for the Proposed Action, the remaining lot would be approximately 30 percent smaller than others in the neighborhood but would be a lot size comparable to smaller lots in other areas of metro Denver. Unlike 5JF3548 and 5JF3549, the lot is not deep, but it is wide enough to potentially relocate the house on the existing lot and maintain it in residential use. To develop an approved site plan, however, the house would need to be reoriented on the remainder parcel to maintain the setback requirements for zoning.

The project team contacted Professional Building Movers Inc. to assess whether the house could be feasibly moved. The company determined that the house could be moved without significant damage. There is no garage on this property. The cost would be approximately \$21,000. This cost does not include a new foundation (or basement), utility disconnect and reconnect, tree removal and site preparation, site cleanup, security fencing, or permitting costs, which could add another \$50,000 or more to the cost. Excavating a new basement also would add to the cost.

The same administrative and maintenance constraints that made moving 5JF3548 and 5JF3549 infeasible also apply to 5JF4542. CDOT has been in contact with the property owner about other commercial property he owns in the project area, but the owner has not been interested in effects to this residential property; it is not clear if the owner resides at the house or if the house is a rental property. Regardless of the current ownership and rental status, attaching a conservation easement to the property would present long-term challenges to future real estate transactions and would represent a long-term maintenance cost that CDOT would have difficulty funding.

The historic integrity of 5JF4542 would be significantly diminished if the house were relocated and reoriented to the east end of its remaining land area. Mature trees and vegetation that screen the house along West 6th Avenue Frontage Road would need to be removed. On the west side of the property (which would be the new orientation to the front façade), there would be no vegetation left screening the house from the roadway. Maintaining the required setback distance from the roadway would also require removal of mature trees on the east side of the parcel (which would be the new backyard area in the reoriented house). The loss of land, reorientation of the house, and removal of vegetation would result in loss of characteristics of its setting. Moving the house would also detach the building from its original, raised basement. The reconstruction of a new basement underneath the house would introduce replacement materials, and result in additional integrity loss to the house.

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After careful consideration, CDOT has determined that moving the house is not a feasible avoidance option. Moving the property would diminish the property's historical integrity. In addition, the cost, complexity, and administration of a conservation easement and difficulty in finding a new owner familiar with and willing to accept the restrictions of this agreement would create a significant challenge to executing this action in a timely manner.

3.2.4 700 Wadsworth Boulevard (5JF4536)

Site Description

The building at 700 Wadsworth Blvd. is a one-story, Ranch-style house with Usonian characteristics (Exhibits 17 and 18). It was constructed in 1947 and is clad in ashlar stone masonry. The property is located on the east side of Wadsworth Boulevard, north of US 6, and its principal façade and entrance face north toward Broadview Drive (see Exhibit 17). The property's western edge is located along Wadsworth Boulevard, and the house is set back 33 feet from the existing edge of pavement for Wadsworth (and elevated approximately 10 feet) on this side (see Exhibit 18). The house's rear, south façade is currently set back 57 feet from the off-ramp (and also elevated approximately 15 feet) from US 6 to Wadsworth Boulevard at its southwest corner, the shortest distance between the rear façade and the off-ramp. A concrete retaining wall marks the west and south boundary of the property.



EXHIBIT 17
700 Wadsworth Blvd., view to the south from Broadview Drive

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EXHIBIT 18
 700 Wadsworth Blvd., view to the southeast across Wadsworth Boulevard

Eligibility Determination

The building at 700 Wadsworth Blvd. is eligible for listing on the NRHP under Criterion C because it is a good example of a late 1940s residence that blends the Ranch and Usonian architectural styles. The building's low-pitched hipped roof, its elongated, asymmetrical façade, and minimal front porch are all character-defining features of the Ranch style. The house also exhibits Usonian architectural features with its use of natural materials, as seen in the ashlar stone siding, the deep overhanging eaves, the centralized chimney that is also clad in stone, the inside-outside stone walls that project forward from the principal façade, and the expanses of tall, integrated plate-glass windows that dematerialized the façade and blend the delineation between the indoors and outdoors.

Effect Determination

As illustrated in Exhibit 19, the building at 700 Wadsworth Blvd. is located along the tight curve of the existing off-ramp from westbound US 6 to northbound Wadsworth Boulevard. In addition to the close horizontal distance to both the ramp and Wadsworth, the property is elevated 10 to 15 feet from the surrounding roadways. Not accounting for the grade difference (which exacerbates options to avoid the property), the auxiliary lane on Wadsworth impacts the house to the west, and the frontage road affects the building to the east, the property would need to be removed under the Proposed Action. Because this property would be demolished, the removal of the building would result in a **direct impact** and an **Adverse Effect** to this historic property under 36 CFR 800.5(i) Criteria of Adverse Effect, which is the physical destruction of, or damage to, all or part of the property.

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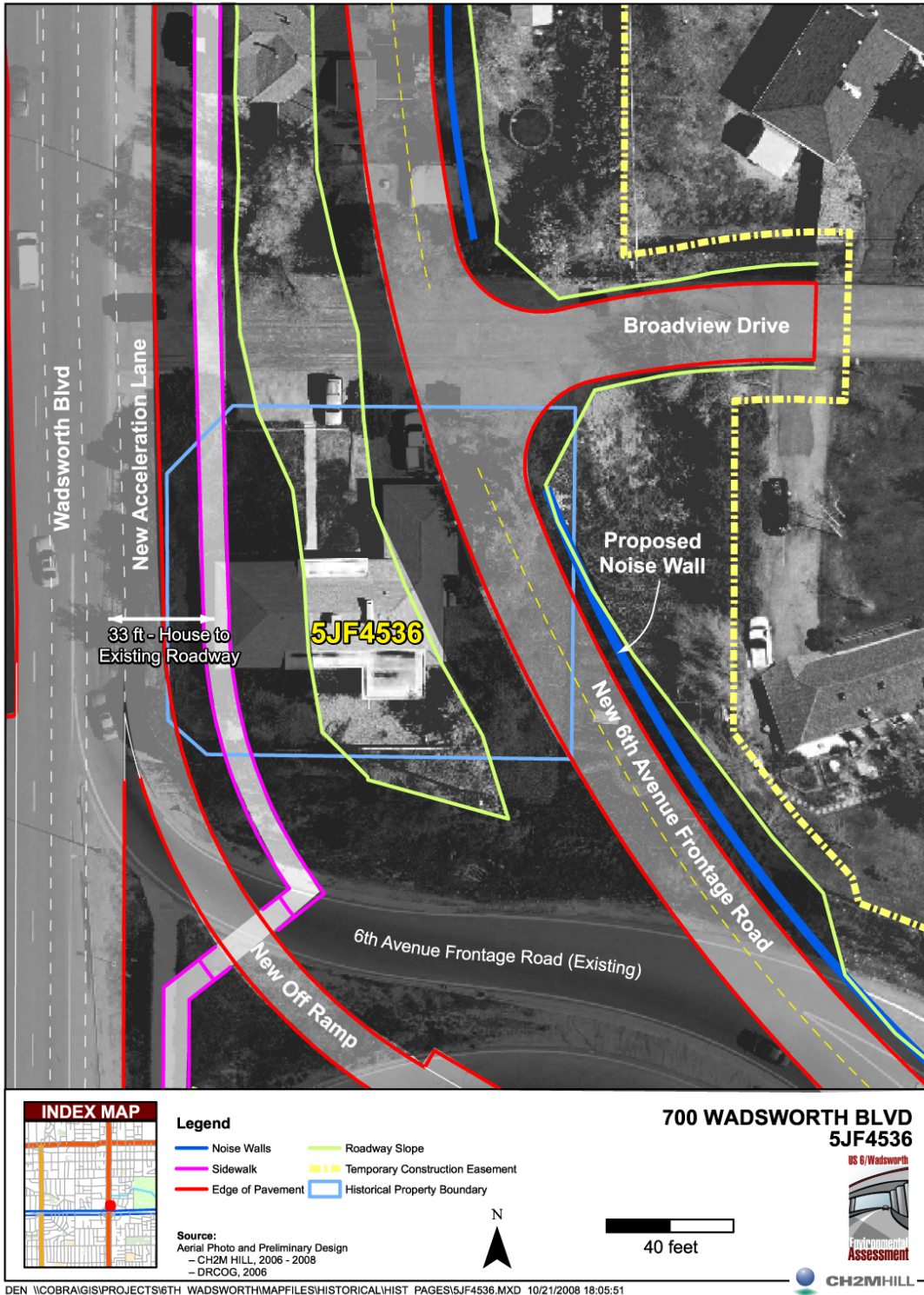


EXHIBIT 19
Effects to 700 Wadsworth Boulevard

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Avoidance Options

CDOT considered numerous design options to avoid direct effect to this property. Both the ramp/frontage road design of the interchange and the widening of Wadsworth affected this property.

As with 5JF3548, 5JF3549, and 5JF4542, effects to the east side of 5JF4536 were unavoidable because of the constraints of the interchange design as described previously. An additional option was considered to save this property that was not available for the other three. This option would create an “island” around the property, where the ramp and the frontage road would be aligned along either side of the property, allowing the property to remain. This “island” option was not possible for the following reasons:

- It would not be possible to provide reasonable access to the property because of the grade differences between the ramp and frontage roads and the house. Moreover, providing direct access to the property from the ramp is not a safe condition because off-ramp traffic would not expect to yield to allow access to the property. Providing access off the frontage road would be acceptable, but it would be difficult to meet sight distance requirements because of the grade differences as the property is on a high point.
- This option would not accommodate the proposed realignment of the frontage road to restore the local road connection to Broadview and separate neighborhood and frontage road traffic. The proposed realignment of the frontage road has significant benefits to the Green Acres neighborhood through improving neighborhood access, reducing cut-through traffic, and improving safety by separating higher speed frontage road traffic from neighborhood traffic.
- Realigning the frontage road to avoid 700 Wadsworth would move the frontage road to the east and would affect one or two contributing properties (7500 Broadview Drive and 715 Emerald Lane) in the Green Acres Historic District.

Unlike the other affected historic properties in the northeast quadrant, the widening of Wadsworth and the addition of the fourth merge lane between the interchange and Highland Drive also affect 5JF4536. To avoid impacts to the property from Wadsworth widening, CDOT considered not adding a fourth lane on Wadsworth between the exit ramp terminus and Highland Drive. This option did not meet the safety and mobility requirements for the project, as illustrated in Exhibit 4 and summarized below.

- If the auxiliary lane were not added, the ramp terminus would need to be changed to a yield condition. The yield condition would cause significant backups along the ramp that would extend to US 6, affecting the traffic movement on US 6.
- The yield condition would also increase the accident potential for rear-end collisions on the off-ramp. The off-ramp is already a high accident location for rear-end collisions, and the yield condition would not be safe.

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- Even if the yield condition were implemented, a drilled caisson retaining wall would be required because of elevation difference between Wadsworth (low) and the property (high), and there is not sufficient horizontal space to build walls with footings. This type of wall construction is expensive and may not be possible without affecting the building at 700 Wadsworth. Because of the narrow area for work, there may not be adequate room to maneuver drilling equipment next to the property to construct a drilled caisson wall. Additionally, the sidewalk would need to be routed around the backside of 700 Wadsworth, which would increase impacts to the east side of this property, as well as the Green Acres Historic District properties.
- To avoid the problems of a single lane yield condition, CDOT considered a two-lane signalized turn at the ramp terminus. This option would solve the queuing and rear-end collision problems but would adversely affect Wadsworth traffic, increasing congestion along Wadsworth as through traffic stops at an additional signal.
- A two-lane signal would also require more space, which would adversely affect the south side of 5JF4536.

Options for 700 Wadsworth are especially limited because the property is located between two sets of constraints (the ramp/frontage road location issues and the auxiliary lane requirements along Wadsworth). There were no feasible options to avoiding effects to this property.

Because of the amount of land required for the Proposed Action and the grade differences between the roadways and the house (the house is on a high point), the house cannot be moved within its existing site. Realigning the frontage road to the east and moving the house to the east on its lot might be possible, but this would create impacts to at least one of the contributing properties in the Green Acres Historic District (see discussion on realigning the frontage road above).

3.2.5 7558 West 9th Avenue (5JF3554)

Site Description

The building at 7558 W. 9th Ave. is an Art Deco-style, one-story, single-family house that was built in 1939 (Exhibit 20). The property is located east of Wadsworth Boulevard along the south side of West 9th Avenue, one parcel east of Wadsworth Boulevard. The house's detached garage is located 170 feet from the eastern boundary of Wadsworth Boulevard. The house's principal façade, entrance, and garage face West 9th Avenue.

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EXHIBIT 20
7558 West 9th Avenue, view to the southeast

Eligibility Determination

The property at 7558 W. 9th Ave. is eligible for listing in the NRHP under Criterion C as a good example of an Art Deco-style, single-family residence in Lakewood. The house's flat, parapetted roof with decorative black glass, geometric lines, and curvilinear entrance wall flanked by glass block lights are characteristic features of the Art Deco style. The house is the only example of an Art Deco single-family residence observed within the APE. Its associated detached garage is located to the southwest of the house and is a contributing historic feature of the property.

Effect Determination

The house at 7558 W. 9th Ave. faces 9th Avenue. The neighboring property to its immediate west, 890 Wadsworth Blvd., which faces Wadsworth Boulevard at 9th Avenue, separates the property from Wadsworth. As illustrated in Exhibit 21, the roadway widening at Wadsworth Boulevard does not directly or indirectly affect this property due to its distance from the roadway. The Proposed Action would require no direct acquisition or use of 5JF3554. The widening of Wadsworth would shorten the distance between Wadsworth Boulevard to the property line from 160 feet to 151 feet but the existing buffer between Wadsworth and the house would remain. The neighboring building at 890 Wadsworth Blvd. would remain in place under the Proposed Action and, therefore, the visual screen toward Wadsworth Boulevard that this building provides would remain unchanged as the result of the widening. Dense vegetation at the west edge of the property between the house at 7558 W. 9th Ave. and the parcel at 890 Wadsworth further screen the house from Wadsworth Boulevard, and this visual buffer would be unaffected. The house is also set back from 9th Avenue, and its principal façade cannot be seen from Wadsworth Boulevard. As a result, indirect visual impacts are not expected from the Proposed Action's widening of Wadsworth Boulevard and construction of sidewalks along Wadsworth Boulevard in this area. Therefore, the Proposed Action would not result in any impact to this property, and the resulting determination is **No Historic Properties Affected**.

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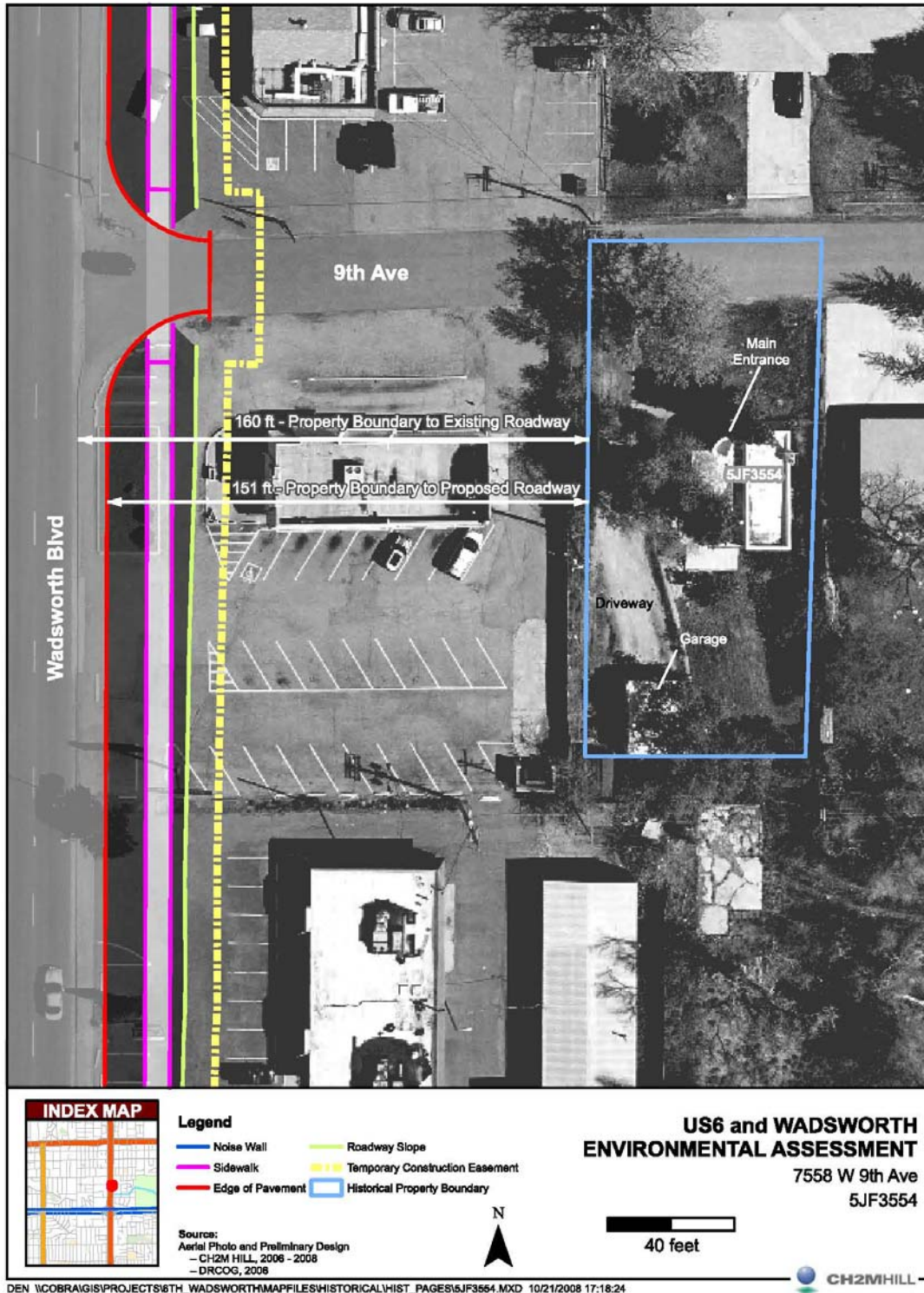


EXHIBIT 21
Effects to 7558 W. 9th Ave.

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Avoidance Options

Because the property is not affected, avoidance options are not necessary.

3.2.6 1215 Wadsworth Boulevard (5JF4511)

Site Description

The structure at 1215 Wadsworth Blvd. is a Dutch Colonial Revival-style, single-family house that was originally built in 1918, and later reconfigured in 1948-1949 (Exhibits 22 through 24). It is located west of Wadsworth Boulevard on the north side of West 12th Avenue. This property can be accessed from both Wadsworth Boulevard and West 12th Avenue. The house's primary entrance is located off of Wadsworth Boulevard, and its principal façade faces Wadsworth Boulevard, approximately 103 feet from the roadway.



EXHIBIT 22
1215 Wadsworth Blvd., view to the northwest

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EXHIBIT 23

1215 Wadsworth Blvd., view to the northwest from the east side of Wadsworth Boulevard



EXHIBIT 24

1215 Wadsworth Blvd., view to the west from the east side of Wadsworth Boulevard

Eligibility Determination

The house at 1215 Wadsworth Blvd. is eligible for listing in the NRHP under Criterion A because the building represents the region's transition from an agricultural community into a suburban city from the 1920s through the 1950s. The house illustrated this change when it was renovated and reconfigured from a barn structure into a single-family residence, while retaining

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the Dutch Colonial Revival character of the original barn through its gambrel roof, flared eaves, dormers, and covered front porch. Although the property does not retain sufficient architectural integrity of its original construction to be eligible under NRHP Criterion C for architectural importance, it is eligible under Criterion A as a direct representative of Lakewood's transition from an agricultural community into a suburban city during the approximate 30-year period between the 1920s and the 1950s.

The three outbuildings that are located at the rear, west side of the property, are not original to the ca. 1920-1950s period of significance and, therefore, are not features that support the historic significance of the property. Because of its relatively undeveloped setting, 1215 Wadsworth Blvd. is one of the few properties remaining on Wadsworth Boulevard that conveys the agricultural setting, feeling, and association of its past use and that of the region's local history. The property's intact historic landscape features consist of the landscaping and trees surrounding the house and the circular gravel driveway that leads from Wadsworth Boulevard to the house. According to as-built drawings for Wadsworth (that show 1215 Wadsworth) and historical photographs, these landscape features and circular drive have remained unchanged since the property's late 1940s conversion, and many of the current trees that screen the house from Wadsworth Boulevard may be original to this period (see Exhibits 25 and 26). The original circular gravel driveway that is seen in historic photos taken in the late 1940s, and shown in a 1960 As-Built Drawing of Wadsworth Boulevard is also still in place, and is significant to the late 1940s-1950s period of significance.



EXHIBIT 25

Historic photo of the house at 1215 Wadsworth Blvd., taken during its reconstruction from a barn to a house in 1948-1949. The circular driveway is seen in the foreground. View is of south façade, looking northwest. Source: Lakewood Historical Society photo collection.

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EXHIBIT 26

Historic photo of the house at 1215 Wadsworth Blvd., 1948-1949. View of west (rear) and south façades, looking northeast. Source: Lakewood Historical Society photo collection.

Landscape features that do not contribute to the historic significance of the property include the areas covering the Dry Gulch culvert at the southwest portion of the property, the additional driveways at the south half of the parcel, and the dirt walking path along Wadsworth Boulevard. The concrete culvert at Dry Gulch that was reconstructed sometime in the late 1960s or early 1970s unearthed this portion of the property, disturbing the original 1940-1950s landscape. The gravel driveways that currently lead from the original, circular driveway southward to 12th Avenue and Wadsworth Boulevard were also added later, probably as the result of the increasing traffic on Wadsworth Boulevard. Finally, the dirt walking path along Wadsworth Boulevard does not appear to be an original feature of the property. In the absence of a concrete sidewalk, this path appears to be the result of pedestrian traffic that has steadily increased with the commercial development of Wadsworth Boulevard in the last 30 years. This pathway feature was not noted on the 1960 as-built drawing (Exhibit 27), and it likely emerged sometime after the widening of Wadsworth Boulevard in ca. 1961, and thus was not created during the property's period of significance, ca. 1920-1950s. Although none of these landscape alterations are visually intrusive to the property's overall historic feeling and association, they do not contribute to the historic setting because they were altered after the period of significance for the property and are not original features to the property.

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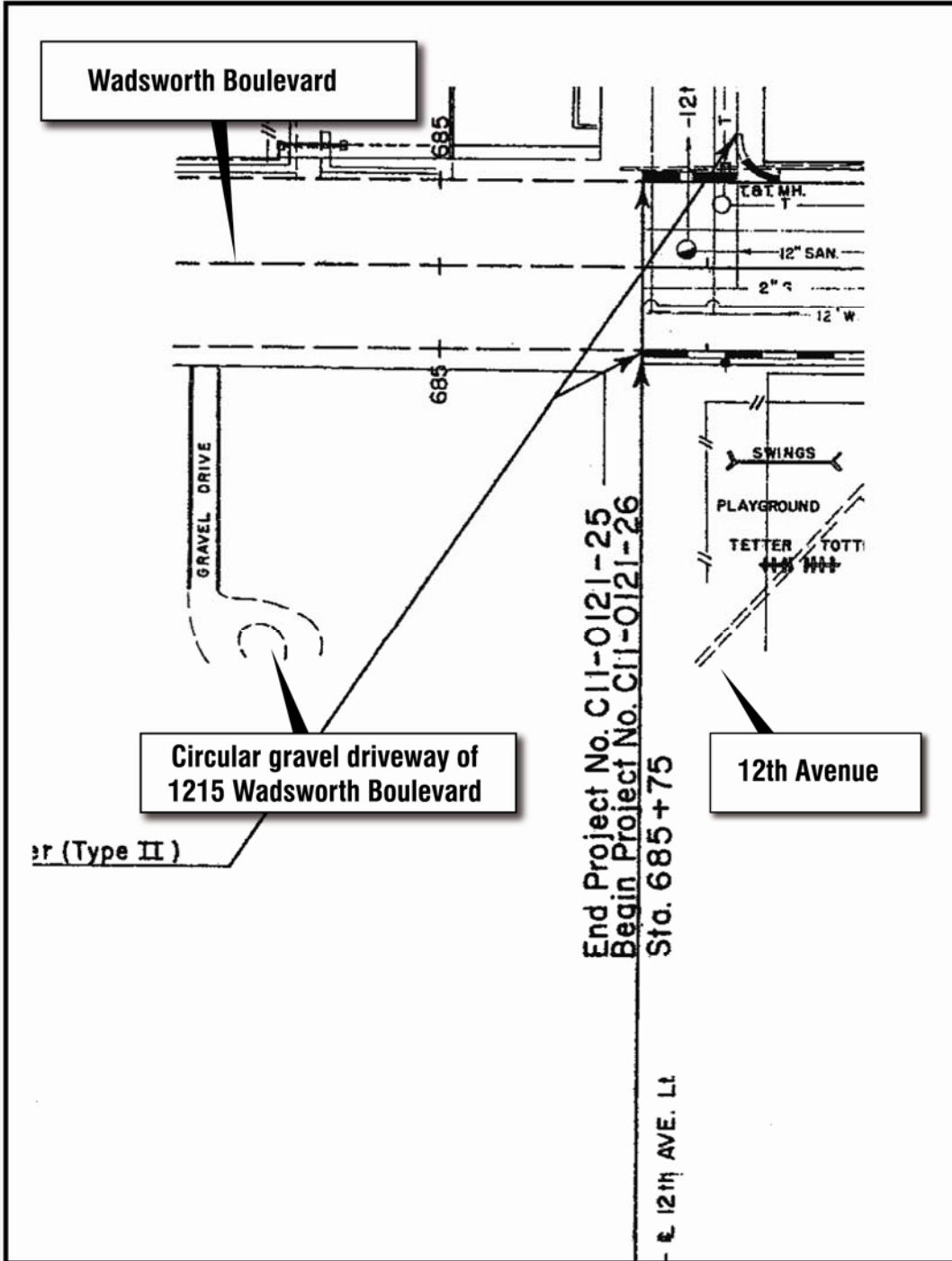


EXHIBIT 27

The circular gravel drive on the 1215 Wadsworth Blvd. property is shown on this portion of a 1960 as-built drawing. Colorado Department of Highways, Plan and Profile of Proposed Colorado Project Number C-11-0121-26. State Highway No. 121, Jefferson County, 11-4-1960.



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Effect Determination

Under the Proposed Action, a concrete sidewalk would be constructed along Wadsworth Boulevard in front of this property where none currently exists. The sidewalk is generally detached with a buffer between the roadway and sidewalk. Detached sidewalks are an important element of the purpose and need for the project to improve pedestrian and bicycle mobility and safety through the project area. At 1215 Wadsworth, detached sidewalks are important because of a projected high volume of pedestrian activity in the area from the Jefferson County Open School immediately to the south of 1215 Wadsworth and the future light rail station north of the property on the east side of Wadsworth. To avoid affecting the large tree at the east central portion of the property (see Exhibit 28), the sidewalk was attached in this location.

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EXHIBIT 28
Rendering of New Sidewalk in front of 1215 Wadsworth Blvd. – before and after conditions (top and bottom, respectively).



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The Proposed Action would require removal of the informal walking path, which is not related to the property's period of significance and is not a feature that contributes to the historic significance of the property. It would also require the removal of a portion of the undeveloped land and part of the original, circular gravel driveway, both of which are original to the period of significance and do support the NRHP eligibility of the property. As illustrated in Exhibit 29, an approximately 37-foot-wide strip along the entire front edge of the property boundary would need to be acquired to accommodate a widened roadway and sidewalk. The roadway would move approximately 12 feet closer to the house, although approximately half of this distance (6 feet) is already within CDOT's ROW along Wadsworth Boulevard.

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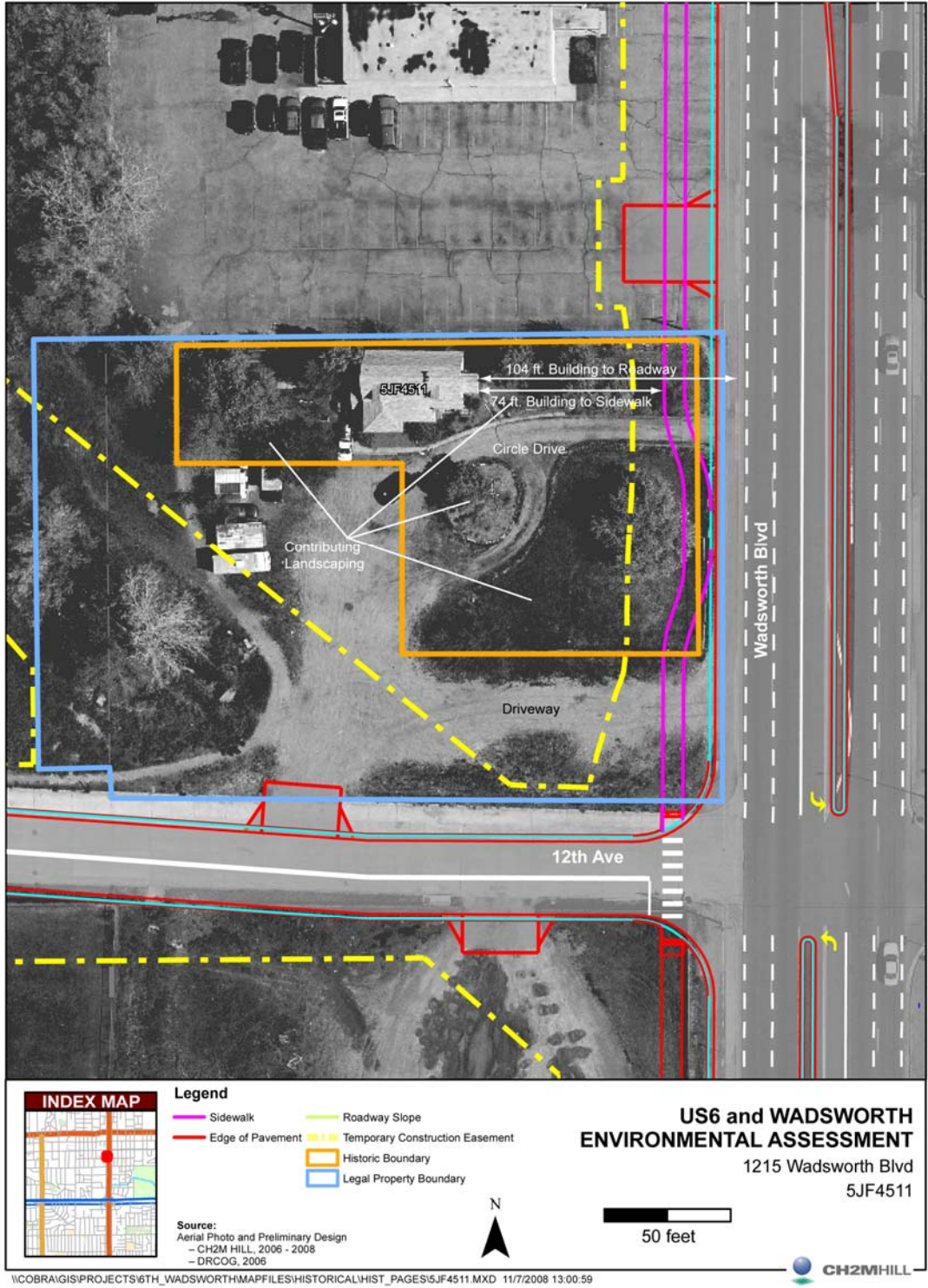


EXHIBIT 29
Effects to 1215 Wadsworth Blvd.

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The primary change to the property and largest acquisition is for the detached sidewalk that would be added to the front of the property. (The dirt walking path present in front of the property is shown in Exhibits 23 and 28.) However, the house would still be set back from the sidewalk by 74 feet and the majority of the circular gravel driveway west of the widening, including the circular portion, would remain intact. The sidewalk design was modified to allow the large tree in the center of the property to remain, as shown in Exhibit 28.

Even with these proposed changes to the landscape along Wadsworth Boulevard, the property would continue to convey its historic significance as a former agricultural property that was converted into a single-family residence, and the site's overall integrity of location, design, setting, materials, workmanship, feeling, and association, would not drastically change or diminish. Therefore, the removal of a minor portion of the historic landscape and setting as a result of implementing the Proposed Action would result in **No Adverse Effect** to the property because the impact does not negatively affect the property's NRHP eligibility.

Although the property's landscape on the south side will be temporarily disturbed when the drainage culvert under Wadsworth Boulevard is widened and lengthened, this area does not contribute to the historical significance of the property. The construction work is not likely to affect the property's trees or other older, historic landscape features at the center and northern areas of the parcel. The current culvert is buried under the property and would continue to be underground and unseen after it is replaced, resulting in no visual impact to the property's historic features. There also is some potential that the Proposed Action could affect the outbuildings in the southwestern portion of the property, but these structures are not historic features of the property.

Avoidance Options

CDOT incorporated a modified sidewalk option to attach the sidewalk in front of the large tree. This design modification avoids impact to the tree, a contributing feature of the historic landscape. Instructions for avoiding the two mature trees along the eastern boundary of the property will be included in the construction specifications to avoid inadvertent damage to the trees during construction.

3.2.7 1230 Wadsworth Boulevard (5JF4513)

Site Description

The building at 1230 Wadsworth Blvd. is a 1928 Craftsman-style, single-family Bungalow residence that currently houses a hair salon business (Exhibit 30). It is located along the east side of Wadsworth Boulevard between 11th and 12th Avenues. The principal entrance fronts Wadsworth and is located approximately 48 feet from the existing roadway. The building is surrounded by asphalt paving on all sides. The primary parking is in the back of the building, although several parking spaces are also available in the front (as shown in Exhibit 30). A large vegetated area is present behind the paved parking area at the rear of the property.

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EXHIBIT 30
1230 Wadsworth Blvd., view to the northeast from across Wadsworth Boulevard

Eligibility Determination

The building at 1230 Wadsworth Blvd. is eligible for listing in the NRHP under Criterion C as a good example of a Craftsman/Bungalow-style single-family residence. This architectural style is evidenced by its wood-frame 3/1 and 4/1 double-hung sash windows, a front porch, and a clipped-gabled roof with overhanging eaves. A windshield survey of Wadsworth Boulevard within the APE indicated that this building is a rare intact example of a late-1920s residence along this segment of the roadway.

The residential setting and landscaping that was original to 1230 Wadsworth Blvd. when it was used as a single-family residence was lost when the landscape was paved over with asphalt to create parking areas in front of and behind the building. Although the house remains intact, this paving resulted in a loss of the residential landscape and historical setting surrounding the building.

Effect Determination

The Proposed Action would construct a new sidewalk along Wadsworth Boulevard in front of 1230 Wadsworth Blvd., and this undertaking would convert a strip of this parcel adjacent to Wadsworth Boulevard for the expanded roadway and sidewalk. As illustrated in Exhibits 31 and 32, the Proposed Action would not affect the structure or its current business use. Burying the overhead power lines along the front of the property would have a beneficial visual effect. Although a portion of the property would be acquired for the Proposed Action, this acquisition would have **No Adverse Effect** to the property because the affected portion of the property does not support the historically important qualities of the house, and the business would remain in place and viable after implementation of the Proposed Action.

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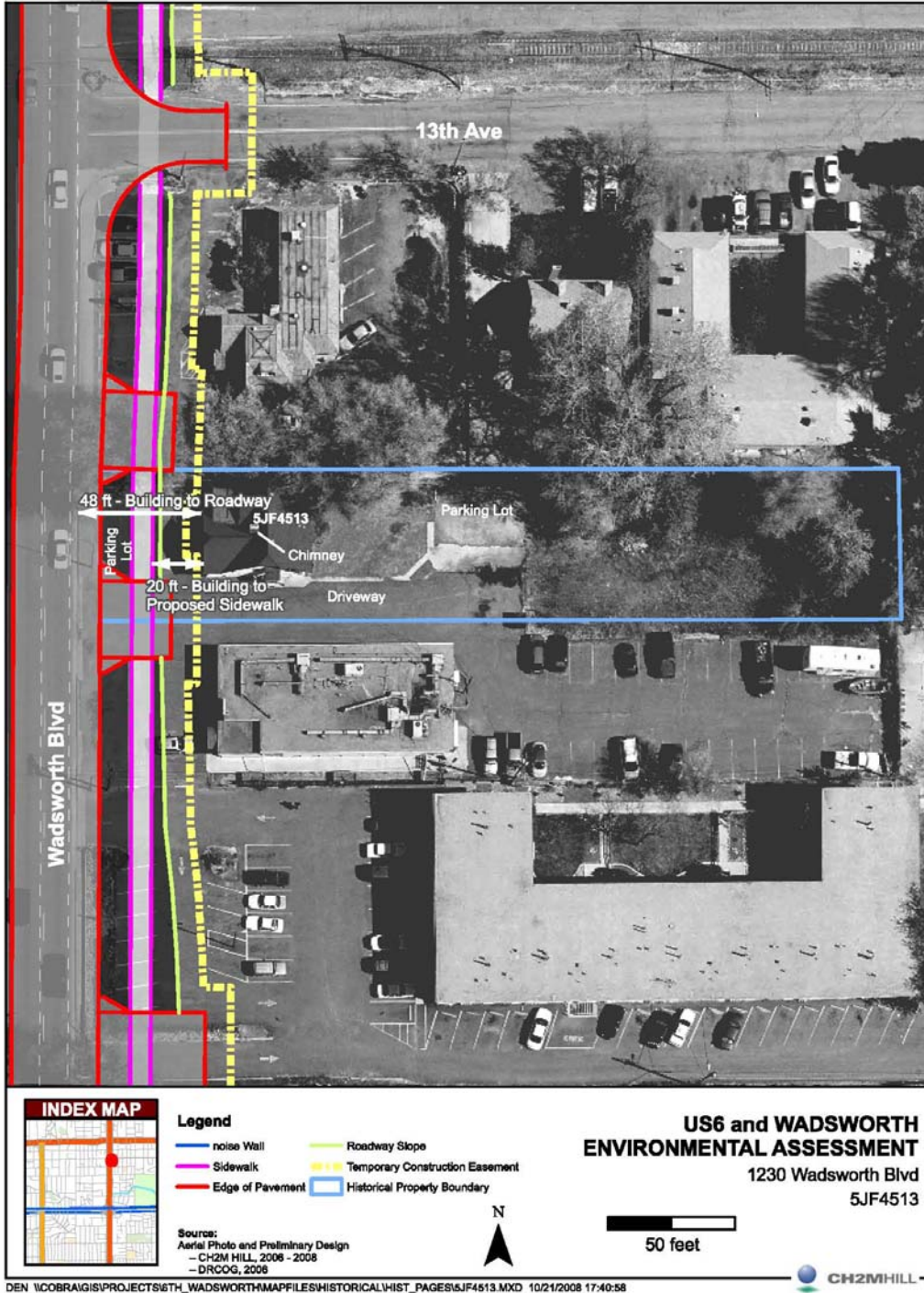


EXHIBIT 31
Effects to 1230 Wadsworth Blvd.

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EXHIBIT 32

Rendering of Wadsworth Boulevard widening and improved sidewalk effects on 1230 Wadsworth Blvd., (5JF4513) – before and after conditions (top and bottom, respectively)

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Avoidance Options

CDOT considered attaching the sidewalk in this location to minimize the effect to the historic property. Because the portion of the property affected does not affect character-defining elements of the property, however, neither the detached nor attached sidewalk adversely affected the historic integrity of the property. CDOT and City of Lakewood right-of-way and planning staff agree that the property can remain in place as a hair salon with the loss of front parking spaces, and acquisition of the property is not necessary to implement the Proposed Action. Detached sidewalks have important pedestrian and bicycle benefits and were, therefore, included in the Proposed Action.

3.2.8 8125 West 6th Avenue Frontage Road (5JF4563)

Site Description

The structure at 8125 W. 6th Ave. Frontage Road is a one-story, Craftsman-style house clad in stucco that was built in 1918 (Exhibits 33 and 34). It is located west of Wadsworth Boulevard, north of US 6, near the northwest quadrant of the interchange. The principal façade and entrance face West 6th Avenue Frontage Road with a very deep (approximately 161-foot) setback from the West 6th Avenue Frontage Road. The principal façade is obscured from view by dense foliage along the roadway and a deep setback from the road.



EXHIBIT 33

8125 W. 6th Ave. Frontage Road, view to the northwest from West 6th Avenue Frontage Road

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EXHIBIT 34

8125 W. 6th Ave. Frontage Road, view to north from frontage road toward principal entrance of house (behind screen of vegetation)

Eligibility Determination

This house is eligible for listing in the NRHP under Criterion C as a good example of the Craftsman architectural style in Lakewood that was popular during the 1910s and 1920s. The style is exemplified by the house's 5/1 sash windows, its steeply pitched, side-gabled roof with clipped gabled ends, overhanging eaves, and its cross-gabled entry porch, which is supported by brackets.

Effect Determination

There would be no direct effect to the property because roadway improvements tie in approximately 300 feet east of the property boundary. Exhibit 35 illustrates the construction limits in relation to the property boundary.

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EXHIBIT 35
Effects to 8125 W. 6th Ave. Frontage Road

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Under the Proposed Action, a sound barrier would be constructed south of the property along US 6 from Wadsworth Boulevard to Garrison Street (as shown in Exhibit 35). The sound wall would not cause an indirect visual impact to the house at 8125 W. 6th Ave. Frontage Road because 1) trees and heavy foliage that obscure the views between the site of the proposed sound wall and the house would remain; 2) the sound wall is located south of the frontage road, so the immediate setting at the property (that is, next to the frontage road) would not change; and 3) there are no important long-range views from the property to the south that would be interrupted by the sound barrier. As illustrated in Exhibit 34, the house's deep 161-foot setback, thick tree stands, and understory shrubbery and foliage also allow for a visual buffer between the West 6th Avenue Frontage Road and the house's principal façade.

Indirect noise impacts are expected to be an improvement over the current noise levels from traffic on US 6. Noise levels are extremely loud, averaging 70 decibels (dB) at the loudest hour during the day, and are projected to increase slightly with the increase in traffic volumes to 71 dB by 2035. The proposed noise walls would reduce projected noise levels by 12 dB, which would have the effect of reducing noise by more than half at this residence. The resulting determination of effect is **No Historic Properties Affected**.

Avoidance Options

Because the property is not adversely affected, avoidance options are not necessary.

3.2.9 401 Wadsworth Boulevard (5JF4586)

Site Description

The building at 401 Wadsworth Blvd. is a gas station constructed in 1958 on the west side of Wadsworth Boulevard at 4th Avenue (Exhibits 36 and 37). Entrances to this property are located at both Wadsworth Boulevard and the north side of 4th Avenue. Two concrete curbs with two gas pumps each are located east of the building and west of Wadsworth Boulevard. The principal façade and entrance to the gas station building, which consists of two garage bays and a shop, are located approximately 64 feet from the edge of Wadsworth Boulevard.

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EXHIBIT 36
401 Wadsworth Blvd., view to the northeast from across Wadsworth Boulevard



EXHIBIT 37
Detail of 401 Wadsworth Blvd., view to the northeast

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Eligibility Determination

The building at 401 Wadsworth Blvd. is eligible for listing in the NRHP under Criterion C for its architectural merit. Built in 1958, the building is a good example of a standardized gas station building that was built by the Sinclair Oil Company during the mid-1950s. The building retains the overall form and materials as it was originally constructed, continues its original function as an operational filling and service station, and has not undergone any alterations. Although the gas pumps themselves are not original to the 1958 gas station and are, therefore, not contributing historic features to the property, the location of the gas pumps is a contributing historic landscape feature that is associated with the gas station.

Effect Determination

The project limits for the Proposed Action extend from 4th Avenue northward. No roadway widening or major construction would be necessary for 401 Wadsworth Blvd. Some curb repair within CDOT's ROW would be implemented as part of the Proposed Action and the existing attached sidewalk would be reconstructed to be a level walking surface but no other physical changes to the gas station property are proposed. The existing sidewalk is within the public right-of-way and is not part of the current boundary of the property. The building, pumps, access, and circulation of the gas station would remain the same as the existing condition. Exhibit 38 illustrates the proposed changes at the gas station property.



EXHIBIT 38

Rendering of improved sidewalk at Sinclair gas station, 401 Wadsworth Blvd., (5JF4586) – before and after conditions (left and right, respectively)

The minor roadway work in front of the property (in CDOT ROW) is considered an indirect visual effect, but it would not negatively affect the property's NRHP eligibility because the setting is not radically changed and the ability of the property to convey its significance is not changed. The resulting effect determination is **No Adverse Effect**.

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Avoidance Options

The Proposed Action design was modified to limit improvements in front of 401 Wadsworth. Detached sidewalks were not implemented in this location because detached sidewalks would have encroached on the east side of the gas pumps; loss of two of the eight pumps may have made the gas station uneconomical, requiring acquisition. Moreover, because the sidewalks to the north of the property (north of the project limits) are attached, an attached sidewalk is a reasonable option for pedestrian and bicycle mobility.

3.2.10 Lakewood School Historic District (5JF4512)

Site Description

The NRHP-eligible Lakewood School Historic District is an elementary, middle, and secondary school complex that comprises the New America School and Jefferson County Open School in Lakewood (Exhibits 39 and 40). The property is located west of Wadsworth Boulevard between West 10th Avenue and West 12th Avenue. It consists of 24 buildings, structures, and landscape features, nine of which are contributing resources to the historic district. With the exception of one building, Building 1, all of the contributing buildings, structures, and landscape features within the school property either front 12th Avenue or are located at the center or rear portion of the parcel. However, Building 1, or 1015 Wadsworth Blvd., faces Wadsworth Boulevard and is positioned near the intersection of Wadsworth Boulevard and 10th Avenue (Exhibit 39). Building 1 is surrounded by a grassy lawn and trees in the immediate vicinity of the building. A paved parking area separates Building 1 and this grassy area from Wadsworth Boulevard (Exhibits 41 and 42).



EXHIBIT 39

Lakewood School Historic District, Building 1, view to the northwest from the intersection of Wadsworth Boulevard and 10th Avenue

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- | | |
|--|---|
| 1 – New America School | Structure 9 – Outbuilding, Jefferson County Open School |
| 2 – Elementary School, Jefferson County Open School | Structure 10 – World War II Memorial |
| 3 – Gymnasium, Jefferson County Open School | Structure 11 – Storage Outbuilding, Jefferson County Open School |
| 4 – Cafeteria and Library, Jefferson County Open School | Structure 12 – Storage Outbuilding, Jefferson County Open School |
| 5 – Administration and Arts Building, Jefferson County Open School | Structure 13 – Storage Outbuilding, Jefferson County Open School |
| 6 – Connecting Building, Jefferson County Open School | Structure 14 – Concessions Building, Jefferson County Open School |
| 7 – Junior High School, Jefferson County Open School | Structure 15 – Storage Outbuilding, Jefferson County Open School |
| 8 – Theater, Jefferson County Open School | Structure 16 – Bleachers, Jefferson County Open School |
| | Structure 17 – Ticket Booth, Jefferson County Open School |

EXHIBIT 40
Lakewood School Historic District

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EXHIBIT 41
Lakewood School Historic District, view to the southwest from Wadsworth Boulevard and 12th Avenue



EXHIBIT 42
Lakewood School Historic District, view southwest from Wadsworth Boulevard toward Building 1 and its parking lots

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The northeast portion of the Lakewood School Historic District site is adjacent to Wadsworth Boulevard. The entire extent of this area is a large, gravel-covered parking area that extends from the parking area in front of Building 1, northward to 12th Avenue, which forms the northern boundary of the historic district (Exhibit 43).



EXHIBIT 43

Parking area in the northeast portion of Lakewood School Historic District, view to the northeast toward Wadsworth Boulevard and 12th Avenue

Eligibility Determination

The Lakewood School Historic District school complex comprising the New America School and Jefferson County Open School is eligible for listing in the NRHP as a Historic District under Criteria A and C. Under Criterion A, the school's contribution to the community and its historical significance resides in the site's history as one of the first public school campuses in Jefferson County. The Lakewood School Complex also meets Criterion C for distinctive characteristics of property types for educational buildings, for its representation of architectural styles of distinct stylistic periods, and for the high artistic values that each of the contributing buildings represents.

Seven buildings and structures and two landscape features are contributing resources within the Lakewood School Historic District. These are illustrated in Exhibit 40 and include Building 1 (New America School), Building 2 (Elementary School), Building 3 (Gymnasium), Building 5 (Administration and Arts), Building 7 (Junior High), Structure 10 (World War II Memorial), and Structure 12 (Storage Outbuilding). Contributing landscape features are the landscaped areas and walkways to the immediate south and east of the contributing school buildings and the Memorial Field including its archway. The remaining 10 noncontributing buildings comprise

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buildings and structures that were built after 1970: Building 4 (Cafeteria and Library), Building 6 (Connecting Building), Building 8 (Theater), Building 9 (Outbuilding), Structure 11 (Storage Outbuilding), Structure 13 (Storage Outbuilding), Structure 14 (Concessions), Structure 15 (Storage Outbuilding), Structure 16 (Bleachers), and Structure 17 (Ticket Booth). Five landscape features that are noncontributing comprise the remaining areas, including the elementary school playground, the baseball field, all parking lots, landscaping adjacent to the post-1960s buildings, and fences within the school complex. The boundaries of the Lakewood School Historic District are the legal boundaries of the school complex, which are formed by Wadsworth Boulevard to the east, West 12th Avenue to the north, West 10th Avenue to the south, and Allison Street to the west.

Effect Determination

The majority of the Lakewood School historic district would be unaffected by the Proposed Action. No changes are proposed along 12th Avenue or at the 12th Avenue and Wadsworth intersection that would affect the property. The Proposed Action would require acquisition of a strip of the school property that fronts Wadsworth (on the east side of the historic district). In this area, the Proposed Action would add a right-turn lane and an attached sidewalk in the area near Building 1 along Wadsworth Boulevard. (The turn lane provides a buffer from fast-moving through traffic on Wadsworth that is similar to a detached sidewalk.) Detached sidewalks would be constructed north of this turning lane along Wadsworth Boulevard.

As illustrated in Exhibit 44, the Proposed Action would require the removal of a portion of the paved and gravel-covered parking areas that front Wadsworth Boulevard within the Lakewood School Historic District. These parking areas, however, are noncontributing landscape features of this historic district because they do not date to the Lakewood School's period of significance. Exhibits 41 and 42 show the affected parking areas in relation to Building 1.

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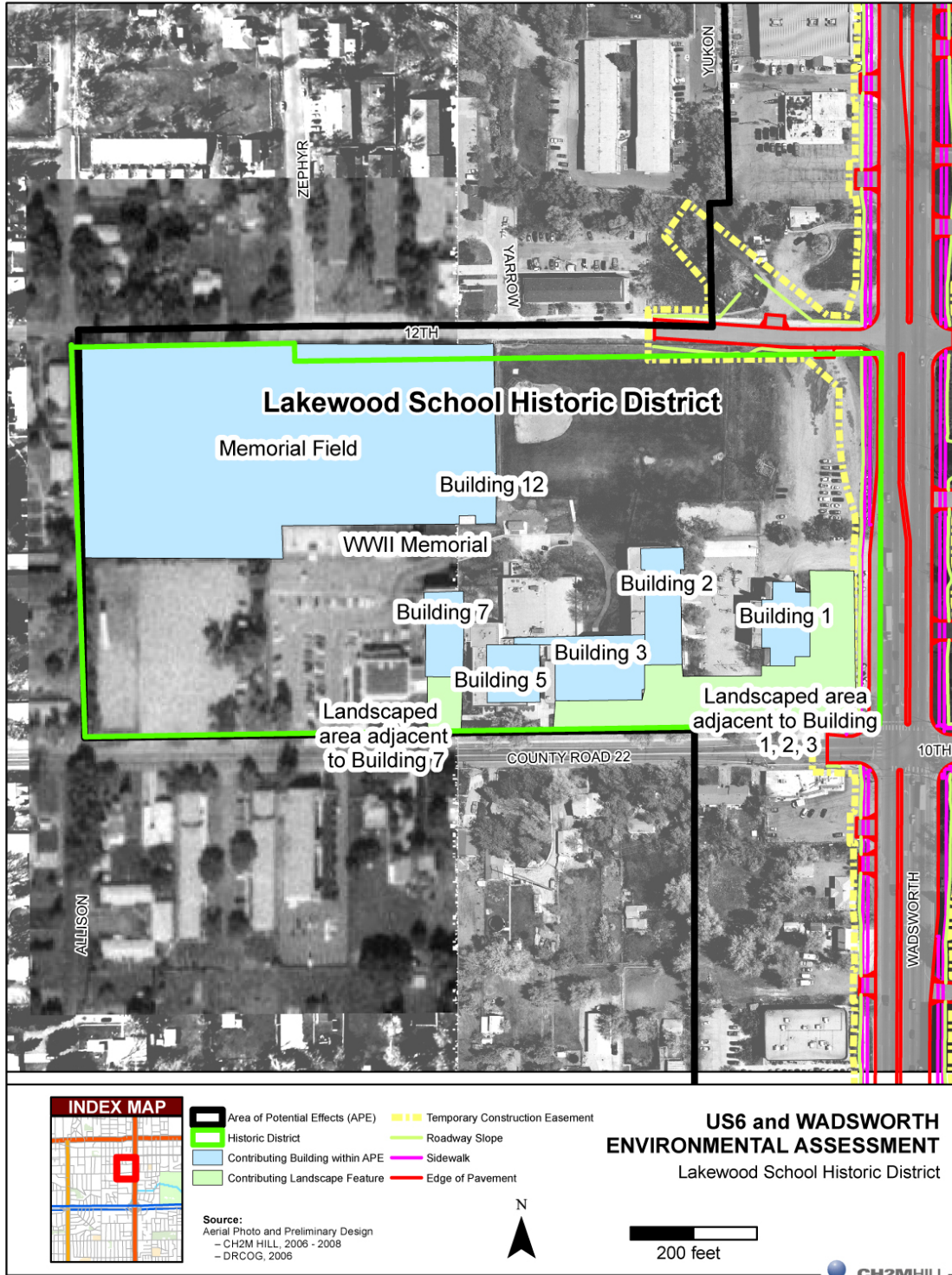


EXHIBIT 44
Effects to Lakewood School Historic District

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The Proposed Action also would not affect the historic contributing landscaping directly in front of Building 1. Exhibit 45 illustrates the future sidewalk condition in relation to the historic landscaping (behind the split rail fence).



EXHIBIT 45

Rendering of effects of Wadsworth Boulevard widening on Lakewood School Historic District at the corner of 10th Avenue and Wadsworth Boulevard – before and after conditions (top and bottom, respectively)

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Indirect visual impacts are also not expected due to the previous loss of integrity of the areas that will be removed in the undertaking. Increased noise levels are expected in the vicinity of Building 1, but this increase is expected to be small, and would not result in a significant impact to the historic district or Building 1 as a contributing element of the district.

The removal of a portion of the school property that does not contribute to its historic significance would result in **No Adverse Effect** to the historic district as a whole or to Building 1 as a contributing element of the historic district. The impact does not affect historic features of the district or building and, therefore, does not negatively affect the property's NRHP eligibility. The portion removed consists of landscape features that are not important historical elements of the district, will be a small percentage of the total property, and are also located on the property's outer eastern edge. The removal of overhead power lines would have a beneficial visual effect for the school property, and addition of sidewalks throughout the project area also would have a beneficial effect in supporting students walking to and around the school. No other historic buildings, structures, or historic landscapes within the NRHP-eligible Lakewood School Historic District would be affected by the Proposed Action because of their location and setback from Wadsworth Boulevard and, thus, their distance from the area of undertaking.

Avoidance Options

CDOT considered not including the deceleration lane for right turns at 10th Avenue as a measure to reduce the encroachment onto the school property. This deceleration lane, however, is necessary to avoid adverse effects to traffic volume and flow on Wadsworth. The school is a notable generator of traffic, including slower moving bus traffic, that requires the deceleration lane. Because 10th Avenue is narrow and not being widened as part of this action, right turns onto 10th Avenue require an even slower turn speed. Without the deceleration lane, a detached sidewalk would have been necessary in this location to separate pedestrians and bicyclists from through traffic. A detached sidewalk would have also encroached on the school property. Because the portion of the property affected does not contribute to the historic significance of the property, neither the sidewalk nor the turn lane adversely affect the historic integrity of the property. These changes to the Proposed Action, therefore, were not necessary to avoid adverse effects to the historic district or any of its contributing elements.

3.2.11 Green Acres Historic District

Site Description

The NRHP-eligible Green Acres Historic District is a residential neighborhood located northeast of the intersection of Wadsworth Boulevard and US 6. The historic district is approximately bounded by Emerald Lane at its western edge, Reed Street at its eastern edge, West 6th Avenue Frontage Road at its south edge, and 9th Place at its northern boundary. It includes the roadways Highland Drive and Broadview Drive that branch eastward from Wadsworth Boulevard. The neighborhood consists of single-family houses primarily built in the 1940s, 1950s, and early 1960s. Most of the lots maintain the 50-foot setbacks required by neighborhood covenants. Most of the parcels are landscaped with broad grassy lawns, mature trees, and ornamental plantings.



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Eligibility Determination

The Green Acres neighborhood is an eligible NRHP historic district as an intact, representative example of a post-World War II residential subdivision. It is eligible under NRHP Criterion A for the neighborhood's establishment in Lakewood between the late 1940s and early 1960s. Platted in 1940, Green Acres began to develop with single-family residences by 1947 and the majority of the houses were built by 1961. The neighborhood's development during this time reflects an important historic trend in suburban growth within the Denver metropolitan area during the Post World War II era, and is an example of a development that led to Lakewood's establishment as an independent city in 1969. The boundaries of the historic district and the contributing properties within the APE for the US 6/Wadsworth project are depicted in Exhibit 46.

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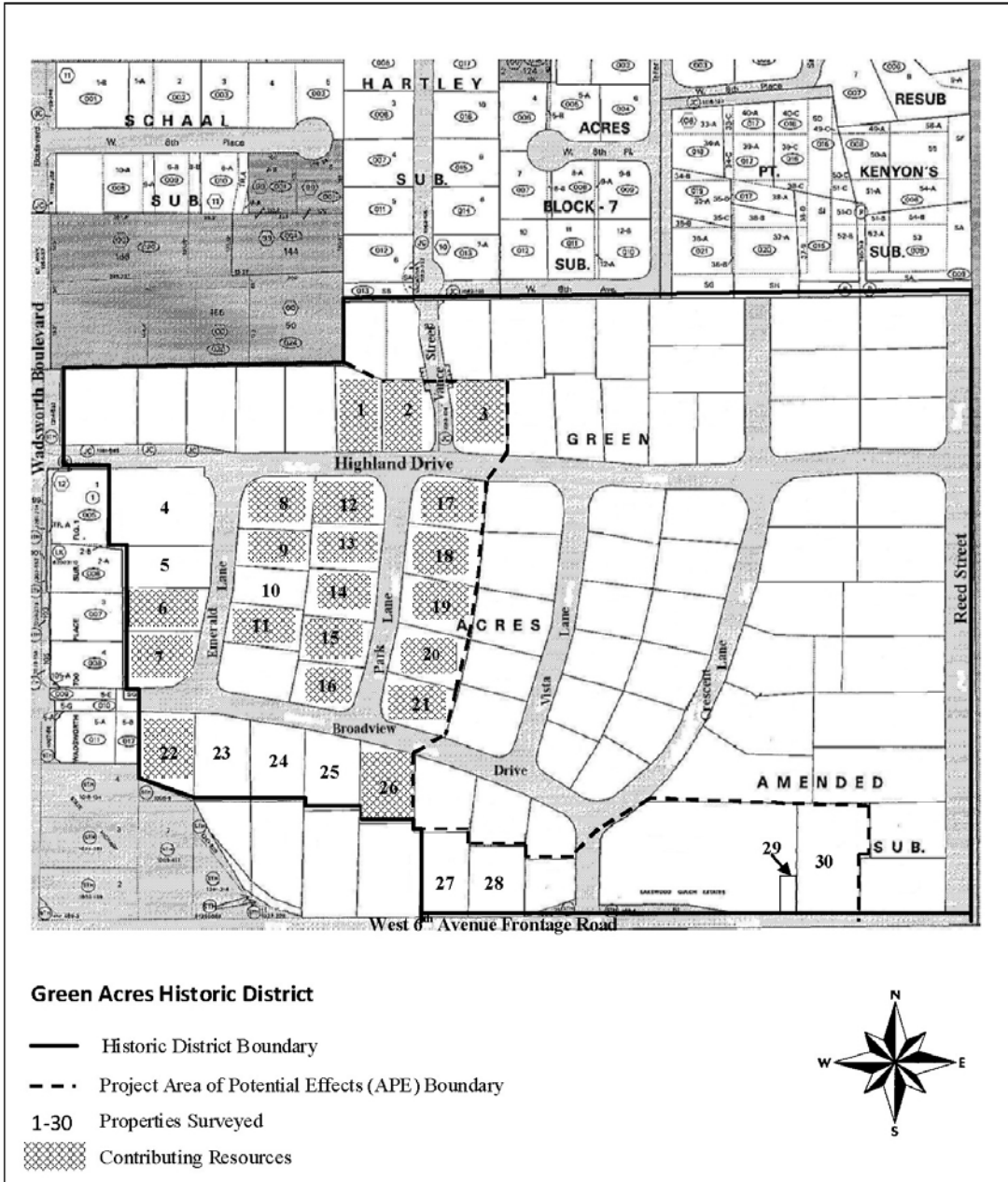


EXHIBIT 46
 Green Acres Historic District

Green Acres stands out as a highly intact post-World War II subdivision that was marketed to the upper-middle class in Lakewood. The 1956 advertisement in the Annual Denver Area Parade of Homes confirms that the housing was costlier than other neighborhoods in Lakewood at that time (Center for Historic Preservation Research, 2006:15). The higher cost may also be due to the neighborhood’s characteristic large lots with generous housing setbacks. The development’s location due west of the older establishment of the Lakewood Country Club

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suggests that some of the early residents may have been members of this prestigious Club. The houses that were built in Green Acres during the 1950s also coincide with the opening of the nearby Denver Federal Center in 1950. When the Federal Government converted the former DOP into the Denver Federal Center, the new campus became a regional center of government operations and employment. Its opening helped revitalize Lakewood as a residential suburb providing middle-class housing to Federal Center employees.

The Green Acres Subdivision is also eligible under NRHP Criterion C as an important example of post-World War II neighborhood planning and design principles. Many of the development's features, such as uniform setbacks, were design elements recommended by the Federal Housing Administration as early as the 1930s that set the standard of post-World War II subdivision planning nationwide. The Green Acres subdivision is also notable as the first community in Lakewood controlled by building standards imposed by a neighborhood association. As such, the houses built in the 1940s and 1950s all maintain the 50-foot setbacks required by the neighborhood covenants. Each property features broad grass-covered lawns that collectively create visual continuity along the street front, while mature trees and plantings are typically set back and embellish the houses. Architecturally, the housing's consistent informal Ranch Style and Split-level architecture reflects an attempt made by developers to appeal to the consumer during a time when these styles had gained widespread popularity in suburban residential architecture. Picture windows, low, horizontal profiles, and horizontally oriented bands of windows became distinguishing features of both domestic styles. These elements also were well-adapted to suburban developments like Green Acres (Ames and McClelland 2002, 49, 66), and as such, the neighborhood is an excellent example of the execution of this suburban model within the greater Denver metropolitan area.

Although approximately one-third of the houses surveyed were found to have sustained varying degrees of alterations, about two-thirds of the neighborhood remains highly intact and with good physical and historical integrity to its original construction. As a result, Green Acres as a whole continues to convey its location, setting, feeling, and association of a mid-twentieth century residential subdivision.

The period of significance for the NRHP-Eligible Green Acres Historic District is 1947 through 1961, which is when the majority of the houses in the neighborhood were built. The district boundaries reflect both the historic plat map dating to the 1940s, and also this period of development (Exhibit 46). Therefore, the boundary encompasses the properties along both sides of Broadview Drive and Highland Drive along the northern and southern boundaries, and is framed by the properties along both sides of Emerald Lane and Reed Street at the west and east ends, respectively. The boundary also includes the area including Vance Street and West 8th Avenue, which was part of the original 1940 plat map and includes neighborhood housing dating to the period of significance, 1947-1961.

The historic district includes approximately 107 homes, of which 30 are included within the APE. As the result of surveyed and evaluations for NRHP eligibility, 20 of these properties have been determined to be contributing features of the NRHP-eligible Green Acres Historic District. The remaining 10 properties evaluated are determined not eligible for nomination to the NRHP



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due to integrity loss from substantial alterations or additions to the principal façade. The boundary of the historic district and the distribution of contributing and non-contributing elements within the APE are illustrated in Exhibit 46.

Effect Determination

The Proposed Action would result in the construction of new ramps to US 6 and the reconstruction of sound barrier walls along US 6. This construction would be located at the southwestern edge of the Green Acres Historic District and, therefore, would not directly or indirectly affect the vast majority of properties within the district. Exhibit 47 illustrates the relationship of the Proposed Action to the historic district and the contributing properties within the district.

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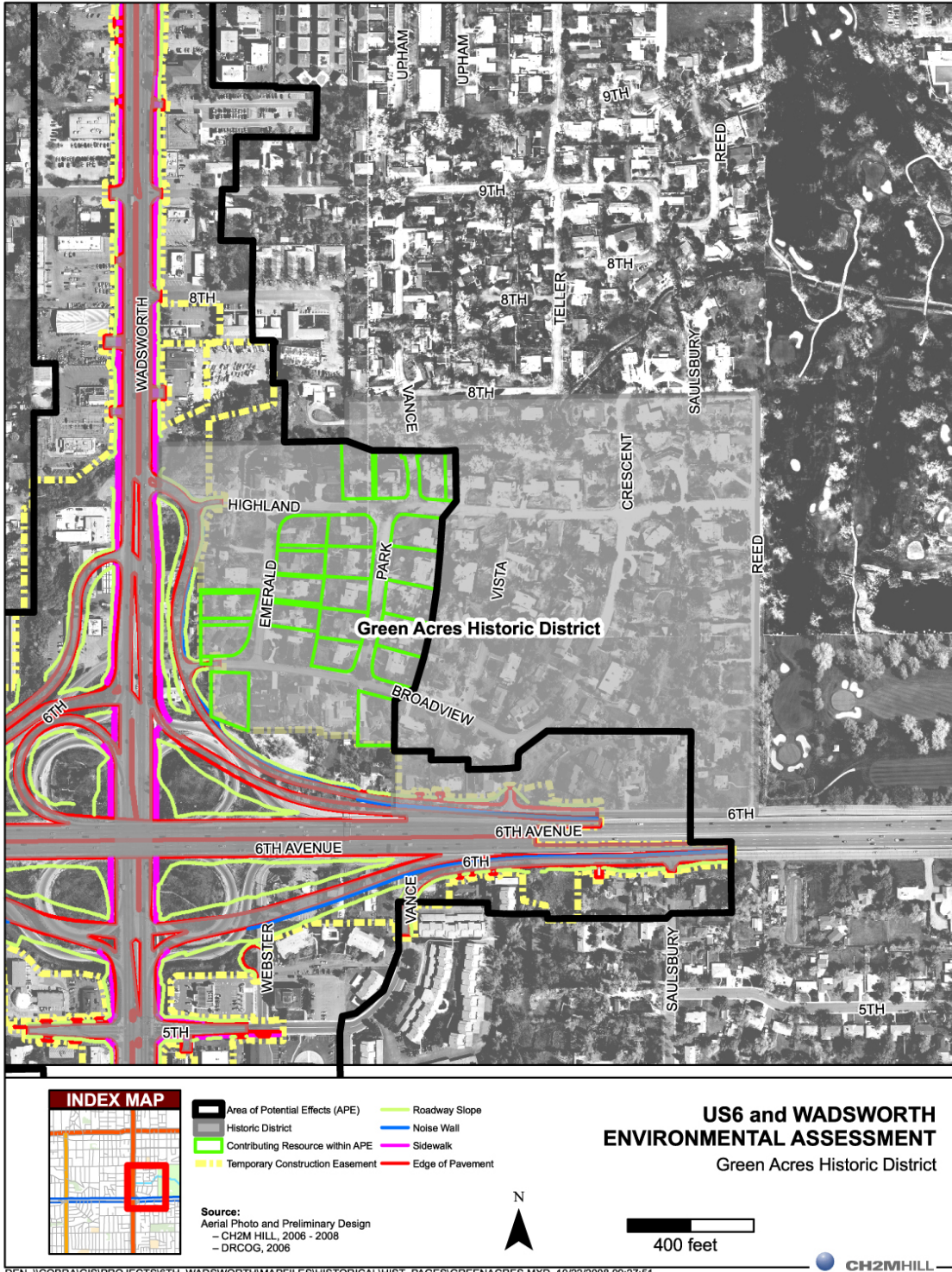


EXHIBIT 47
Effects to Green Acres Historic District, including effects to 5JF4529, 5JF4530, and 5JF4537 (westernmost properties)



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The Proposed Action would have minor visual effects to three contributing properties that would be adjacent to a new proposed sound wall. Effects to the contributing properties along the western edge of the historic district (5JF4537, 5JF4529, and 5JF4530) are discussed individually below. The effect of the proposed sound wall on the historic district as a whole is very minor and not adverse. The new sound wall constructed along the western edge of the historic district would replace the buffer of other residential homes (not part of the historic district) that would be removed to the west of the historic district boundary along Wadsworth. The wall would be only minimally visible from the three properties closest to the wall and not visible from other properties within the district. Replacing the existing noise walls along US 6 frontage road (near Property 26 in Exhibit 46) would have minimal effects because the walls are located far from the district boundary, and there are currently noise walls installed in this location.

The Proposed Action would have beneficial effects to the neighborhood setting. Installation of noise walls (and replacement of existing walls) would effectively reduce traffic noise for most of the homes within the historic district. Reduced noise would be a beneficial effect to the residential setting of the neighborhood. The realignment of the frontage road also would have a beneficial effect on the residential character of the neighborhood. Traffic cutting through the neighborhood streets to access the frontage road creates a higher volume and speed of traffic than is typical of a residential area. Neighbors have consistently expressed concern about cut-through traffic, and reducing traffic volumes and speeds through area residential neighborhoods is a tenet of the project's purpose and need. The restoration of a local road network and separation of frontage road and neighborhood street traffic are beneficial effects to the residential neighborhood and return it to its original local road configuration consistent with the neighborhood's original development.

The minor visual effect of noise walls on the historic district and the improved noise and traffic conditions in the neighborhood would result in **No Adverse Effect** to the historic district.

Three contributing properties could experience indirect visual effects from the construction of the sound barrier walls at the southwest portion of the neighborhood. These properties are currently screened from Wadsworth by existing residential structures that would be removed under the Proposed Action. The proposed sound barrier walls would replace the visual screen from Wadsworth that the removed properties currently provide, and would provide better noise and visual screening from Wadsworth traffic than currently exists. These three properties are 7500 Broadview Drive (5JF4537), 705 Emerald Lane (5JF4529), and 715 Emerald Lane (5JF4530), each of which is described in detail below. The project would also require acquisition of a small corner of 5JF4537, which also is described below.

Avoidance Options

CDOT included shorter noise walls for the houses along the frontage road where it wraps around Wadsworth to avoid greater visual effects to the residential neighborhood. The shorter walls balance noise benefits with appropriate scaling for a residential setting. This design option avoided effects that could adversely affect the NRHP integrity of the historic district or any of its contributing elements.

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3.2.12 7500 Broadview Drive (5JF4537)

Site Description

The building at 7500 Broadview Drive is a single-family Ranch-style residence that was built in 1947 in the NRHP-eligible Green Acres Historic District (Exhibit 48). It is located at the west end of Broadview Drive, and the principal façade and entrance both face north toward Broadview Drive.



EXHIBIT 48
7500 Broadview Drive, view to the south from Broadview Drive

Eligibility Determination

This residence is a contributing resource to the NRHP-eligible Green Acres Historic District because it is a good example of a 1947 Ranch-style single-family house that was common to the post-World War II architectural styles of the Green Acres neighborhood. The property also displays the deep setbacks and landscape features, such as broad grassy lawns and mature trees, that are historic characteristics of the subdivision. The property retains good physical and historic integrity to its original construction, and contributes to the setting, feeling, and association of the Green Acres neighborhood as a whole.

Effect Determination

7500 Broadview Drive would be directly impacted by the Proposed Action. Exhibit 49 illustrates the direct effects to this property. The noise wall would cut through the tip of the southwest corner of the property. The total area affected would be approximately 180 square feet, and the noise wall would be approximately 28 feet from the western façade of the building. (The roadway slope line shown in green in Exhibit 49 would be replaced by a retaining wall so that

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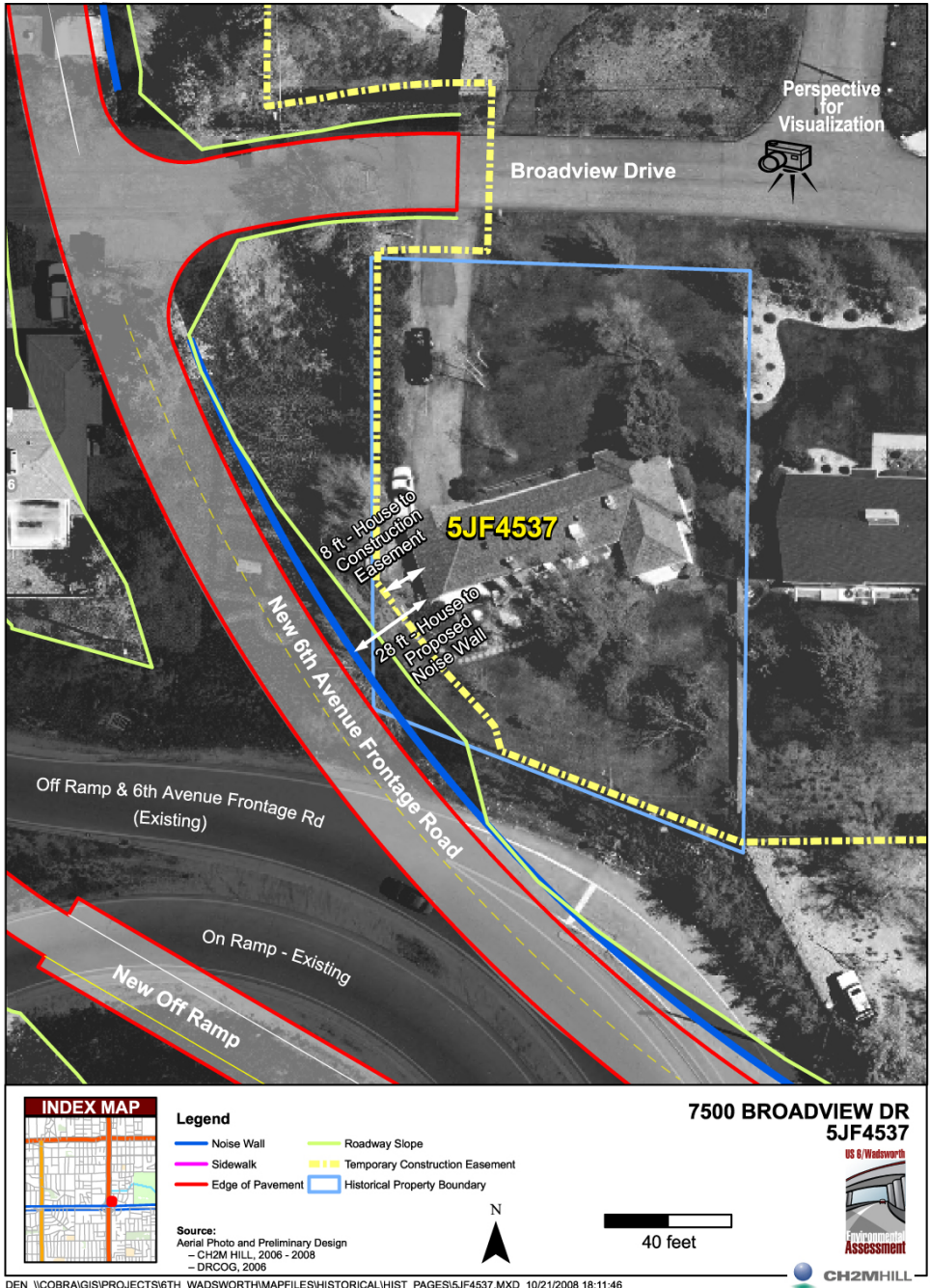


EXHIBIT 49
Effects to 5JF4537, 7500 Broadview Drive

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the slope does not extend into the yard as shown in the exhibit.) This acquisition would have a negligible effect on the property. It represents only a fraction of the total parcel area, affects only the tip of the corner of the property, does not affect the building or its primary back or front yard landscaping or setting, and does not affect the building. The impact to the corner of the property does not affect historic features of the building and, therefore, does not negatively affect the property's NRHP eligibility.

In addition to the small property acquisition, visual analysis determined that the Preferred Alternative could have a minor visual impact to this property. Noise walls would be constructed at the rear and side of the property, but the walls would not be visually intrusive as they would be set back from the historic property, would be shorter than the walls along the frontage road (of 15 feet), and would blend into the setting like privacy fences. Exhibit 50 illustrates the visual effect anticipated for this property. The perspective for the visualization is shown by the camera symbol in Exhibit 49.



EXHIBIT 50

Relationship of proposed noise wall to 7500 Broadview Drive, view to the southwest from Broadview Drive; existing and projected future conditions (left and right, respectively)

This property is currently screened from Wadsworth by existing residential structures and trees that would be removed under the Proposed Action. The proposed sound barrier wall would replace the visual screen from Wadsworth that the removed properties currently provide, and would provide better noise screening than currently exists.

The construction of a sound wall near the rear, western edge of the property boundary would have only a minor effect to the historic property because of its rear/side location and lower height, which would have an effect similar to a residential privacy fence. The wall would not block an important viewshed from the property, and would be barely visible from the principal façade or from Broadview Drive, which the house faces. As illustrated in Exhibit 50, the wall would not be visually intrusive even from the side of the property.

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This property is eligible under Criterion C for its architectural significance, and neither the direct impact to the surrounding property nor the indirect visual effects will diminish the qualities that make this property eligible to the NRHP, and therefore, will result in **No Adverse Effect**. As discussed with the district as a whole, the sound wall does not adversely affect the integrity of the historic district or its contributing elements.

Avoidance Options

CDOT is incorporating a retaining wall to minimize the encroachment of the roadway slope onto the 7500 Broadview Drive property. The resulting 179 square feet impact area represents a minor acquisition. CDOT also considered not installing a noise barrier in this location, but the noise benefits were substantial and outweighed the effects of the minor property acquisition. As noted previously, CDOT included shorter walls to balance the noise benefits and the residential character of the neighborhood.

3.2.13 705 Emerald Lane (5JF4529)

Site Description

The building at 705 Emerald Lane is a Split-Level single-family residence that was built in 1961 (Exhibit 51). The property is located on a corner lot at Emerald Lane and Broadview Drive. The house's principal façade and entrance are oriented at an angle facing southeast.



EXHIBIT 51
705 Emerald Lane, view to the northwest from Broadview Drive

Eligibility Determination

This residence is a contributing resource to the NRHP-eligible Green Acres Historic District because it is a good example of a 1961 Split-Level single-family house that was common to the post-World War II architectural styles of the Green Acres neighborhood. The property also

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displays the deep setbacks and landscape features, such as broad grassy lawns and mature trees, that are historic characteristics of the subdivision. The property retains good physical and historic integrity to its original construction, and contributes to the setting, feeling, and association of the Green Acres neighborhood as a whole.

Effect Determination

Although 705 Emerald Lane would not be directly impacted by the Proposed Action, visual analysis determined that the Proposed Action would cause an indirect visual impact to this property due to the construction of sound barrier walls near its western and southwestern boundaries. Unlike the other sound barriers proposed along US 6, the sound barriers in the northeast quadrant of the interchange are proposed to be shorter (approximately 8 to 9 feet) to be more similar in character to a privacy fence. Because of grade differences, the visual effect is further minimized, as displayed in Exhibit 52.

This property is currently screened from Wadsworth by existing residential structures and trees that would be removed under the Proposed Action. The proposed sound barrier wall would replace the visual screen from Wadsworth that the removed properties currently provide, and would provide better noise and visual screening from Wadsworth traffic than currently exists.



EXHIBIT 52

Visualization of proposed noise wall between 705 Emerald Lane and 715 Emerald Lane, view to the west from Emerald Lane

The construction of sound walls near the rear edge of the property boundary would result in **No Adverse Effect** because the visual impact of the sound walls is relatively minor due to their location at the rear of the property, distance from the property boundary, and low height, which would have a visual effect similar to a residential privacy fence. The wall would not block an important viewshed from the property. As discussed with the district as a whole, the sound wall does not adversely affect the integrity of the historic district or its contributing elements.

Avoidance Options

As noted previously, CDOT included shorter noise walls to balance the noise benefits with the visual effects of noise walls on the residential neighborhood setting.

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3.2.14 715 Emerald Lane (5JF4530)

Site Description

The structure at 715 Emerald Lane is a single-family Ranch-style residence that was built in 1953 in the NRHP-eligible Green Acres Historic District (Exhibit 53). It is located at the west side of Emerald Lane, and the principal façade and entrance both face east toward Emerald Lane.



EXHIBIT 53
715 Emerald Lane, view to the west from Emerald Lane

Eligibility Determination

This residence is a contributing resource to the NRHP-eligible Green Acres Historic District because it is an example of a 1953 Ranch-style, single-family house that was common to the post-World War II architectural styles of the Green Acres neighborhood. The property also displays the deep setbacks and landscape features, such as broad grassy lawns and mature trees, that are historic characteristics of the subdivision. The property retains good physical and historic integrity to its original construction, and contributes to the setting, feeling, and association of the Green Acres neighborhood as a whole.

Effect Determination

Although 715 Emerald Lane would not be directly impacted by the Proposed Action, visual analysis determined that the Proposed Action could cause an indirect visual impact to this property similar to the effect for 705 Emerald Lane described above and illustrated in Exhibit 52. As with 705 Emerald Lane, the sound walls would be located near the rear, western edge of the property boundary, would be barely visible from the property's front entrance, and would

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result in **No Adverse Effect**. As discussed with the district as a whole, the sound wall does not adversely affect the integrity of the historic district or its contributing elements.

Avoidance Options

As noted previously, CDOT included shorter noise walls to balance the noise benefits with the visual effects of noise walls on the residential neighborhood setting.

3.2.15 Meadowlark Hills Historic District

Site Description

The NRHP-eligible Meadowlark Hills Historic District is a post-World War II residential subdivision of single-family residences. The district includes 10 contributing buildings within the APE: 545 Meadowlark Drive (5JF4590), 540 Meadowlark Drive (5JF4591), 555 Everett St. (5JF4592), 560 Everett St. (5JF4593), 585 Estes St. (5JF4594), 580 Estes St. (5JF4595), 585 Dudley St. (5JF4596), 580 Dudley St. (5JF4597), 585 Dover St. (5JF4598), and 590 Dover St. (5JF4599) (Exhibit 54). The neighborhood is located west of Wadsworth Boulevard and south of US 6 and the West 6th Avenue Frontage Road. The 10 properties that are included in the APE are the parcels that run along north edge of the Meadowlark Hills Historic District and neighborhood. While the entrances to these properties face east or west, the sides of their properties face north toward West 6th Avenue Frontage Road. They are located along Dover Street, Dudley Street, Everett Street, Estes Street, and Meadowlark Drive, each of which intersects with West 6th Avenue Frontage Road, which forms the northern boundary of the Meadowlark Hills Historic District.

Constructed between 1953 and 1956, Meadowlark Hills is a master-planned development that includes curvilinear, tree-lined streets, parks, and a community shopping center. The Meadowlark Hills Subdivision was a planned development created in 1953 when the Meadowlark Land Corporation purchased the property from Charles T. Gore and Daniel J. Kellogg. Extensive primary research including newspaper archives and historical and municipal repositories identified the subdivision developers as the Meadowlark Land Company, the Meadowlark Development Company, House Beautiful Homes, Inc., but it did not identify contractors or architects associated with the construction. Although the entire neighborhood was not surveyed as part of this survey, a windshield survey through the neighborhood identified approximately six or seven house plans or models of either Ranch style or Split-Level style houses, each displaying strong uniformity in architectural design and materials. All of the buildings within the Meadowlark Hills subdivision are faced with a combination of blonde, red, and brown brick that the contractor purchased from the local Lakewood Brick and Tile Company located along 13th Avenue (Norman, 2002: 31-32). The consistency in building materials and housing styles bestows strong visual and architectural consistency to the neighborhood as a whole.

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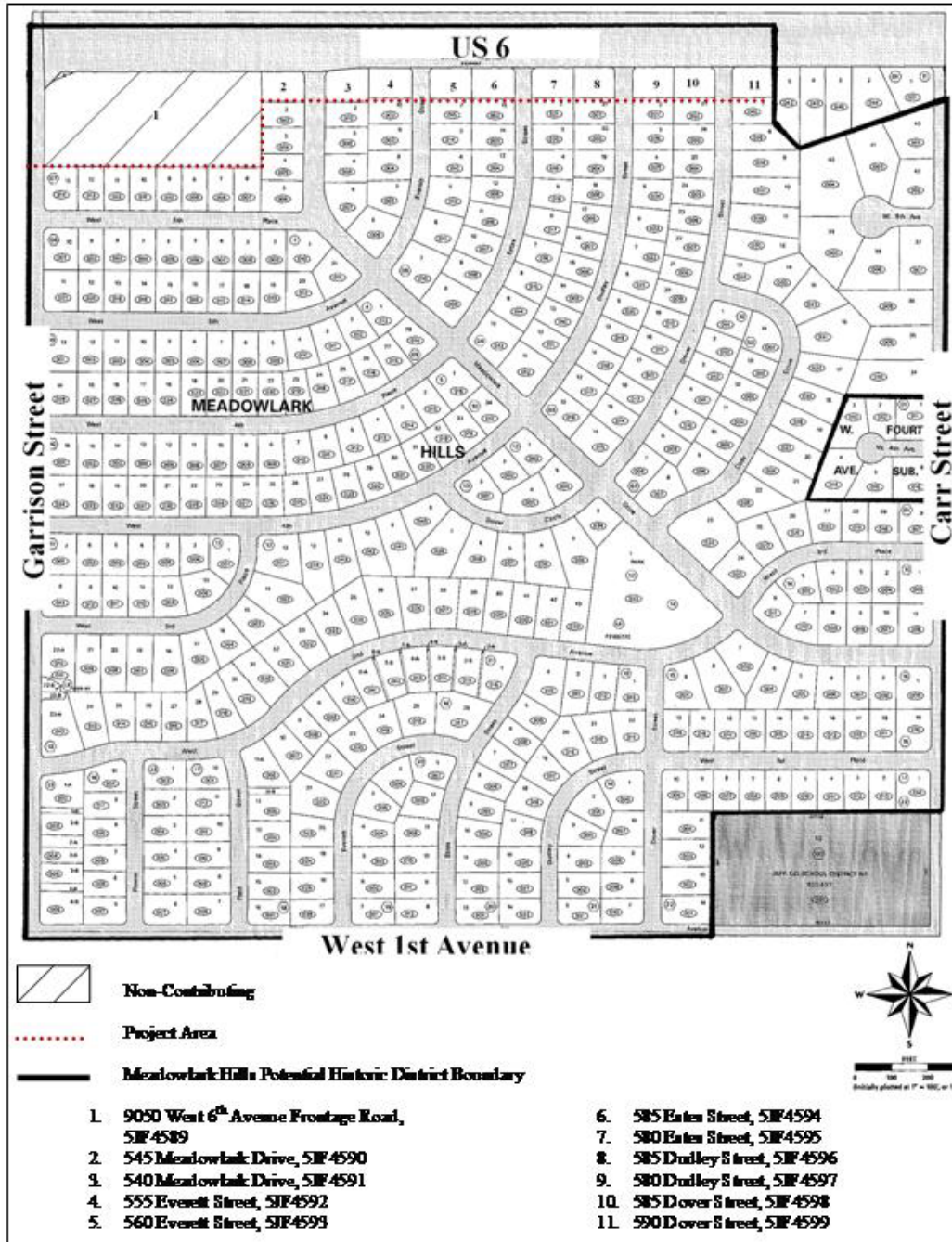


EXHIBIT 54
Meadowlark Hills Historic District

Eligibility Determination

The Meadowlark Hills Subdivision is eligible under Criterion A for the neighborhood's establishment in Lakewood during the 1950s. The neighborhood's development during this

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time reflects an important historic trend in suburban growth within the Denver metropolitan area during the Post World War II era, and is an example of a movement that led to the creation of Lakewood as an independent city in 1969. Meadowlark Hill's establishment during the mid-1950s also coincides with the opening of the nearby Denver Federal Center in 1950. When the Federal Government converted the former DOP into the Federal Center, the new federal campus became a regional center of government operations and employment. Its opening helped revitalized Lakewood as a residential suburb providing middle-class housing to Federal Center employees.

The Meadowlark Hills Subdivision is eligible under Criterion C as an important example of post-World War II neighborhood planning and design principles. Many of the development's features, such as the curvilinear streets and uniform setbacks were design elements recommended by the Federal Housing Administration as early as the 1930s, and set the standard of post-World War II subdivisions nationwide. Moreover, the housing's consistent informal Ranch Style and Split-level architecture reflects an attempt made by developers to appeal to the consumer during this period of increasing popularity for these trends during the 1950s. Picture windows, low, horizontal profiles, and horizontally oriented bands of windows became distinguishing features of both domestic styles. These elements also were well-adapted to middle class suburban developments, such as Meadowlark Hills (Ames and McClelland 2002, 49, 66). Meadowlark Hills is an excellent example of the execution of this suburban model within the greater Denver metropolitan area.

The Meadowlark Hills subdivision maintains a remarkably high degree of historic physical integrity. Unlike many other residential subdivisions of its era, a windshield survey through the neighborhood revealed little to no evidence of demolition, alterations to the front of the house, or other large-scale alterations. The most common alteration to the housing appears to be replacement of original metal-framed windows; however, this renovation is not universal and a large degree of original fenestration appears to remain intact throughout the neighborhood.

The period of significance for the NRHP-eligible Meadowlark Hills Historic District is the neighborhood's period of construction, 1953-1956. The district boundaries encompass the neighborhood's construction limits, as completed in 1956. This area is generally bound by West 6th Avenue/Frontage Road to the north, Carr Street to the east, West 1st Avenue to the south, and Garrison Street to the west. Excepted areas within this area but outside of the boundary include a group of six properties on West Fourth Avenue that were built in the mid-1960s, nearly 10 years after Meadowlark Hills was completed. Additionally, the school property at the southeast corner of the neighborhood, South Lakewood Elementary, was constructed in 1994 and, therefore, is not included within the historic district boundary.

Effect Determination

The effects of the reconstruction of the US 6 and Wadsworth Boulevard interchange would tie into existing US 6 significantly east of the Meadowlark Hills Historic District, as illustrated in Exhibit 55.

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EXHIBIT 55
Relationship of roadway improvements to the Meadowlark Hills Historic District

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The 10 contributing buildings that could experience indirect visual impacts from the construction of the sound barrier walls are located at the northern edge of the NRHP-eligible Meadowlark Hills Historic District. These properties are all eligible because they are intact, Ranch- or Split-Level-style, single-family houses representing one of the six post-World War II architectural styles of the Meadowlark Hills subdivision. Uniform neighborhood setbacks and landscape features, such as a grassy front lawn, are historic characteristics of the subdivision. Photographs of these properties are included in Exhibit 56. These homes face east-west, and US 6 is seen in the background (to the north).



545 Meadowlark Drive (5JF4590)



540 Meadowlark Drive (5JF4591)



560 Everett St. (5JF4593)



555 Everett St. (5JF4592)



580 Estes St. (5JF4595)



585 Estes St. (5JF4594)



580 Dudley St. (5JF4597)



585 Dudley St. (5JF4596)



590 Dover St. (5JF4599)



585 Dover St. (5JF4598)

EXHIBIT 56

Contributing Resources within the APE in the Meadowlark Hills Historic District

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The Proposed Action would construct a sound wall on CDOT's ROW between the West 6th Avenue Frontage Road and US 6 (adjacent to US 6), north of the Meadowlark Hills Historic District. This undertaking would not directly affect the Meadowlark Hills Historic District or any of the 10 contributing resources within the APE. The orientation of each of the houses within the district also precludes them from indirect visual impacts. Each house faces east or west, while noise walls would be installed north of the district boundary on the opposite side of the two-lane roadway of West 6th Avenue Frontage Road. (Odd numbered houses face east, and even numbered houses face west.) There would be no visual impact to the principal façades of these historic properties, and the change in the street view from these properties out to US 6 (looking north) would not change substantially from the current views. Instead of viewing a jersey barrier or guard rail, the view would be of a taller wall. The wall would block the view of traffic on US 6, but this view does not contribute to the historic setting of the subdivision. Moreover, a variety of trees and shrubbery obscure the views from many of the houses to the site of the proposed sound barriers. As illustrated in Exhibit 57, the sound walls would be located a distance from the residences and would not be visually intrusive.



EXHIBIT 57

Rendering of proposed sound walls at 545 Meadowlark Drive (top) and 580 Estes St. (bottom) – before and after conditions (left and right, respectively)

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There would be a significant reduction in noise at these residences from the installation of sound barriers. All of the properties within the APE in this district would experience more than a 10 dB reduction in noise, resulting in nearly half the noise of the existing condition. This reduction in noise would be a beneficial indirect effect to the historic properties within the district. Other properties within the historic district outside of the APE would also benefit from noise reduction.

There would be no visual impact to the principal façades of any of the houses, there would be no change in the street view from the houses, and the view to US 6 at the edges of the houses would not change substantially from the current view. This new wall would block the view of traffic on US 6, but this view shed does not contribute to the historic setting of the NRHP-eligible Meadowlark Hills subdivision. The construction of the sound wall will, therefore, result in **No Adverse Affect** to the historic district or any of its contributing elements.

Avoidance Options

Because neither the historic district nor any of its contributing elements are adversely affected, no avoidance options were necessary.

3.3 Mitigation

During the development of the Proposed Action, other options were considered in order to avoid or minimize effects to historic resources wherever possible. The end result was the selection of the Proposed Action that avoids as many historic resources as is feasible. Adverse Effects for specific historic structures and sites under the Proposed Action are discussed in Section 3.2 above.

Mitigation measures will be part of a Memorandum of Agreement (MOA) among CDOT, FHWA, and the Colorado SHPO, and will be specific to those resources for which the project results in an Adverse Effect. CDOT proposes Colorado Historical Society Office of Archaeology and Historic Preservation (OAHP) Level 2 documentation be provided for all adversely affected properties. Other mitigation measures, if necessary, will be negotiated during the development of the MOA.

According to *CDOT Standard Specifications* (CDOT, 1999), in the event that cultural deposits are discovered during construction within the APE, work will cease in the area of discovery and the CDOT archaeologist will be notified. The CDOT archaeologist or a designated representative will evaluate any such discovery and, in consultation with SHPO, complete proper mitigation measures before construction activities resume. As noted above, agreed-upon mitigation measures will be formalized in an MOA.



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4.0 Conclusion

The *Historic Resources Survey: US 6 and Wadsworth Boulevard, Lakewood, Colorado* (June 2008) recorded 151 historic properties built prior to 1966 and found nine properties officially eligible for individual listing in the NRHP. Additionally, 39 resources are officially eligible as contributing resources to three different recommended historic districts. The NRHP-eligible Lakewood School Historic District consists of seven contributing buildings and two contributing landscape features. The NRHP-eligible Green Acres Historic District consists of 20 contributing buildings. The NRHP-eligible Meadowlark Hills Historic District consists of 10 contributing buildings. Of these 48 total historic properties (nine individual and 39 contributing), the Proposed Action would directly adversely affect four properties.

All of the four directly, adversely impacted properties under the Proposed Action are residences or converted residences that are located along West 6th Avenue Frontage Road in the northeast quadrant of the interchange. All four of the properties, 7395 6th Ave. Frontage Road (5JF3548) (house), 7423 6th Ave. Frontage Road (5JF3549) (house), 7433 6th Ave. Frontage Road (5JF4542) (house), and 700 Wadsworth Blvd. (5JF4536) (construction business in a converted residence), would be acquired. All of the buildings on these four properties, including garages and outbuildings if present, would be removed under the proposed undertaking. As documented in this report, CDOT considered numerous options to avoid effects to these properties but no feasible options are available that would meet the project's purpose and need and maintain the properties in place.

The Green Acres Historic District as a whole and three contributing properties (715 Emerald Lane [5JF4530], 705 Emerald Lane [5JF4529], and 7500 Broadview Drive [5JF4537]) would not be adversely affected by the Proposed Action. Although the edges of the historic district are adjacent to roadway changes, the majority of homes within the district are distant from the reconfigured roadways and sound walls; the Proposed Action has a beneficial effect to traffic noise and volumes through the neighborhood. The three contributing properties closest to the sound wall would have minor indirect visual impacts due to the construction of a sound barrier wall at the rear and/or side edges of each of these properties. One property (5JF4537) would also require a minor acquisition of a corner of the property. These indirect and direct impacts will not diminish the qualities that make these properties eligible to the NRHP, and therefore, the Proposed Action will result in **No Adverse Effect** to these properties.

One residential property located along Wadsworth Boulevard (1215 Wadsworth Blvd.) will be directly impacted by the Proposed Action because a portion of the contributing historic landscape will be removed from the property. The proposed changes to the property's historic landscape will be minimal, and will not diminish the qualities that make this property eligible



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to the NRHP. The Proposed Action was modified to preserve the large tree in front of the property. There will be **No Adverse Effect** to this property.

A portion of property would be required from the Lakewood School Historic District and from 1230 Wadsworth Blvd. In both cases, the portion of property that would be acquired does not contribute to the historic significance of the property, and, therefore, there is **No Adverse Effect** to these properties as a result of the Proposed Action.

The Meadowlark Hills Historic District is located away from direct improvements but will be affected by the installation of a sound wall near the northern end of the district. There are 10 contributing resources along the northern end of the district that are within the APE. None of these properties will be directly adversely affected, as the primary facades of the houses are oriented away from the noise wall, there is a long distance between the noise wall and the potentially affected properties, and there are no important view sheds that would be blocked by the noise wall. There would be a benefit to the neighborhood and the residential setting of the historic district from the reduction in traffic noise provided by the wall. There is **No Adverse Effect** to the Meadowlark Hills Historic District or any of its contributing resources.



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December 2008

5.0 References

Ames, David L. and Linda Flint McClelland, 2002. *Historic Residential Suburbs: Guidelines for Evaluation and Documentation for the National Register of Historic Places*. Prepared for the U.S. Department of Interior, National Park Service, National Register of Historic Places. September.

Colorado Department of Highways, 1960. As-built drawing, *Plan and Profile of Proposed Colorado Project Number C-11-0121-26*. State Highway No. 121, Jefferson County, 11-4-1960.

Center for Historic Preservation Research, 2006. *Database of the Annual Denver Area Parade of Homes*. Prepared for the Office of Archaeology and Historic Preservation, Colorado Historical Society.

Colorado Department of Transportation (CDOT), 1999. *Standard Specifications for Road and Bridge Construction*, pp. 94-95.

Norman, Cathleen M. (Preservation Publishing), 2002. *Survey Report for the 2001 Cultural Resource Survey of Historic Northeast Lakewood*. Prepared for the Two Creeks Neighborhood Association, Edgewood Neighborhood Association, City of Lakewood, and Colorado Historical Society. May 31.

TEC, 2008. *Historic Resources Survey: US 6 and Wadsworth Boulevard, Lakewood, Colorado*. Prepared for the Colorado Department of Transportation. July.



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Appendix – Section 106 Correspondence



FINAL Determination of Effects to Historic Properties

December 2008

STATE OF COLORADO

DEPARTMENT OF TRANSPORTATION



Region 6
2000 South Holly Street
Denver, Colorado 80222
720-497-6950

July 20, 2007

Ms. Amy Pallante
Section 106 Coordinator
State Historic Preservation Office
1300 Broadway
Denver, CO 80203

Subject: Invitation to Participate in Agency Scoping Meetings for the US 6 and Wadsworth Environmental Assessment – August 16, 2007, 10:30 a.m. to 12:00 p.m.

Dear Ms. Pallante:

On behalf of the Colorado Department of Transportation (CDOT) and the Federal Highway Administration (FHWA), I would like to invite you to an informational scoping meeting for the US 6 (6th Avenue) and Wadsworth Boulevard Environmental Assessment (EA) on **August 16, 2007** from [Session]. CDOT and FHWA are preparing this EA in accordance with the National Environmental Policy Act (NEPA) and the Council on Environmental Quality (CEQ) and FHWA NEPA-implementing regulations (Title 40 of the Code of Federal Regulations [CFR], Parts 1500-1508 [40 CFR 1500-1508] and 23 CFR 771, respectively). The Regional Transportation District (RTD) is a cooperating agency for the EA, and the City of Lakewood is a partnering agency.

In the EA, CDOT will evaluate transportation improvements for the US 6 and Wadsworth Boulevard interchange and for Wadsworth Boulevard between approximately 3rd Avenue and 13th Avenue. CDOT, the City of Lakewood, and area residents have identified these transportation improvements as among the highest priorities in the west metro area. The US 6 and Wadsworth Boulevard interchange is a gateway to Lakewood's downtown (Belmar) and city center (Lakewood Commons) areas. Wadsworth Boulevard is a highly traveled commuter and bus transit corridor and the only continuous north-south through street in the west metro area.

CDOT is collecting data to better characterize transportation and infrastructure needs and the environmental resources present in the project area. Enclosed is a short project fact sheet. We will distribute a Scoping Packet with more information the week of August 6, 2007. Some of the transportation issues we must address include traffic congestion, neighborhood and business access, poor interchange operations, high accident rates, undersized drainage facilities, and inadequate bicycle and pedestrian facilities. We will be challenged to develop transportation improvements while minimizing community and environmental impacts.

topics of interest and focus discussion. We hope that this format will allow you to have more time to discuss the issues of importance to your agency and to interact with other agencies with similar interests. The following is the schedule of meeting times, invited agencies, and topics. We have included State Historic Preservation Office in the 10:30 to 12:00 a.m. session. Please note that the information that CDOT will present at these sessions will be identical, but it is anticipated that discussion will be targeted to the areas of interest listed for the sessions. CDOT, FHWA, RTD, and the City of Lakewood plan to participate in each session.

Schedule of Agency Scoping Meetings, US 6 and Wadsworth EA

Time	Invited Agencies	Topics of Interest
8:30 a.m. to 10:00 a.m.	Colorado Department of Local Affairs Colorado State Parks Denver Regional Council of Governments Department of Housing and Urban Development Federal Transit Administration Jefferson County Open Space Jefferson Economic Council U.S. Environmental Protection Agency	Social and Community Resources <ul style="list-style-type: none"> • Bicycles/Pedestrians • Cumulative Impacts • Environmental Justice • Land Use • Public Involvement • Right of Way • Socioeconomics • Visual/Aesthetic Considerations
10:30 a.m. to 12:00 p.m.	Colorado Department of Public Health and Environment, Air Pollution Control Division Colorado Department of Public Health and Environment, Hazardous Materials and Waste Division Department of Interior, Office of Environmental Policy and Compliance Jefferson County Department of Health and Environment Jefferson County Highways and Transportation Department Regional Air Quality Council State Historic Preservation Office U.S. Environmental Protection Agency	Human and Built Environment <ul style="list-style-type: none"> • Air Quality • Archaeology • Hazardous Materials • Historic Resources • Noise • Paleontology • Safety • Section 4(f) / 6(f) • Traffic
1:00 p.m. to 2:30 p.m.	Colorado Division of Wildlife Federal Emergency Management Agency U.S. Environmental Protection Agency U.S. Fish and Wildlife Service Urban Flood and Drainage Control District U.S. Army Corps of Engineers	Natural Environment <ul style="list-style-type: none"> • Floodplains • Hydraulics • Noxious Weeds • Stormwater • Threatened and Endangered Species • Vegetation • Water Quality • Wetlands • Wildlife / Fisheries

CDOT and FHWA are committed to completing this EA in 12 to 24 months. In order to meet a streamlined schedule, we need your help in scoping to identify all of the transportation and environmental issues that need to be addressed in the EA process. Our consultant, CH2M HILL, will be visiting your offices to drop off a Scoping Packet and answer any questions about the upcoming

meetings. Your participation is very important. Please RSVP to Ms. Mandy Whorton, CH2M HILL Environmental Manager, by Tuesday, August 14, 2007, if you plan to attend and/or have any questions about the meetings.

If this project does not require involvement of your agency, please return the enclosed form to let us know not to follow up with you about your participation.

I and the rest of the project team look forward to seeing you on August 16, 2007. Please do not hesitate to contact me at (720) 497-6955 or Ms. Whorton at 720-286-5239 if you have any questions or concerns.

Sincerely,



Seyed Kalantar, P.E
CDOT Region 6 Project Manager

c: Kirk Webb, CDOT Project Environmental Manager
Mandy Whorton, CH2M HILL Project Environmental Manager
Project Administrative Record



**COLORADO
HISTORICAL
SOCIETY**

The Colorado History Museum 1300 Broadway Denver, Colorado 80203-2137

August 7, 2007

Kirk Webb
Project Environmental Manager
CDOT, Region 6
2000 South Holly Street
Denver, CO 80222

Re: US 6 and Wadsworth Environmental Assessment Scoping Meeting. (CHS #50636)


Dear Mr. Webb,

Thank you for your correspondence dated July 25, 2007 and received by our office on July 26, 2007 regarding the review of the above-mentioned project under Section 106 of the National Historic Preservation Act (Section 106).

Unfortunately, our office is not able to attend the scoping meeting scheduled for August 16, 2007 for this project. We would like to use this letter to recommend that CDOT begin the Section 106 process as early in the planning process as possible. We have included a flow chart that illustrates the relationship between NEPA and Section 106. We recommend identifying consulting parties during the scoping stage so that you may gather their comments earlier in the process. Once the consulting parties are determined, we recommend initiating consultation with our office and the other consulting parties regarding an appropriate Area of Potential Effects (APE) for the project.

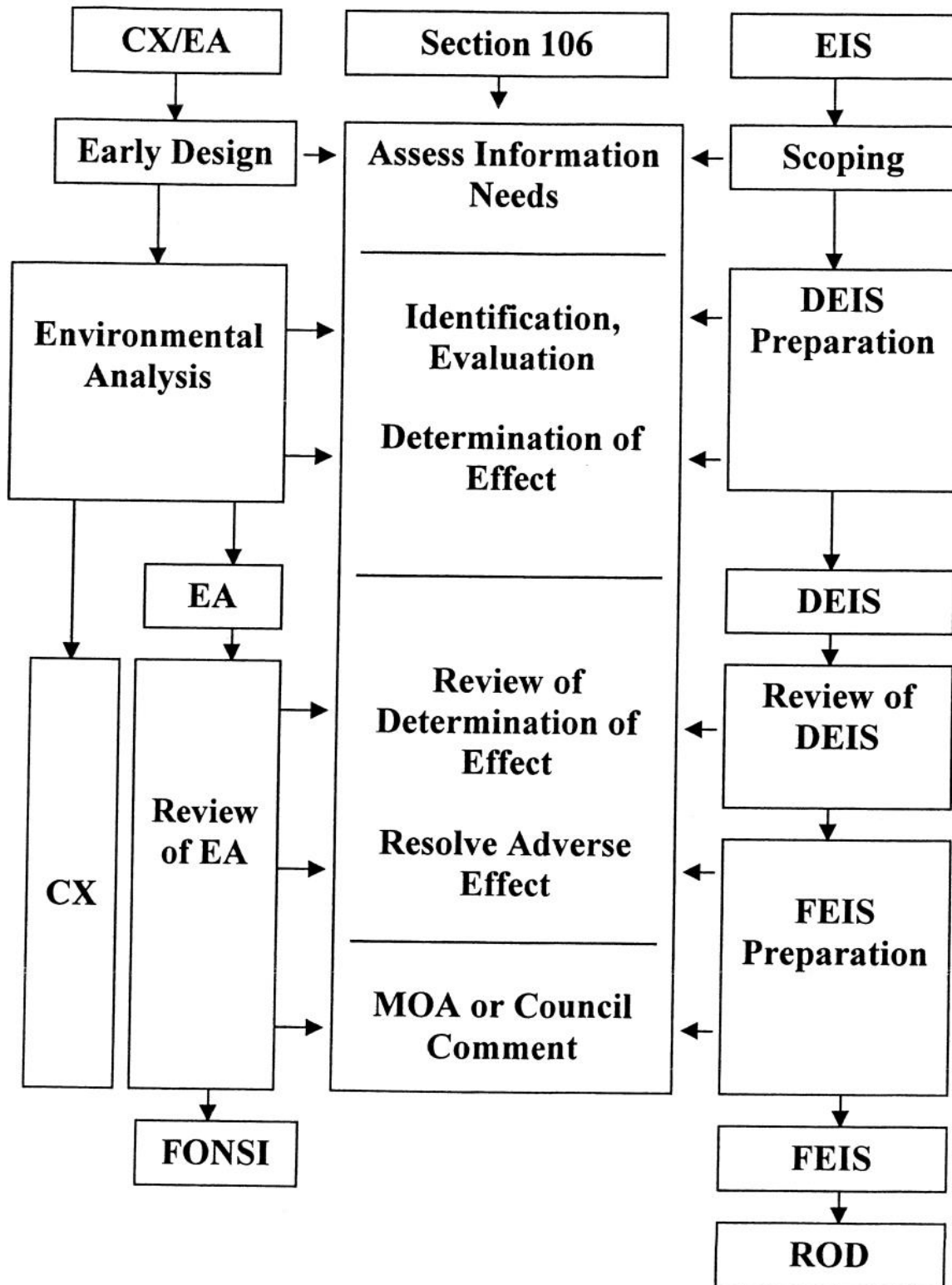
We apologize for not being able to attend the meeting, but hope the above comments will aid during the scoping process. If we may be of further assistance, please contact Amy Pallante, our Section 106 Compliance Coordinator, at (303) 866-4678.

Sincerely,

For 
Georgianna Contiguglia
State Historic Preservation Officer

Cc: Dianna Litvak /CDOT Region 6

COORDINATION BETWEEN NEPA AND SECTION 106



The Public and Consulting Parties must be notified and given the opportunity to comment during each step of the Section 106 review process.



U.S. Department of
Transportation

Route Slip

Distribution:

To: Name	Date	Org/Rtg Symbol
See Below (Re:6th Ave/Wadsworth Blvd)	9/17/2007	

Remarks:

To: K. Webb, D. Jepson, CDOT
 ✓ M. Whorton, CH2M Hill
 Marcee Allen

- Per Your Request
- For Your Information
- Per Our Conversation
- Note and Return
- Discuss With Me
- For Your Approval
- For Your Signature
- Comment
- Take Appropriate Action
- Please Answer
- Prepare Reply For Signature Of

Attached please find a copy of a letter addressed to A. Chalepha, a copy of a Tribal Mailing List and a map for the above-mentioned project. Letters were mailed to M. Chalepah as well as the other individuals on the attached list, with appropriate copies, on September 17

From: Name	Telephone	Org/Rtg Symbol
Joyce Curtis, FHWA	(720) 963-3002	



U.S. Department
of Transportation
**Federal Highway
Administration**

**Colorado Federal
Aid Division**

12300 W. Dakota Ave.
Suite 180
Lakewood, CO 80228

September 14, 2007

Mr. Alonzo Chalepah, Chairman
Apache Tribe of Oklahoma
P.O. Box 1220
Anadarko, OK 73005

Dear Mr. Chalepah:

Subject: Request for Section 106 Consultation; 6th Avenue/Wadsworth Blvd.
Interchange Environmental Assessment, Jefferson County, Colorado

The Federal Highway Administration (FHWA) and Colorado Department of Transportation (CDOT), in cooperation with the Regional Transportation District (RTD) and City of Lakewood, are preparing an Environmental Assessment (EA) that will address the effects of proposed improvements to the 6th Avenue (US Highway 6) Wadsworth Boulevard interchange in Jefferson County, Colorado. The proposed study area includes an approximate one-mile segment of Wadsworth Blvd. (4th to 14th) and the entire interchange at 6th Avenue. In order to improve local transportation movements along this major urban corridor and gateway to the City of Lakewood, the project proposes reconstruction of the interchange and widening of Wadsworth to add additional travel lanes and sidewalks. Pursuant to the National Environmental Policy Act (NEPA) of 1969 and the Council on Environmental Quality (CEQ) implementing regulations (40 CFR 1500-1508), FHWA and CDOT are documenting the potential social, economic and environmental consequences of this action. Please refer to the enclosed aerial map for a view of the project study area.

FHWA will serve as the lead agency for this undertaking, and CDOT staff will facilitate the tribal consultation process. The agencies are seeking the participation of regional Native American tribal governments in cultural resources consultation for the undertaking, as described in Section 106 of the National Historic Preservation Act and implementing regulations 36 CFR 800 et seq. As a consulting party, you are offered the opportunity to identify concerns about cultural resources and comment on how the project might affect them. Further, if it is found that the project will impact cultural resources that are eligible for inclusion on the National Register of Historic Places and are of religious or cultural significance to your tribe, your role in the consultation process would include participation in resolving how best to avoid, minimize, or mitigate those impacts. It is our hope that by describing the proposed undertaking we can be more effective in protecting areas important to American Indian people. If you have interest in this undertaking and in cultural resources that may be of religious or cultural significance to your tribe, we invite you to be a consulting party.

**MOVING THE
AMERICAN
ECONOMY**

As shown on the enclosed map, the project corridor is located in a heavily urbanized section of Lakewood; no areas exist that have not been extensively disturbed for many decades by residential and commercial development. The Study Area as shown on the map extends several city blocks on either side of Wadsworth; however, this boundary does not necessarily reflect the Area of Potential Effect (APE) to be developed for cultural resource studies, as defined by 36 CFR 800.16(d). A comprehensive survey and assessment of historic properties in the APE will be conducted as part of the environmental documentation. Tribes that elect to become consulting parties for the undertaking will be notified of the results of the survey and asked to comment on our eligibility and effects determinations. Any information you may have regarding places or sites important to your tribe, that are located within or near the project area, would assist us in our efforts to comprehensively identify and evaluate cultural resources.

The metropolitan Denver area is home to a number of American Indian residents. If you are aware of members of your tribe living in proximity to the study area who would be interested in participating in the NEPA consultation process on some level, please notify us so that we may facilitate that interaction.

We are committed to ensuring that tribal governments are informed of and involved in decisions that may impact places with cultural significance. If you are interested in becoming a consulting party for the 6th Ave./Wadsworth EA, please complete and return the enclosed Consultation Interest Response Form to CDOT Native American consultation liaison Dan Jepson **within 60 days** at the address or facsimile number listed at the bottom of that sheet. Mr. Jepson can also be reached via Email at daniel.jepson@dot.state.co.us, or by telephone at (303)757-9631. The 60-day period has been established to encourage your participation at this early stage in project development. Failure to respond within this time frame will not prevent your tribe from becoming a consulting party at a later date. However, studies and decision-making will proceed and it may become difficult to reconsider previous determinations or findings, unless significant new information is introduced.

Thank you for considering this request for consultation.

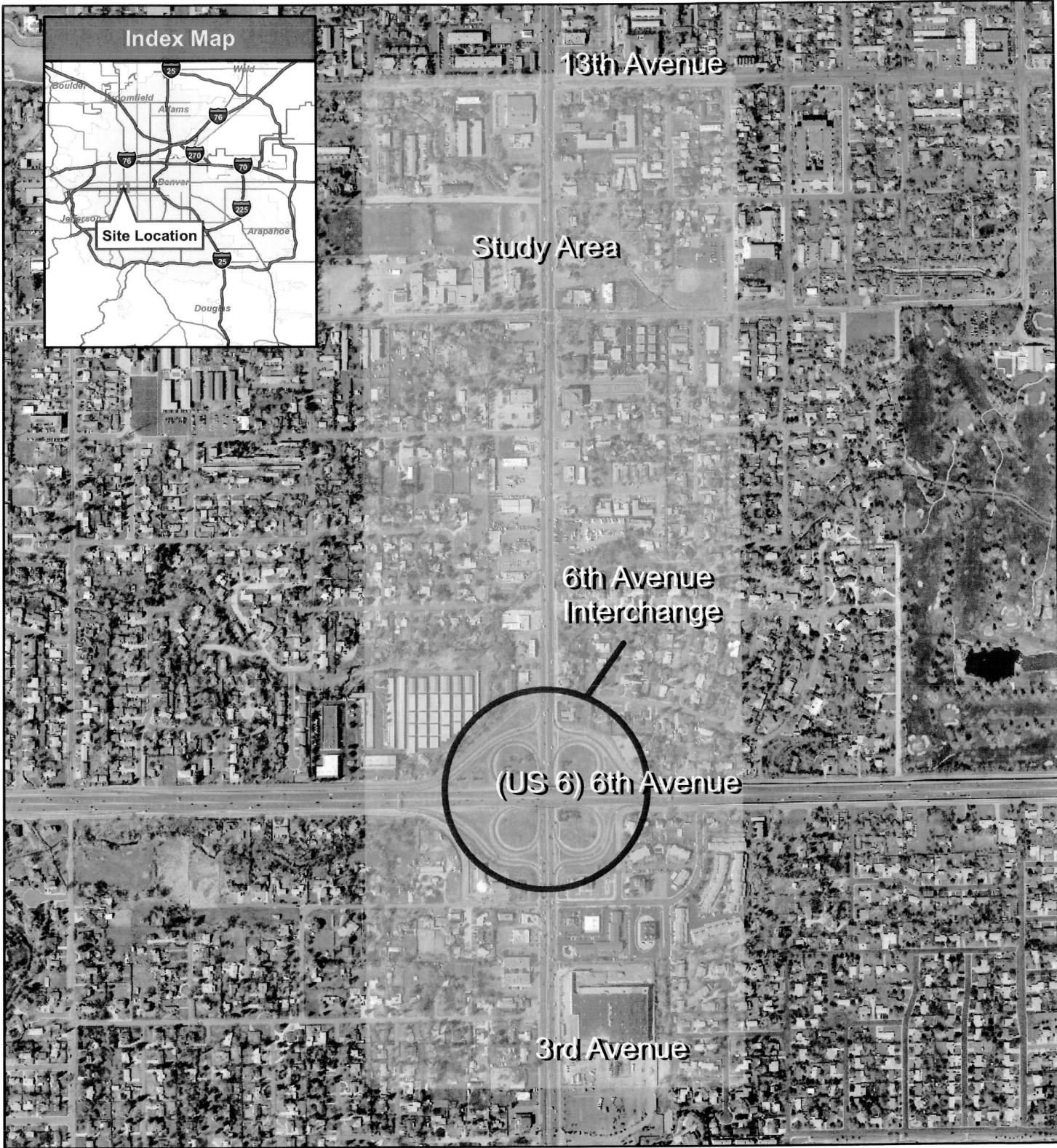
Sincerely yours,



for David A. Nicol, P.E.
Division Administrator

Enclosures

- cc: A. Bullcoming, Director, Tribal Env. Program
- K. Webb, CDOT Region 6
- D. Jepson, CDOT Env. Programs
- M. Whorton, CH2M Hill
- M. Allen, FHWA



**US 6 and WADSWORTH
ENVIRONMENTAL ASSESSMENT
PROJECT LOCATION MAP**

SOURCE:
CH2M

MAP CREATED: 08-3-07



**FEDERAL HIGHWAY ADMINISTRATION/COLORADO DEPARTMENT OF
TRANSPORTATION SECTION 106 TRIBAL CONSULTATION INTEREST RESPONSE FORM**

PROJECT: 6th Avenue/Wadsworth Blvd. Interchange Environmental Assessment

The _____ Tribe [is / is not] (circle one) interested in becoming a consulting party for the Colorado Department of Transportation project referenced above, for the purpose of complying with Section 106 of the National Historic Preservation Act and its implementing regulations (36 CFR 800). If your tribe will be a consulting party, please answer the questions below.

Signed: _____
Name and Title

CONSULTING PARTY STATUS [36 CFR §800.2(c)(3)]

Do you know of any specific sites or places to which your tribe attaches religious and cultural significance that may be affected by this project?

Yes No If yes, please explain the general nature of these places and how or why they are significant (use additional pages if necessary). Locational information is not required.

SCOPE OF IDENTIFICATION EFFORTS [36 CFR §800.4(a)(4)]

Do you have information you can provide us that will assist us in identifying sites or places that may be of religious or cultural significance to your tribe?

Yes No If yes, please explain.

CONFIDENTIALITY OF INFORMATION [36 CFR §800.11(c)]

Is there any information you have provided here, or may provide in the future, that you wish to remain confidential?

Yes No If yes, please explain.

Please complete and return this form via US Mail or fax to:

Dan Jepson, Section 106 Native American Liaison
Colorado Department of Transportation
Environmental Programs Branch
4201 E. Arkansas Ave.
Denver, CO 80222
FAX: (303)757-9445

TRIBAL MAILING LIST
6th Ave./Wadsworth Interchange EA

Mr. Alonzo Chalepah, Chairman
Apache Tribe of Oklahoma
P.O. Box 1220
Anadarko, OK 73005

Copy to:

Ms. Angela Bullcoming, Director
Environmental Program
Apache Tribe of Oklahoma
P.O. Box 1220
Anadarko, OK 73005

Mr. Darrell Flyingman, Governor
Cheyenne and Arapaho Tribes of Oklahoma
P.O. Box 38
Concho, OK 73022

Copy to:

Mr. Richard Williams, NAGPRA Representative
Cheyenne and Arapaho Tribes of Oklahoma
P.O. Box 38
Concho, OK 73022

Mr. Gordon Yellowman, NAGPRA Representative
Cheyenne and Arapaho Tribes of Oklahoma
P.O. Box 38
Concho, OK 73022

Mr. Richard Brannan, Chairman
Northern Arapaho Business Council
P.O. Box 396
Fort Washakie, WY 82514

Copy to:

Ms. Jo Ann White
Tribal Historic Preservation Officer
Northern Arapaho Tribe
P.O. Box 396
Ft. Washakie, WY 82514

Mr. Eugene Little Coyote, Chairman
Northern Cheyenne Tribe
P.O. Box 128
Lame Deer, MT 59043

Copy to:

Mr. Conrad Fisher
Tribal Historic Preservation Officer
Northern Cheyenne Tribe
P.O. Box 128
Lame Deer, MT 59043

Mr. Joseph J. Brings Plenty, Sr., Chairman
Cheyenne River Sioux Tribal Council
P.O. Box 590
Eagle Butte, SD 57625

Copy to:

Mr. Albert M. LeBeau III
Tribal Historic Preservation Officer
Cheyenne River Sioux Tribe
P.O. Box 590
Eagle Butte, SD 57625

Mr. Billy Evans Horse, Chairman
Kiowa Tribe of Oklahoma
P.O. Box 369
Carnegie, OK 73015

Copy to:

Reverend George Daingkau, NAGPRA Representative
Kiowa Tribe of Oklahoma
118 N. Stephens
Hobart, OK 73015

Mr. Wallace Coffey, Chairman
Comanche Tribal Business Committee
Comanche Nation of Oklahoma
P.O. Box 908
Lawton, OK 73502

Copy to:

Ms. Ruth Toahty, NAGPRA Coordinator
Comanche Nation of Oklahoma
P.O. Box 908
Lawton, OK 73502

Mr. Ron His-Horse-Is-Thunder, Chairman
Standing Rock Sioux Tribal Council
P.O. Box D
Fort Yates, ND 58538

Copy to:

Mr. Tim Mentz, Sr.
Tribal Historic Preservation Officer
Standing Rock Sioux Tribe
P.O. Box D
Ft. Yates, ND 58538

Mr. Rodney Bordeaux, President
Rosebud Sioux Tribe
P.O. Box 430
Rosebud, SD 57570

Copy to:

Mr. Terry Gray, NAGPRA Coordinator
Rosebud Sioux Tribe
Sinte Gleska University
P.O. Box 105
Mission, SD 57555

Ms. Maxine Natchees, Chairwoman
Ute Tribe of the Uintah and Ouray Agency
P.O. Box 1990
Ft. Duchesne, UT 84026

Copy to:

Ms. Betsy Chapoose, Director
Cultural Rights and Protection Office
Ute Tribe of the Uintah and Ouray Agency
P.O. Box 190
Ft. Duchesne, UT 84026

Mr. Clement Frost, Chairman
Southern Ute Indian Tribe
P.O. Box 737
Ignacio, CO 81137

Copy to:

Mr. Neil Cloud, NAGPRA Representative
Culture Preservation Office
Southern Ute Indian Tribe
P.O. Box 737
Ignacio, CO 81137

Mr. Manuel Heart, Chairman
Ute Mountain Ute Tribe
P.O. Box 348
Towaoc, CO 81334

Copy to:

Mr. Terry Knight, NAGPRA Representative
Ute Mountain Ute Tribe
P.O. Box 468
Towaoc, CO 81334

Mr. Lester Thompson, Jr., Chairman
Crow Creek Sioux Tribal Council
P.O. Box 658
Fort Thompson, SD 57325

Mr. John Yellow Bird Steele, President
Oglala Sioux Tribal Council
P.O. Box H
Pine Ridge, SD 57770

CDOT REGION AND CONSULTANT
(Send copy of letter/packet to the following individuals)

Mr. Kirk Webb
CDOT Region 6 Environmental Office

Ms. Mandy Whorton, Project Manager
CH2M HILL
9191 S. Jamaica St.
Englewood, CO 80112

**NORTHERN CHEYENNE TRIBE
TRIBAL HISTORICAL PRESERVATION OFFICE
P.O. Box 128
Lame Deer, Montana 59043
Tel: (406) 477-6035 Fax: (406) 477-6210**

Native American Consultation Response Form

Site Name:	6 th Avenue / Wadsworth Blvd.
TCNS Notification ID Number:	
Site Address: Fax 303-757-9445	US Dept. Of Trans. Colorado Fecl. Aid Division Attn: David A. Nicol, P.E.

Response:

- **REQUEST ADDITIONAL INFORMATION** _____ (Initials of duly authorized Tribal Official) I require the following additional information in order to provide a finding of effect for this purpose undertaking: _____
- **NO ADVERSE EFFECT** _____ (Initials of duly authorized Tribal Official)
I believe the proposed project would have no adverse effect on these properties.
- **ADVERSE EFFECT** _____ (Initials of duly authorized Tribal Official)
Based on the information given, I believe the proposed project would cause an adverse effect on these properties.
- **NO INTEREST** _____ (Initials of duly authorized Tribal Official)
I have identified that there are no properties of religious and cultural significance to the Northern Cheyenne in the proposed construction area.
- **NO EFFECT** _____ (Initials of duly authorized Tribal Official)
I have determined that there are no properties of religious and cultural significance to the Northern Cheyenne Tribe that are listed on the National Register within the area of potential effect or that the proposed project will have no effect on any such properties that may be present.
- **NO COMMENT** CF _____ (Initials of duly authorized Tribal Official)
- **Other (Specify)** _____

Exception: If archaeological materials or human remains are encountered during construction, the State Historic Preservation Office and applicable Native American Tribes will be notified.

CF
Signature

10/24/07
Date

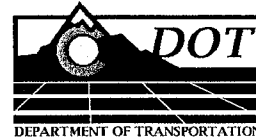
Mr. Conrad Fisher, Director N.C.T./THPO
Printed Name

1(406) 477-6035
Telephone No.

STATE OF COLORADO

DEPARTMENT OF TRANSPORTATION

Environmental Programs Branch
Shumate Building
4201 East Arkansas Avenue
Denver, Colorado 80222
(303) 757-9259



December 12, 2007

Ms. Georgianna Contiguglia
State Historic Preservation Officer
Colorado Historical Society
1300 Broadway
Denver, CO 80203

SUBJECT: Area of Potential Effects (APE) Consultation, US 6 and Wadsworth Environmental Assessment
(CHS # 50636)

Dear Ms. Contiguglia:

This letter and the attached maps and materials constitute the Federal Highway Administration (FHWA) and Colorado Department of Transportation's (CDOT) request for comment on the Area of Potential Effects (APE) for the project referenced above. In coordination with the City of Lakewood, CDOT has undertaken the project to improve mobility, safety, and transit within the corridor, an integral part of the transportation network in the west metropolitan area. Amy Pallante of your staff received scoping materials from this project this summer that provide details on the purpose and need of the project and other issues that have been discussed thus far.

APE Consultation

APE consultation with Amy Pallante took place on November 15, 2007 to discuss the proposed APE and survey methodology issues. Agreement was reached regarding the APE boundary as depicted on the attached maps and the accompanying technical memorandum, which explains the factors that influenced the APE.

At this time, we are also requesting the participation of the following consulting parties that have an interest in historical resources within the project corridor: Lakewood Heritage Center, Lakewood Historical Society, the Jefferson County Historical Commission and Colorado Preservation, Inc. Each will receive the APE maps and the technical memorandum that you have received. We will forward any responses that we may receive to you.

We hereby request your comments on the APE outlined herein. Your response is necessary for the Federal Highway Administration's compliance with Section 106 of the National Historic Preservation Act, and the Advisory Council on Historic Preservation's regulations. Thank you in advance for your prompt attention to this matter. If you require additional information, please contact CDOT Region 6 Senior Historian Dianna Litvak at (303) 757-9461.

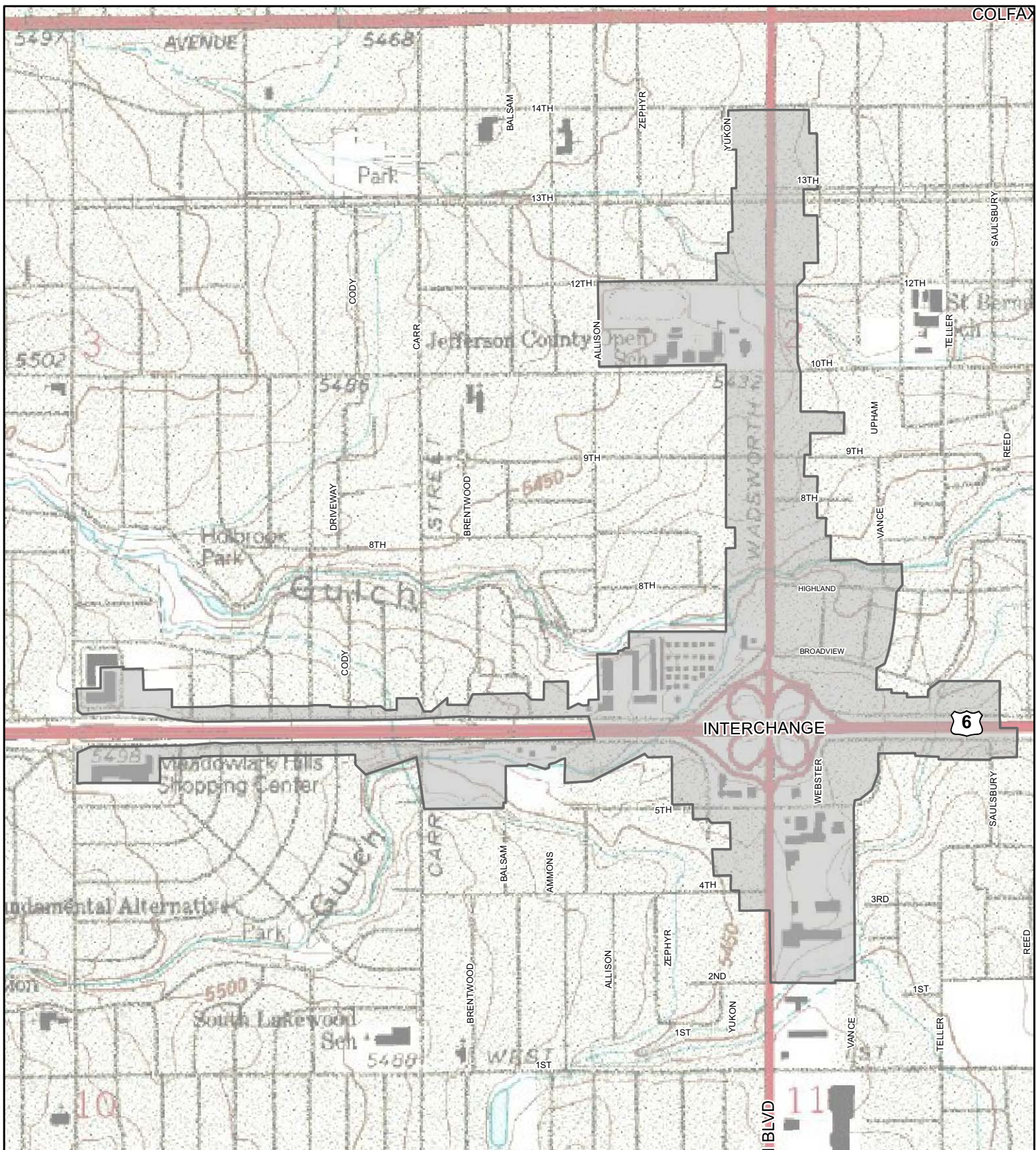
Very truly yours,


Jim Paulmeno

Region 6 Planning and Environmental Manager


Enclosures: APE map, Parcel Atlas and Technical Memorandum

cc: Marcee Allen, FHWA
Lisa Schoch, CDOT Environmental Programs Branch
Mandy Whorton, CH2MHill
File



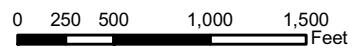
US 6 and WADSWORTH ENVIRONMENTAL ASSESSMENT

Legend

 Area of Potential Effects (APE)

SOURCE:
Jefferson County Assessor

MAP CREATED: 05-02-2008



N



Area of Potential Effects, Historic Properties

PREPARED FOR: US 6 and Wadsworth Project Leadership Team

PREPARED BY: CH2M HILL

DATE: November 12, 2007

The draft APE was developed based on the footprint of anticipated project improvements. It has been refined through consultation with the CDOT Region 6 Historian. Although alternatives have not been developed, CDOT has established the APE considering the following factors:

- Parcels fronting the west side of Wadsworth Boulevard are deep (300 feet), and potential direct, visual, and noise effects from improvements along this portion of Wadsworth Boulevard would be limited to this first row of properties. Structures are either set back a distance from the roadway/sidewalk or there is sufficient space within the lots to relocate buildings on site for the same use if necessary (that is, existing businesses could continue to operate in the same location, so there would be little change to the second row properties).
- Many of the parcels fronting the east side of Wadsworth Boulevard also are 300 feet deep, and impacts would be within the first row for these properties (as described for the west side of Wadsworth Boulevard). In cases where the parcels are less than 200 feet deep, the first two rows of properties were included in the APE to account for the potential impacts to a second row property if the first row property was removed.
- The Jefferson County Open School property on the west side of Wadsworth between 10th and 12th Avenues contains a number of buildings, most of which will not be affected by the project. The property has not been recorded in Compass yet and is likely to be eligible to the National Register. In the interest of adding to the public historical record, CDOT will survey the whole property.
- Access to/from residential neighborhoods from Wadsworth Boulevard will not likely change. (Access to commercial properties, which are included in the APE, could change)
- Substantial right-of-way (ROW) in the western quadrants of the interchange provides adequate area for project improvements to the ramps within existing ROW.
- ROW in the eastern quadrants of the interchange is not likely sufficient to accommodate project improvements. The APE includes an area that would allow for ramp lengths that would meet design speed standards. The APE extends to along Park Avenue and Broadview Drive north of the interchange to account for potential changes to access for the properties located between the US 6 frontage road and Broadview Drive. Noise walls are present along US 6 in the area east of the interchange, and no change to noise or visual conditions would be expected from project improvements.

- The western terminus for project improvements is Garrison Street. Although physical improvements to US 6 are not included in the scope of this project, changes to frontage road operations and/or addition of sound walls (indirect effects) could occur and influenced the extension of the APE as two “fingers” along the US 6 frontage roads. No physical improvements (direct effects) would occur outside of CDOT ROW. Since most of the properties face side roads (east or west) rather than the frontage roads, the effects of noise walls or changes to traffic patterns would be to outdoor spaces or side facades. The APE was extended 30 feet from edge of pavement to account for these potential indirect effects.

CDOT plans to survey properties within the APE constructed in 1965 or earlier. Approximately 140 properties, including approximately 60 properties along the US 6 frontage roads, would be surveyed. CDOT may, pending discussions with the State Historic Preservation Office, propose limited/abbreviated site forms be completed for some of these properties.

STATE OF COLORADO

DEPARTMENT OF TRANSPORTATION

Region 6, Planning and Environmental

2000 South Holly Street

Denver, CO 80222

(303) 757-9385

(303) 757-9036 FAX



December 14, 2007

Mr. Dennis Dempsey
Jefferson County Planning and Zoning Department
100 Jefferson County Parkway
Suite 3550
Golden, CO 80419-3550

SUBJECT: Section 106 Historic Properties Consultation, US 6 and Wadsworth Environmental Assessment

Dear Mr. Dempsey:

The US 6 and Wadsworth Environmental Assessment (EA) will evaluate transportation improvements and their effect upon the environment for the US 6 and Wadsworth Boulevard Interchange and Wadsworth between approximately 4th Avenue and 14th Avenue. The Federal Highway Administration and the Colorado Department of Transportation are preparing this EA in accordance with federal environmental regulations, including the National Environmental Policy Act (NEPA); the Council on Environmental Quality and FHWA regulations (Title 40 of the Code of Federal Regulations [CFR], Parts 1500-1508 and 23 CFR 771, respectively). The Regional Transportation District is a cooperating agency for the EA, and the City of Lakewood is a partnering agency.

CDOT, the City of Lakewood, and area residents have identified the need for transportation improvements in this corridor as among the highest priorities in the west metro area. The improvements being considered will improve safety and mobility for automobile, truck, transit, bicycle and pedestrian travel through the project area. As part of the EA evaluation process, a review of historic properties is being conducted.

Because the project is receiving federal transportation funding, FHWA and CDOT are preparing environmental clearances that are required as part of federal regulations. These include Section 106 of the National Historic Preservation Act, as amended (Section 106, 16 USC 470f, and its implementing regulations (36CFR 800)). The project team is seeking the input of local community organizations to help identify issues that may relate to the effects and mitigation of the project to historic properties. More information about this process can be found on the Advisory Council on Historic Preservation's website: <http://www.achp.gov>.

Historic Properties Identification

The project team has conducted a reconnaissance ("windshield") survey of cultural resources in the project area. During the reconnaissance survey, the project team identified resources that had been previously recorded in the site files of the State Historic Preservation Office (SHPO) and also observed unrecorded historic properties located in proximity to the corridor. Information gathered from this

process was used to identify the boundary of the cultural resources Area of Potential Effects (APE), defined in the Section 106 regulations as:

The geographic area or areas within which an undertaking may directly or indirectly cause alterations in the character or use of historic properties, if any such properties exist. The area of potential effects is influenced by the scale and nature of an undertaking and may be different for different kinds of effects caused by the undertaking (36 CFR 800.16(d)).

The cultural resources APE for this project is depicted on the attached figures and described in the accompanying technical memorandum.

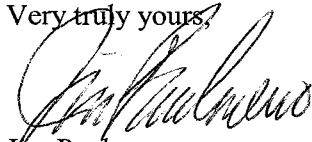
Section 106 Consultation

We are contacting local historical organizations to help identify any historic buildings, districts, sites, objects, or archaeological sites of significance within the project area and to seek comments on the APE for this project. FHWA and CDOT would like to formally offer the Jefferson County Historical Commission the opportunity to participate as a consulting party for the Section 106 compliance process, as provided in Section 800.3(f)(1) of the regulation. Any information you can provide will help ensure that important historical resources are considered in the project planning process.

If you are interested in participating as a consulting party for this EA under the Section 106 guidelines, please respond in writing within 30 days of receipt of this letter to Dianna Litvak, CDOT Region 6 Senior Staff Historian, at the address on the letterhead or via email at dianna.litvak@dot.state.co.us. We request that your response include a statement of demonstrated interest in historic properties associated with this EIS, as stipulated in the Section 106 regulation.

If you elect to become a consulting party, we will continue to keep you informed of our historical research within the project area and any potential effects to historic properties. If you require additional information or have questions about the Section 106 process, please contact Ms. Litvak at (303)757-9461.

Very truly yours,



Jim Paulmeno
Region 6 Planning and Environmental Manager

Enclosure: Maps of Area of Potential Effects and Technical Memorandum

cc: Marcee Allen, FHWA
Georgianna Contiguglia, Colorado SHPO
Lisa Schoch, Environmental Programs Branch
Mandy Whorton, CH2MHill
file

STATE OF COLORADO

DEPARTMENT OF TRANSPORTATION

Region 6, Planning and Environmental
2000 South Holly Street
Denver, CO 80222
(303) 757-9385
(303) 757-9036 FAX



December 14, 2007

Mrs. Karen Zoltenko
Lakewood Historical Society
485 Winona Court
Denver, CO 80204

SUBJECT: Section 106 Historic Properties Consultation, US 6 and Wadsworth Environmental Assessment

Dear Mrs. Zoltenko:

The US 6 and Wadsworth Environmental Assessment (EA) will evaluate transportation improvements and their effect upon the environment for the US 6 and Wadsworth Boulevard Interchange and Wadsworth between approximately 4th Avenue and 14th Avenue. The Federal Highway Administration and the Colorado Department of Transportation are preparing this EA in accordance with federal environmental regulations, including the National Environmental Policy Act (NEPA); the Council on Environmental Quality and FHWA regulations (Title 40 of the Code of Federal Regulations [CFR], Parts 1500-1508 and 23 CFR 771, respectively). The Regional Transportation District is a cooperating agency for the EA, and the City of Lakewood is a partnering agency.

CDOT, the City of Lakewood, and area residents have identified the need for transportation improvements in this corridor as among the highest priorities in the west metro area. The improvements being considered will improve safety and mobility for automobile, truck, transit, bicycle and pedestrian travel through the project area. As part of the EA evaluation process, a review of historic properties is being conducted.

Because the project is receiving federal transportation funding, FHWA and CDOT are preparing environmental clearances that are required as part of federal regulations. These include Section 106 of the National Historic Preservation Act, as amended (Section 106, 16 USC 470f, and its implementing regulations (36CFR 800)). The project team is seeking the input of local community organizations to help identify issues that may relate to the effects and mitigation of the project to historic properties. More information about this process can be found on the Advisory Council on Historic Preservation's website: <http://www.achp.gov>.

Historic Properties Identification

The project team has conducted a reconnaissance ("windshield") survey of cultural resources in the project area. During the reconnaissance survey, the project team identified resources that had been previously recorded in the site files of the State Historic Preservation Office (SHPO) and also observed unrecorded historic properties located in proximity to the corridor. Information gathered from this process was used to identify the boundary of the cultural resources Area of Potential Effects (APE), defined in the Section 106 regulations as:

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The cultural resources APE for this project is depicted on the attached figures and described in the accompanying technical memorandum.

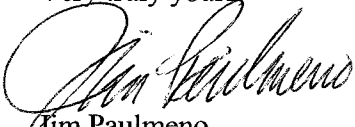
Section 106 Consultation

We are contacting local historical organizations to help identify any historic buildings, districts, sites, objects, or archaeological sites of significance within the project area and to seek comments on the APE for this project. FHWA and CDOT would like to formally offer the Jefferson County Historical Commission the opportunity to participate as a consulting party for the Section 106 compliance process, as provided in Section 800.3(f)(1) of the regulation. Any information you can provide will help ensure that important historical resources are considered in the project planning process.

If you are interested in participating as a consulting party for this EA under the Section 106 guidelines, please respond in writing within 30 days of receipt of this letter to Dianna Litvak, CDOT Region 6 Senior Staff Historian, at the address on the letterhead or via email at dianna.litvak@dot.state.co.us. We request that your response include a statement of demonstrated interest in historic properties associated with this EIS, as stipulated in the Section 106 regulation.

If you elect to become a consulting party, we will continue to keep you informed of our historical research within the project area and any potential effects to historic properties. If you require additional information or have questions about the Section 106 process, please contact Ms. Litvak at (303)757-9461.

Very truly yours



Jim Paulmeno

Region 6 Planning and Environmental Manager

Enclosure: Maps of Area of Potential Effects and Technical Memorandum

cc: Marcee Allen, FHWA
Georgianna Contiguglia, Colorado SHPO
Lisa Schoch, Environmental Programs Branch
Mandy Whorton, CH2MHill
file

STATE OF COLORADO

DEPARTMENT OF TRANSPORTATION

Region 6, Planning and Environmental

2000 South Holly Street
Denver, CO 80222
(303) 757-9385
(303) 757-9036 FAX



December 14, 2007

Mrs. Winifred Ferril
Lakewood Heritage Center
797 S. Wadsworth Blvd.
Lakewood, CO 80226

SUBJECT: Section 106 Historic Properties Consultation, US 6 and Wadsworth Environmental Assessment

Dear Mrs. Ferril:

The US 6 and Wadsworth Environmental Assessment (EA) will evaluate transportation improvements and their effect upon the environment for the US 6 and Wadsworth Boulevard Interchange and Wadsworth between approximately 4th Avenue and 14th Avenue. The Federal Highway Administration and the Colorado Department of Transportation are preparing this EA in accordance with federal environmental regulations, including the National Environmental Policy Act (NEPA); the Council on Environmental Quality and FHWA regulations (Title 40 of the Code of Federal Regulations [CFR], Parts 1500-1508 and 23 CFR 771, respectively). The Regional Transportation District is a cooperating agency for the EA, and the City of Lakewood is a partnering agency.

CDOT, the City of Lakewood, and area residents have identified the need for transportation improvements in this corridor as among the highest priorities in the west metro area. The improvements being considered will improve safety and mobility for automobile, truck, transit, bicycle and pedestrian travel through the project area. As part of the EA evaluation process, a review of historic properties is being conducted.

Because the project is receiving federal transportation funding, FHWA and CDOT are preparing environmental clearances that are required as part of federal regulations. These include Section 106 of the National Historic Preservation Act, as amended (Section 106, 16 USC 470f, and its implementing regulations (36CFR 800)). The project team is seeking the input of local community organizations to help identify issues that may relate to the effects and mitigation of the project to historic properties. More information about this process can be found on the Advisory Council on Historic Preservation's website: <http://www.achp.gov>.

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The cultural resources APE for this project is depicted on the attached figures and described in the accompanying technical memorandum.

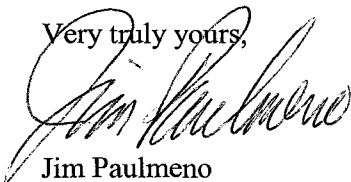
Section 106 Consultation

We are contacting local historical organizations to help identify any historic buildings, districts, sites, objects, or archaeological sites of significance within the project area and to seek comments on the APE for this project. FHWA and CDOT would like to formally offer the Jefferson County Historical Commission the opportunity to participate as a consulting party for the Section 106 compliance process, as provided in Section 800.3(f)(1) of the regulation. Any information you can provide will help ensure that important historical resources are considered in the project planning process.

If you are interested in participating as a consulting party for this EA under the Section 106 guidelines, please respond in writing within 30 days of receipt of this letter to Dianna Litvak, CDOT Region 6 Senior Staff Historian, at the address on the letterhead or via email at dianna.litvak@dot.state.co.us. We request that your response include a statement of demonstrated interest in historic properties associated with this EIS, as stipulated in the Section 106 regulation.

If you elect to become a consulting party, we will continue to keep you informed of our historical research within the project area and any potential effects to historic properties. If you require additional information or have questions about the Section 106 process, please contact Ms. Litvak at (303)757-9461.

Very truly yours,



Jim Paulmeno
Region 6 Planning and Environmental Manager

Enclosure: Maps of Area of Potential Effects and Technical Memorandum

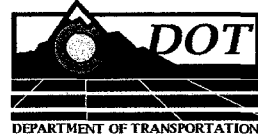
cc: Marcee Allen, FHWA
Georgianna Contiguglia, Colorado SHPO
Lisa Schoch, Environmental Programs Branch
Mandy Whorton, CH2MHill
file

STATE OF COLORADO

DEPARTMENT OF TRANSPORTATION

Region 6, Planning and Environmental

2000 South Holly Street
Denver, CO 80222
(303) 757-9385
(303) 757-9036 FAX



December 14, 2007

Mr. Mark Rodman
Executive Director
Colorado Preservation, Inc.
333 W. Colfax Ave.
Suite 300
Denver, CO 80204

SUBJECT: Section 106 Historic Properties Consultation, US 6 and Wadsworth Environmental Assessment

Dear Mr. Rodman:

The US 6 and Wadsworth Environmental Assessment (EA) will evaluate transportation improvements and their effect upon the environment for the US 6 and Wadsworth Boulevard Interchange and Wadsworth between approximately 4th Avenue and 14th Avenue. The Federal Highway Administration and the Colorado Department of Transportation are preparing this EA in accordance with federal environmental regulations, including the National Environmental Policy Act (NEPA); the Council on Environmental Quality and FHWA regulations (Title 40 of the Code of Federal Regulations [CFR], Parts 1500-1508 and 23 CFR 771, respectively). The Regional Transportation District is a cooperating agency for the EA, and the City of Lakewood is a partnering agency.

CDOT, the City of Lakewood, and area residents have identified the need for transportation improvements in this corridor as among the highest priorities in the west metro area. The improvements being considered will improve safety and mobility for automobile, truck, transit, bicycle and pedestrian travel through the project area. As part of the EA evaluation process, a review of historic properties is being conducted.

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Historic Properties Identification

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The cultural resources APE for this project is depicted on the attached figures and described in the accompanying technical memorandum.

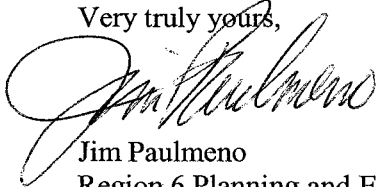
Section 106 Consultation

We are contacting local historical organizations to help identify any historic buildings, districts, sites, objects, or archaeological sites of significance within the project area and to seek comments on the APE for this project. FHWA and CDOT would like to formally offer the Jefferson County Historical Commission the opportunity to participate as a consulting party for the Section 106 compliance process, as provided in Section 800.3(f)(1) of the regulation. Any information you can provide will help ensure that important historical resources are considered in the project planning process.

If you are interested in participating as a consulting party for this EA under the Section 106 guidelines, please respond in writing within 30 days of receipt of this letter to Dianna Litvak, CDOT Region 6 Senior Staff Historian, at the address on the letterhead or via email at dianna.litvak@dot.state.co.us. We request that your response include a statement of demonstrated interest in historic properties associated with this EIS, as stipulated in the Section 106 regulation.

If you elect to become a consulting party, we will continue to keep you informed of our historical research within the project area and any potential effects to historic properties. If you require additional information or have questions about the Section 106 process, please contact Ms. Litvak at (303)757-9461.

Very truly yours,



Jim Paulmeno
Region 6 Planning and Environmental Manager

Enclosure: Maps of Area of Potential Effects and Technical Memorandum

cc: Marcee Allen, FHWA
Georgianna Contiguglia, Colorado SHPO
Lisa Schoch, Environmental Programs Branch
Mandy Whorton, CH2MHill
file



OFFICE of ARCHAEOLOGY and HISTORIC PRESERVATION

December 26, 2007

Jim Paulmeno
Region 6 Planning and Environmental Manager
Senior Historian
Colorado Department of Transportation
Region 6
2000 S. Holly Street
Denver, CO 80222

RECEIVED
1/2/08

Re: Area of Potential Effects (APE) Consultation, US 6 and Wadsworth Environmental Assessment. (CHS #50636)

Dear Mr. Paulmeno,

Thank you for your correspondence dated December 12, 2007 and received by our office on December 24, 2007 regarding the review of the above-mentioned project under Section 106 of the National Historic Preservation Act (Section 106).

After review of the provided information, we do not object to the Area of Potential Effects (APE) as presented in your submission. We do recommend additional consultation on the APE along West 6th Ave if the proposed project should change and have the potential to affect the area of West 6th Ave left out of the APE.

We request being involved in the consultation process with the local government, which as stipulated in 36 CFR 800.3 is required to be notified of the undertaking, and with other consulting parties. Additional information provided by the local government or consulting parties might cause our office to re-evaluate our eligibility and potential effect findings.

Please note that our compliance letter does not end the 30-day review period provided to other consulting parties.

If we may be of further assistance, please contact Amy Pallante, our Section 106 Compliance Coordinator, at (303) 866-4678.

Sincerely,

for

Georgianna Contiguglia
State Historic Preservation Officer

cc: Dianna Litvak/CDOT Region 6



The Lakewood Historical Society, Inc.

801 South Yarrow Street, Lakewood, CO 80226

1

January 22, 2008

Dianna Litvak
Colorado Department of Transportation
Region 6, Planning and Environmental
2000 South Holly Street
Denver, CO 80222

Re: Section 106 Historic Properties Consultation, US 6 and Wadsworth Environmental Assessment

Dear Ms. Litvak:

Thank you for inviting the Lakewood Historical Society to become a consulting party for the US 6 and Wadsworth Environmental Assessment. The Lakewood Historical Society Board met, and its members decided that the Lakewood Historical Society would like to participate as a consulting party for the project under Section 106 guidelines.

The By-Laws of the Lakewood Historical Society state that our function is to disseminate "historical information designed to inspire and maintain interest in local history". We accomplish this purpose, in part, by identifying historic buildings or sites that have played a significant role in the development of the history of Lakewood. Part of our mission is to cooperate with other organizations toward the achievement of common goals.

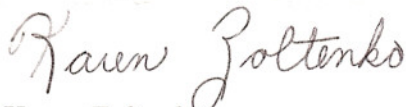
The Lakewood Historical Society has identified the following sites that may be worthy of documentation by the Colorado Department of Transportation as part of the EA of the US 6 and Wadsworth project:

- The Jefferson County Open School, formerly the Lakewood School at 10th & Wadsworth. The World War II Memorial on the school property would also be included, if it falls within the APE for this project.
- The site located on the Northeast corner of 10th & Wadsworth that was a gas station. Although we have not done any research on the building, its integrity seems to be fairly intact.

- The building directly behind 3 Margaritas Restaurant at 806 Wadsworth Blvd. The building is now being used for storage; however, we believe that it was once a Valentine Diner. Kris Autobee documented this building along with the Valentine Diner located in Belmar Park for CDOT, last fall.
- The house located on the Southeast corner of 13th & Wadsworth. We believe that this house held the offices of the architect who was responsible for most of the houses in the Green Acres neighborhood.

We are looking forward to hearing from you about how we need to further proceed, and about your progress on the US 6 and Wadsworth EA. Please let me know if you have any questions.

Sincerely,



Karen Zoltenko
President, Lakewood Historical Society

Re SHPO response to US 6 and Wadsworth Area of Potential Effects.txt
From: Dennis Dempsey [ddempsey@co.jefferson.co.us]
Sent: Friday, January 18, 2008 2:56 PM
To: Litvak, Diana
Subject: Re: SHPO response to US 6 and Wadsworth Area of Potential Effects

After reviewing the subject properties within the APE for the US 6-Wadsworth project, we do not have any comments at this time. We would like to continue to be a consulting party for this project.

Thank you,

Dennis Dempsey
Planner / Historical Commission Staff Liaison Jefferson County, Colorado

Dennis Dempsey
Jefferson County Planning & Zoning
100 Jefferson County Pkwy. #3550
Golden CO 80419-3550
Ph: 303-271-8734 or 8765
Fax: 303-271-8706
email: ddempsey@jeffco.us

>>> "Litvak, Diana" <Diana.Litvak@dot.state.co.us> 01/08/08 3:49 PM

>>> >>>

Dear Mr. Dempsey,

In correspondence dated Dec. 14th, Jim Paulmeno of CDOT requested the participation of the Jefferson County Historical Commission as a consulting party for the US 6-Wadsworth Environmental Assessment. We have since received comments from SHPO on our proposed Area of Potential Effects.

I have attached the SHPO correspondence to this message for your files, as well as a digital scan of the original letter I sent you. Your agency has 30 days to provide comments, the deadline for which would be January 22, 2008. If you have no comments, please also let me know.

Communication can be by email, telephone, or regular mail, according to your preference.

If you have any questions or comments about these materials or the project, please contact me directly.

Please let me know if I can answer any questions or if you have comments. <<SHPO APE response.pdf>> <<12_14_Let_Dempsey0001.pdf>>

Diana Litvak
CDOT Region 6 Senior Historian
2000 S. Holly Street
Denver, CO 80222
303-757-9461 (direct phone)
303-757-9036 (fax)
Diana.Litvak@dot.state.co.us

STATE OF COLORADO

DEPARTMENT OF TRANSPORTATION

Region 6
2000 South Holly Street
Denver, Colorado 80222
720-497-6950



February 18, 2008

Ms. Amy Pallante
State Historic Preservation Office
1300 Broadway
Denver, CO 80203

Subject: US 6 and Wadsworth Environmental Assessment Progress, February 2008

Dear Ms. Pallante:

Thank you for your continued interest in the US 6 and Wadsworth Environmental Assessment (EA). We continue to move quickly through the EA process, and I wanted to update you on our progress. Since August 2008 when we held our agency scoping meeting, we have been busy developing evaluation criteria and preparing preliminary design concepts for Wadsworth Boulevard between 4th and 14th Avenues and the US 6 and Wadsworth Boulevard interchange. The project website (www.US6Wadsworth.com) has been updated to reflect this progress.

Evaluation Criteria

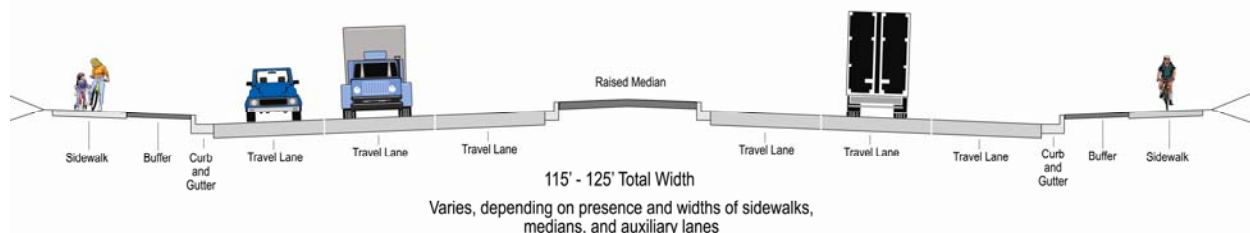
We developed evaluation criteria in two stages: Level 1 Screening and Level 2 Evaluation. The Level 1 Screening is intended to (a) eliminate from detailed analysis concepts with fatal flaws and (b) identify a reasonable range of alternatives that could meet the project purpose and need, can be implemented at a reasonable cost, and would not result in unacceptable environmental or community impacts. Level 2 Evaluation provides quantitative measures of criteria to (a) compare the design concepts carried forward from the Level 1 Screening and (b) assist in the selection of alternatives that will be carried forward for analysis in the EA. Information on the evaluation criteria is available on the project website.

Design Concepts

The project team has developed and screened for fatal flaws preliminary design concepts for Wadsworth Boulevard and the interchange. Eleven Wadsworth Boulevard and eight interchange concepts were presented and explained at a Public Open House on February 12, 2008. Of these, CDOT has recommended one Wadsworth Boulevard and four interchange concepts be examined in the Level 2 Evaluation. From these, CDOT will select alternative(s) to be considered in the EA.

Wadsworth Boulevard Concepts

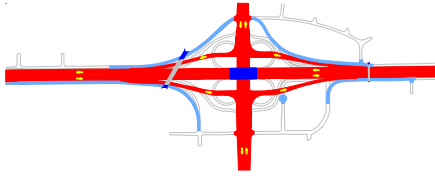
CDOT recommends that one Wadsworth Boulevard concept—six travel lanes with a raised median and sidewalks—be carried forward. The basic elements of this concept are shown below. It is likely that multiple alternatives, each varying the design elements, will be developed out of this concept.



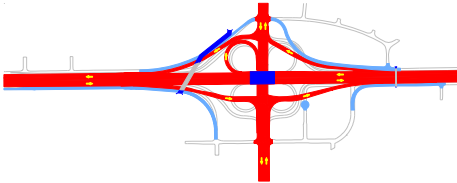
Interchange Concepts

CDOT recommends the following four interchange design concepts for further evaluation.

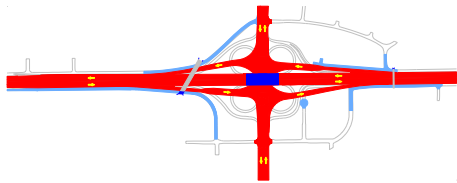
Interchanges



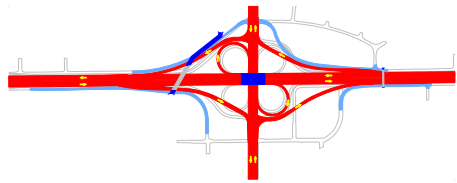
Tight Diamond



Tight Diamond with Loop



Single Point Urban Interchange (SPUI)



Partial Cloverleaf

Examples

- I-70 and Federal Boulevard
- US 6 and Indiana Avenue
- I-70 and Denver West Boulevard

- I-25 and Colorado Boulevard
- I-25 and Castle Pines Parkway

- C-470 and Morrison Road
- I-25 and University Boulevard
- US 85 (Santa Fe) and Evans Avenue

- US 36 and Federal Boulevard
- US 285 and US 85
(Hampden and Santa Fe)

Additional details on the concepts can be found on the project website (see [Alternatives](#) and [Study Documents](#) pages).

Next Steps

We are on track with our accelerated schedule to identify final alternative(s) in April 2008, complete a draft EA for agency and public review in August 2008, and produce a decision document by the end of the year (December 2008). We will provide you another update in April when we have identified alternatives that will be analyzed in the EA. We also will hold another public Open House at that time to present the results of the Level 2 Evaluation, including preliminary estimates of environmental, transportation, and property effects of these concepts. We do not plan any formal agency meetings until the review of the EA document in August 2008. (At that time, we plan to hold agency review meetings in a similar format to our scoping meetings.) In the meantime, please feel free to contact me (720-497-6955) or our Consultant Environmental Manager, Mandy Whorton, (720-286-5239) if you have any questions or comments. Thank you for your continued interest and participation in our EA.

Sincerely,

Seyed Kalantar, P.E.
CDOT Region 6 Project Manager

STATE OF COLORADO

DEPARTMENT OF TRANSPORTATION

Region 6, Planning and Environmental
2000 South Holly Street
Denver, CO 80222
(303) 757-9385
(303) 757-9036 FAX



July 2, 2008

Mr. Edward Nichols
State Historic Preservation Officer
Colorado Historical Society
1300 Broadway Avenue
Denver, CO 80203

SUBJECT: Determination of Eligibility – US 6 and Wadsworth Environmental Assessment, Lakewood, Colorado

Dear Mr. Nichols:

This letter and the attached Historic Resources Survey Report constitute the Determinations of Eligibility for the project referenced above. The report was prepared as part of the Environmental Assessment (EA) for the proposed construction of transportation improvements at the US 6 and Wadsworth Boulevard interchange and along Wadsworth Boulevard between 4th and 14th Avenues in Lakewood, Colorado.

Project Description

The purpose of the US 6 and Wadsworth Boulevard project is to improve traffic flow and safety, accommodate high traffic volumes, and increase multi-modal travel options and connections at the US 6 and Wadsworth Boulevard interchange and along Wadsworth Boulevard between 4th Avenue and 14th Avenue.

The project area includes US 6 (also designated as 6th Avenue) and Wadsworth Boulevard (also designated as State Highway 121). The east-west limits along US 6 are from the eastern interchange ramps with Wadsworth Boulevard west to Garrison Street. On Wadsworth Boulevard, the project limits are 4th Avenue to 14th Avenue. This area is a vital regional hub of the western Denver metropolitan area and the heart of the City of Lakewood.

The Colorado Department of Transportation (CDOT), Federal Highway Administration (FHWA), City of Lakewood, area residents, businesses, and commuters have prioritized making improvements to fix the transportation problems in the project area through previous planning efforts. CDOT's goal is to identify a proposed action that meets transportation needs, is compatible with local and regional plans, minimizes environmental harm, and can be implemented within cost constraints.

Description of the Recommended Alternative

The existing design and configuration of the interchange and roadway within the project limits have not kept pace with traffic and multi-modal travel demands. Improvements are needed to improve safety for motorists, pedestrians, and bicyclists; correct design deficiencies that contribute to safety

concerns and operational inefficiencies; increase infrastructure capacity to meet current and future traffic volumes; and support multi-modal connections.

To address these needs, CDOT proposes to reconstruct the existing cloverleaf interchange and replace it with a tight diamond with a loop in the northwest quadrant. The loop provides the greatest capacity for the highest volume traffic movement: westbound to southbound in the evening rush hour. The new interchange would provide adequate acceleration and deceleration lanes, improve intersection spacing, and remove weaving conflicts. Existing noise walls on the east side of Wadsworth near the interchange would be reconstructed where needed, and noise walls would be extended between US 6 and the US 6 frontage road west of Wadsworth to commercial developments at Garrison Street. The existing US 6 bridge over Wadsworth would be reconstructed, and its profile would be raised to accommodate a longer span across Wadsworth.

CDOT also proposes to widen Wadsworth Boulevard between 4th and 14th Avenues to provide an additional travel lane in each direction and detached sidewalks along both sides of the highway; these improvements would complement improvements north and south of the project area. In addition, access along Wadsworth would be controlled with a raised median.

Additional detail on the Recommended Alternative will be submitted as part of the Effects Determination for this project.

Eligibility Determinations

A total of 275 cultural resources are located within the boundaries of the project area. Of these, 156 resources were constructed during or before 1965. The date 1965 was chosen because it is CDOT's standard practice to evaluate historic resources that are 45 years or older in order to allow for a period for completion of final design and construction for the proposed project. Eighteen of the 156 pre-1966 historic resources have been previously surveyed and evaluated. Due to changes to the resources, or to augment or correct existing inventory forms, 13 of these 18 historic resources were resurveyed in this inventory. The remaining 5 resources not resurveyed have previously received official determinations of eligibility (as part of RTD's West Corridor EIS) and did not warrant additional survey. Therefore, a total of 151 cultural resources (156 resources minus the 5 resources not resurveyed) were surveyed in this inventory. These 151 resources consist of 137 architectural resources, 7 landscape features, and 7 linear resources.

Results of this cultural resources inventory recommend that 9 historic resources are individually eligible for nomination to the NRHP. Additionally, three potential historic districts have been identified within the project area. A recommended Lakewood School Historic District consists of 6 buildings and structures and 2 landscape features that are recommended as contributing resources, and eleven buildings and structures and 5 landscape features are recommended as noncontributing to this potential historic district. A recommended Green Acres Historic District includes 20 contributing buildings and 10 noncontributing buildings within the project area. Finally, a recommended Meadowlark Hills Historic District includes 10 contributing buildings and one noncontributing building within the project area. The remaining historic resources surveyed in this inventory consist of 70 architectural resources and 7 linear resources, and all of these are recommended to be not eligible for nomination to the NRHP.

The eligibility determinations for all surveyed resources are summarized in the following table

ELIGIBILITY RECOMMENDATIONS			
US 6 and Wadsworth Environmental Assessment			
Site No.	Address	Description	NRHP Eligibility Recommendation
5JF3548	7395 6th Ave. Frontage Rd.	Residence	Eligible, Criterion C
5JF3549	7423 6th Ave. Frontage Rd.	Residence	Eligible, Criterion C
5JF3554	7558 West 9th Ave.	Residence	Eligible, Criterion C
5JF4511	1215 Wadsworth Blvd.	Residence	Eligible, Criterion A
5JF4513	1230 Wadsworth Blvd.	Residence	Eligible, Criterion C
5JF4536	700 Wadsworth Blvd.	Residence	Eligible, Criterion C
5JF4542	7433 6th Ave. Frontage Rd.	Residence	Eligible, Criterion C
5JF4563	8125 6th Ave. Frontage Rd.	Residence	Eligible, Criterion C
5JF4586	401 Wadsworth Blvd.	Gas Station	Eligible, Criteria A and C
5JF4512	1015 Wadsworth Blvd.	Building 1: New America School	Contributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Building 2: Elementary School	Contributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Building 3: Gymnasium	Contributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Building 7: Junior High	Contributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Structure 10: World War II Memorial	Contributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Structure 12: Storage Outbuilding	Contributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Memorial Field	Contributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Landscaped areas adjacent to Buildings 1, 2, 3, 7	Contributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Building 4: Cafeteria and Library	Noncontributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Building 5: Administration and Arts	Noncontributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Building 6: Connecting Building	Noncontributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Building 8: Theater	Noncontributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Structure 9: Outbuilding	Noncontributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Structure 11: Storage Outbuilding	Noncontributing, Lakewood School Historic District

ELIGIBILITY RECOMMENDATIONS			
US 6 and Wadsworth Environmental Assessment			
5JF4512	1015 Wadsworth Blvd.	Structure 13: Storage Outbuilding	Noncontributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Structure 14: Concessions Building	Noncontributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Structure 15: Storage Outbuilding	Noncontributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Structure 16: Bleachers	Noncontributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Structure 17: Ticket Booth	Noncontributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Baseball Field	Noncontributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Parking Areas	Noncontributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Playground	Noncontributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Landscaped areas adjacent to Buildings 4, 5, 6, and 8	Noncontributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Fences	Noncontributing, Lakewood School Historic District
5JF3610	700 Park Ln.	Residence	Contributing, Green Acres Historic District
5JF4526	7445 Highland Dr.	Residence	Contributing, Green Acres Historic District
5JF4527	7415 Highland Dr.	Residence	Contributing, Green Acres Historic District
5JF4528	7385 Highland Dr.	Residence	Contributing, Green Acres Historic District
5JF4529	705 Emerald Ln.	Residence	Contributing, Green Acres Historic District
5JF4530	715 Emerald Ln.	Residence	Contributing, Green Acres Historic District
5JF4537	7500 Broadview Dr.	Residence	Contributing, Green Acres Historic District
5JF4541	7370 Broadview Dr.	Residence	Contributing, Green Acres Historic District
5JF4543	740 Emerald Ln.	Residence	Contributing, Green Acres Historic District
5JF4544	730 Emerald Ln.	Residence	Contributing, Green Acres Historic District
5JF4545	701 Park Ln.	Residence	Contributing, Green Acres Historic District
5JF4546	721 Park Ln.	Residence	Contributing, Green Acres Historic District
5JF4547	741 Park Ln.	Residence	Contributing, Green Acres Historic District
5JF4548	761 Park Ln.	Residence	Contributing, Green Acres Historic District
5JF4549	781 Park Ln.	Residence	Contributing, Green Acres Historic District
5JF4550	790 Park Ln.	Residence	Contributing, Green Acres Historic District
5JF4551	660 Park Ln.	Residence	Contributing, Green Acres Historic District
5JF4552	680 Park Ln.	Residence	Contributing, Green Acres Historic District
5JF4553	720 Park Ln.	Residence	Contributing, Green Acres Historic District

ELIGIBILITY RECOMMENDATIONS			
US 6 and Wadsworth Environmental Assessment			
5JF4636	710 Emerald Ln.	Residence	Contributing, Green Acres Historic District
5JF4531	725 Emerald Ln.	Residence	Noncontributing, Green Acres Historic District
5JF4532	765 Emerald Ln.	Residence	Noncontributing, Green Acres Historic District
5JF4538	7474 Broadview Dr.	Residence	Noncontributing, Green Acres Historic District
5JF4539	7460 Broadview Dr.	Residence	Noncontributing, Green Acres Historic District
5JF4540	7400 Broadview Dr.	Residence	Noncontributing, Green Acres Historic District
5JF4610	7393 6th Ave. Frontage Rd.	Residence	Noncontributing, Green Acres Historic District
5JF4611	7373 6th Ave. Frontage Rd.	Residence	Noncontributing, Green Acres Historic District
5JF4612	7225 6th Ave. Frontage Rd.	Residence	Noncontributing, Green Acres Historic District
5JF4613	7199 6th Ave. Frontage Rd.	Residence	Noncontributing, Green Acres Historic District
5JF4637	720 Emerald Ln.	Residence	Noncontributing, Green Acres Historic District
5JF4590	545 Meadowlark Dr.	Residence	Contributing, Meadowlark Hills Historic District
5JF4591	540 Meadowlark Dr.	Residence	Contributing, Meadowlark Hills Historic District
5JF4592	555 Everett St.	Residence	Contributing, Meadowlark Hills Historic District
5JF4593	560 Everett St.	Residence	Contributing, Meadowlark Hills Historic District
5JF4594	585 Estes St.	Residence	Contributing, Meadowlark Hills Historic District
5JF4595	580 Estes St.	Residence	Contributing, Meadowlark Hills Historic District
5JF4596	585 Dudley St.	Residence	Contributing, Meadowlark Hills Historic District
5JF4597	580 Dudley St.	Residence	Contributing, Meadowlark Hills Historic District
5JF4598	585 Dover St.	Residence	Contributing, Meadowlark Hills Historic District
5JF4599	590 Dover St.	Residence	Contributing, Meadowlark Hills Historic District
5JF4589	9050 6th Ave.	Shopping Center	Noncontributing, Meadowlark Hills Historic District
5JF2907	7550 West 13th Ave.	Residence	Not Eligible
5JF2908	1296 Wadsworth Blvd.	Residence	Not Eligible
5JF2911	1370 Yukon St.	Residence	Not Eligible
5JF2912	1355 Wadsworth Blvd.	Residence	Not Eligible
5JF2913	1345 Wadsworth Blvd.	Residence	Not Eligible
5JF2914	1360 Yukon St.	Residence	Not Eligible
5JF4510	Northwest Cloverleaf Ruin	Residence	Not Eligible
5JF4514	1224 Wadsworth Blvd.	Residence	Not Eligible
5JF4515	1220 Wadsworth Blvd.	Residence	Not Eligible
5JF4516	1208 Wadsworth Blvd.	Residence	Not Eligible
5JF4517	1204 Wadsworth Blvd.	Residence	Not Eligible
5JF4518	1200 Wadsworth Blvd.	Residence	Not Eligible
5JF4519	1190 Wadsworth Blvd.	Residence	Not Eligible
5JF4520	1050 Wadsworth Blvd.	Office building	Not Eligible
5JF4521	1000 Wadsworth Blvd.	Auto service station	Not Eligible

ELIGIBILITY RECOMMENDATIONS			
US 6 and Wadsworth Environmental Assessment			
5JF4522	7585 West 10th Ave.	Converted residence	Not Eligible
5JF4523	900 Wadsworth Blvd.	Residence	Not Eligible
5JF4524	850 Wadsworth Blvd.	Residence	Not Eligible
5JF4525	820 Wadsworth Blvd.	Commercial equipment sales / rental	Not Eligible
5JF4533	730 Wadsworth Blvd.	Residence	Not Eligible
5JF4534	720 Wadsworth Blvd.	Residence	Not Eligible
5JF4535	710 Wadsworth Blvd.	Residence	Not Eligible
5JF4554	999 Wadsworth Blvd.	Residence	Not Eligible
5JF4555	7630 West 10th Ave.	Residence	Not Eligible
5JF4556	975 Wadsworth Blvd.	Residence	Not Eligible
5JF4557	965 Wadsworth Blvd.	Residence	Not Eligible
5JF4558	829 Wadsworth Blvd.	Vehicle Maintenance building	Not Eligible
5JF4559	785 Wadsworth Blvd.	Restaurant	Not Eligible
5JF4560	659 Wadsworth Blvd.	Office building	Not Eligible
5JF4561	699 Wadsworth Blvd.	Strip mall	Not Eligible
5JF4562	7707 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4564	8149 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4565	8159 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4566	7400 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4567	7420 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4568	7430 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4569	594 Webster St.	Residence	Not Eligible
5JF4570	550 Webster St.	Residence	Not Eligible
5JF4571	500 Carr St.	Residence	Not Eligible
5JF4572	485 Balsam St.	Residence	Not Eligible
5JF4573	8130 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4574	8126 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4575	8020 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4576	8010 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4577	8000 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4578	7996 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4579	7960 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4580	7950 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4581	7707 West 5th Ave.	Residence	Not Eligible
5JF4582	7700 West 5th Ave.	Residence	Not Eligible
5JF4583	490 Yukon St.	Residence	Not Eligible

ELIGIBILITY RECOMMENDATIONS			
US 6 and Wadsworth Environmental Assessment			
5JF4584	7610 West 5th Ave.	Office building	Not Eligible
5JF4585	495 Wadsworth Blvd.	Gas Station	Not Eligible
5JF4587	7655 West 4th Ave.	Residence	Not Eligible
5JF4588	7630 West 4th Ave.	Residence	Not Eligible
5JF4600	600 Field St.	Residence	Not Eligible
5JF4601	605 Everett St.	Residence	Not Eligible
5JF4602	600 Everett St.	Residence	Not Eligible
5JF4603	605 Estes St.	Residence	Not Eligible
5JF4604	600 Estes St.	Residence	Not Eligible
5JF4605	605 Dudley St.	Residence	Not Eligible
5JF4606	600 Dudley St.	Residence	Not Eligible
5JF4607	605 Cody Court	Residence	Not Eligible
5JF4608	610 Cody Court	Residence	Not Eligible
5JF4609	8401 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4614	7390 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4615	7200 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4616	7150 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4617	575 Saulsbury St.	Residence	Not Eligible
5JF4618	580 Saulsbury St.	Residence	Not Eligible
5JF817.7	13th Ave. Railroad	Railroad bed	Not Eligible
5JF2268 (JEF.036)	McIntyre Gulch Culvert	Concrete box culvert	Not Eligible
5JF3013.1	Lakewood Lateral	Trench and culvert	Not Eligible
5JF4508.1	Wadsworth Blvd.	Highway	Not Eligible
5JF4508.2	Lakewood Gulch Culvert	Concrete box culvert	Not Eligible
5JF4509.1	US 6	Highway	Not Eligible
5JF4509.2	US 6 and Wadsworth Blvd. Bridge	Bridge	Not Eligible
5JF2877	1398 Wadsworth Blvd.	Commercial	Not Eligible (in APE, part of RTD West Corridor)
5JF2878	1370 Wadsworth Blvd.	Residence	Not Eligible (in APE, part of RTD West Corridor)
5JF2881	1350 Wadsworth Blvd.	Residence	Not Eligible (in APE, part of RTD West Corridor)
5JF2882	1330 Wadsworth Blvd.	Residence	Not Eligible (in APE, part of RTD West Corridor)
5JF2883	7525 West 13th Ave.	Residence	Not Eligible (in APE, part of RTD West Corridor)

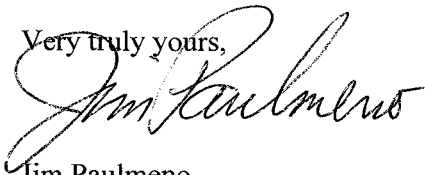
Request for Concurrence

We request your concurrence with the Determinations of Eligibility outlined in the table above. Your response is necessary for FHWA's compliance with Section 106 of the National Historic Preservation Act, and the Advisory Council on Historic Preservation's regulations.

This submittal has also been forwarded to the Jefferson County Historical Commission and the Lakewood Historical Society. We will forward their responses once we receive them.

Thank you in advance for your prompt attention to this matter. If you need further information, please contact CDOT Region 6 Historian, Dianna Litvak, at (303) 757-9461.

Very truly yours,



Jim Paulmeno
Region 6 Planning and Environmental Manager

Enclosures: Historic Resources Report
 Site Forms

cc: (without enclosures)
 Dianna Litvak, CDOT Region 6
 Kirk Webb, CDOT Region 6
 Lisa Schoch, CDOT EPB
 Seyed Kalantar, CDOT Region 6
 Marcee Allen, FHWA
 Mandy Whorton, CH2M HILL
 file

STATE OF COLORADO

DEPARTMENT OF TRANSPORTATION

Region 6, Planning and Environmental
2000 South Holly Street
Denver, CO 80222
(303) 757-9385
(303) 757-9036 FAX



July 2, 2008

Ms. Karen Zoltenko
Lakewood Historical Society
801 S. Yarrow Street
Lakewood, CO 80226

SUBJECT: Determination of Eligibility for Historic Properties – US 6 and Wadsworth Environmental Assessment, Lakewood, Colorado

Dear Ms. Zoltenko:

This letter and the attached Historic Resources Survey Report constitute the Determinations of Eligibility under Section 106 of the National Historic Preservation Act for the project referenced above. The report was prepared as part of the Environmental Assessment (EA) for the proposed construction of transportation improvements at the US 6 and Wadsworth Boulevard interchange and along Wadsworth Boulevard between 4th and 14th Avenues in Lakewood, Colorado.

Project Description

The purpose of the US 6 and Wadsworth Boulevard project is to improve traffic flow and safety, accommodate high traffic volumes, and increase multi-modal travel options and connections at the US 6 and Wadsworth Boulevard interchange and along Wadsworth Boulevard between 4th Avenue and 14th Avenue.

The project area includes US 6 (also designated as 6th Avenue) and Wadsworth Boulevard (also designated as State Highway 121). The east-west limits along US 6 are from the eastern interchange ramps with Wadsworth Boulevard west to Garrison Street. On Wadsworth Boulevard, the project limits are 4th Avenue to 14th Avenue. This area is a vital regional hub of the western Denver metropolitan area and the heart of the City of Lakewood.

The Colorado Department of Transportation (CDOT), Federal Highway Administration (FHWA), City of Lakewood, area residents, businesses, and commuters have prioritized making improvements to fix the transportation problems in the project area through previous planning efforts. CDOT's goal is to identify a proposed action that meets transportation needs, is compatible with local and regional plans, minimizes environmental harm, and can be implemented within cost constraints.

Description of the Recommended Alternative

The existing design and configuration of the interchange and roadway within the project limits have not kept pace with traffic and multi-modal travel demands. Improvements are needed to improve safety for motorists, pedestrians, and bicyclists; correct design deficiencies that contribute to safety concerns and operational inefficiencies; increase infrastructure capacity to meet current and future traffic volumes; and support multi-modal connections.

To address these needs, CDOT proposes to reconstruct the existing cloverleaf interchange and replace it with a tight diamond with a loop in the northwest quadrant. The loop provides the greatest capacity for the highest volume traffic movement: westbound to southbound in the evening rush hour. The new interchange would provide adequate acceleration and deceleration lanes, improve intersection spacing, and remove weaving conflicts. Existing noise walls on the east side of Wadsworth near the interchange would be reconstructed where needed, and noise walls would be extended between US 6 and the US 6 frontage road west of Wadsworth to commercial developments at Garrison Street. The existing US 6 bridge over Wadsworth would be reconstructed, and its profile would be raised to accommodate a longer span across Wadsworth.

CDOT also proposes to widen Wadsworth Boulevard between 4th and 14th Avenues to provide an additional travel lane in each direction and detached sidewalks along both sides of the highway; these improvements would complement improvements north and south of the project area. In addition, access along Wadsworth would be controlled with a raised median.

Additional detail on the Recommended Alternative will be submitted as part of the Effects Determination for this project.

Eligibility Determinations

A total of 275 cultural resources are located within the boundaries of the project area. Of these, 156 resources were constructed in or before 1965. The date 1965 was chosen because it is CDOT's standard practice to evaluate historic resources that are 45 years or older in order to allow for a period for completion of final design and construction for the proposed project. Eighteen of the 156 pre-1966 historic resources have been previously surveyed and evaluated. Due to changes to the resources, or to augment or correct existing inventory forms, 13 of these 18 historic resources were resurveyed in this inventory. The remaining 5 resources not resurveyed have previously received official determinations of eligibility and did not warrant additional survey. Therefore, a total of 151 cultural resources (156 resources minus the 5 resources not resurveyed) were surveyed in this inventory. These 151 resources consist of 137 architectural resources, 7 landscape features, and 7 linear resources.

Results of this cultural resources inventory recommend that 9 historic resources are individually eligible for nomination to the NRHP. Additionally, three potential historic districts have been identified within the project area. A recommended Lakewood School Historic District consists of 6 buildings and structures and 2 landscape features that are recommended as contributing resources, and eleven buildings and structures and 5 landscape features are recommended as noncontributing to this potential historic district. A recommended Green Acres Historic District includes 20 contributing buildings and 10 noncontributing buildings within the project area. Finally, a recommended Meadowlark Hills Historic District includes 10 contributing buildings and one noncontributing building within the project area. The remaining historic resources surveyed in this inventory consist of 70 architectural resources and 7 linear resources, and all of these are recommended to be not eligible for nomination to the NRHP.

The eligibility determinations for all surveyed resources are summarized in the following table.

ELIGIBILITY RECOMMENDATIONS			
US 6 and Wadsworth Environmental Assessment			
Site No.	Address	Description	NRHP Eligibility Recommendation
5JF3548	7395 6th Ave. Frontage Rd.	Residence	Eligible, Criterion C
5JF3549	7423 6th Ave. Frontage Rd.	Residence	Eligible, Criterion C
5JF3554	7558 West 9th Ave.	Residence	Eligible, Criterion C
5JF4511	1215 Wadsworth Blvd.	Residence	Eligible, Criterion A
5JF4513	1230 Wadsworth Blvd.	Residence	Eligible, Criterion C
5JF4536	700 Wadsworth Blvd.	Residence	Eligible, Criterion C
5JF4542	7433 6th Ave. Frontage Rd.	Residence	Eligible, Criterion C
5JF4563	8125 6th Ave. Frontage Rd.	Residence	Eligible, Criterion C
5JF4586	401 Wadsworth Blvd.	Gas Station	Eligible, Criteria A and C
5JF4512	1015 Wadsworth Blvd.	Building 1: New America School	Contributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Building 2: Elementary School	Contributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Building 3: Gymnasium	Contributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Building 7: Junior High	Contributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Structure 10: World War II Memorial	Contributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Structure 12: Storage Outbuilding	Contributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Memorial Field	Contributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Landscaped areas adjacent to Buildings 1, 2, 3, 7	Contributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Building 4: Cafeteria and Library	Noncontributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Building 5: Administration and Arts	Noncontributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Building 6: Connecting Building	Noncontributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Building 8: Theater	Noncontributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Structure 9: Outbuilding	Noncontributing, Lakewood School Historic District

ELIGIBILITY RECOMMENDATIONS			
US 6 and Wadsworth Environmental Assessment			
5JF4512	1015 Wadsworth Blvd.	Structure 11: Storage Outbuilding	Noncontributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Structure 13: Storage Outbuilding	Noncontributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Structure 14: Concessions Building	Noncontributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Structure 15: Storage Outbuilding	Noncontributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Structure 16: Bleachers	Noncontributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Structure 17: Ticket Booth	Noncontributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Baseball Field	Noncontributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Parking Areas	Noncontributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Playground	Noncontributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Landscaped areas adjacent to Buildings 4, 5, 6, and 8	Noncontributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Fences	Noncontributing, Lakewood School Historic District
5JF3610	700 Park Ln.	Residence	Contributing, Green Acres Historic District
5JF4526	7445 Highland Dr.	Residence	Contributing, Green Acres Historic District
5JF4527	7415 Highland Dr.	Residence	Contributing, Green Acres Historic District
5JF4528	7385 Highland Dr.	Residence	Contributing, Green Acres Historic District
5JF4529	705 Emerald Ln.	Residence	Contributing, Green Acres Historic District
5JF4530	715 Emerald Ln.	Residence	Contributing, Green Acres Historic District
5JF4537	7500 Broadview Dr.	Residence	Contributing, Green Acres Historic District
5JF4541	7370 Broadview Dr.	Residence	Contributing, Green Acres Historic District
5JF4543	740 Emerald Ln.	Residence	Contributing, Green Acres Historic District
5JF4544	730 Emerald Ln.	Residence	Contributing, Green Acres Historic District
5JF4545	701 Park Ln.	Residence	Contributing, Green Acres Historic District
5JF4546	721 Park Ln.	Residence	Contributing, Green Acres Historic District
5JF4547	741 Park Ln.	Residence	Contributing, Green Acres Historic District
5JF4548	761 Park Ln.	Residence	Contributing, Green Acres Historic District
5JF4549	781 Park Ln.	Residence	Contributing, Green Acres Historic District
5JF4550	790 Park Ln.	Residence	Contributing, Green Acres Historic District

ELIGIBILITY RECOMMENDATIONS			
US 6 and Wadsworth Environmental Assessment			
5JF4551	660 Park Ln.	Residence	Contributing, Green Acres Historic District
5JF4552	680 Park Ln.	Residence	Contributing, Green Acres Historic District
5JF4553	720 Park Ln.	Residence	Contributing, Green Acres Historic District
5JF4636	710 Emerald Ln.	Residence	Contributing, Green Acres Historic District
5JF4531	725 Emerald Ln.	Residence	Noncontributing, Green Acres Historic District
5JF4532	765 Emerald Ln.	Residence	Noncontributing, Green Acres Historic District
5JF4538	7474 Broadview Dr.	Residence	Noncontributing, Green Acres Historic District
5JF4539	7460 Broadview Dr.	Residence	Noncontributing, Green Acres Historic District
5JF4540	7400 Broadview Dr.	Residence	Noncontributing, Green Acres Historic District
5JF4610	7393 6th Ave. Frontage Rd.	Residence	Noncontributing, Green Acres Historic District
5JF4611	7373 6th Ave. Frontage Rd.	Residence	Noncontributing, Green Acres Historic District
5JF4612	7225 6th Ave. Frontage Rd.	Residence	Noncontributing, Green Acres Historic District
5JF4613	7199 6th Ave. Frontage Rd.	Residence	Noncontributing, Green Acres Historic District
5JF4637	720 Emerald Ln.	Residence	Noncontributing, Green Acres Historic District
5JF4590	545 Meadowlark Dr.	Residence	Contributing, Meadowlark Hills Historic District
5JF4591	540 Meadowlark Dr.	Residence	Contributing, Meadowlark Hills Historic District
5JF4592	555 Everett St.	Residence	Contributing, Meadowlark Hills Historic District
5JF4593	560 Everett St.	Residence	Contributing, Meadowlark Hills Historic District
5JF4594	585 Estes St.	Residence	Contributing, Meadowlark Hills Historic District
5JF4595	580 Estes St.	Residence	Contributing, Meadowlark Hills Historic District
5JF4596	585 Dudley St.	Residence	Contributing, Meadowlark Hills Historic District
5JF4597	580 Dudley St.	Residence	Contributing, Meadowlark Hills Historic District
5JF4598	585 Dover St.	Residence	Contributing, Meadowlark Hills Historic District
5JF4599	590 Dover St.	Residence	Contributing, Meadowlark Hills Historic District
5JF4589	9050 6th Ave.	Shopping Center	Noncontributing, Meadowlark Hills Historic District
5JF2907	7550 West 13th Ave.	Residence	Not Eligible
5JF2908	1296 Wadsworth Blvd.	Residence	Not Eligible
5JF2911	1370 Yukon St.	Residence	Not Eligible
5JF2912	1355 Wadsworth Blvd.	Residence	Not Eligible
5JF2913	1345 Wadsworth Blvd.	Residence	Not Eligible
5JF2914	1360 Yukon St.	Residence	Not Eligible
5JF4510	Northwest Cloverleaf Ruin	Residence	Not Eligible
5JF4514	1224 Wadsworth Blvd.	Residence	Not Eligible
5JF4515	1220 Wadsworth Blvd.	Residence	Not Eligible
5JF4516	1208 Wadsworth Blvd.	Residence	Not Eligible
5JF4517	1204 Wadsworth Blvd.	Residence	Not Eligible
5JF4518	1200 Wadsworth Blvd.	Residence	Not Eligible
5JF4519	1190 Wadsworth Blvd.	Residence	Not Eligible

ELIGIBILITY RECOMMENDATIONS			
US 6 and Wadsworth Environmental Assessment			
5JF4520	1050 Wadsworth Blvd.	Office building	Not Eligible
5JF4521	1000 Wadsworth Blvd.	Auto service station	Not Eligible
5JF4522	7585 West 10th Ave.	Converted residence	Not Eligible
5JF4523	900 Wadsworth Blvd.	Residence	Not Eligible
5JF4524	850 Wadsworth Blvd.	Residence	Not Eligible
5JF4525	820 Wadsworth Blvd.	Commercial equipment sales / rental	Not Eligible
5JF4533	730 Wadsworth Blvd.	Residence	Not Eligible
5JF4534	720 Wadsworth Blvd.	Residence	Not Eligible
5JF4535	710 Wadsworth Blvd.	Residence	Not Eligible
5JF4554	999 Wadsworth Blvd.	Residence	Not Eligible
5JF4555	7630 West 10th Ave.	Residence	Not Eligible
5JF4556	975 Wadsworth Blvd.	Residence	Not Eligible
5JF4557	965 Wadsworth Blvd.	Residence	Not Eligible
5JF4558	829 Wadsworth Blvd.	Vehicle Maintenance building	Not Eligible
5JF4559	785 Wadsworth Blvd.	Restaurant	Not Eligible
5JF4560	659 Wadsworth Blvd.	Office building	Not Eligible
5JF4561	699 Wadsworth Blvd.	Strip mall	Not Eligible
5JF4562	7707 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4564	8149 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4565	8159 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4566	7400 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4567	7420 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4568	7430 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4569	594 Webster St.	Residence	Not Eligible
5JF4570	550 Webster St.	Residence	Not Eligible
5JF4571	500 Carr St.	Residence	Not Eligible
5JF4572	485 Balsam St.	Residence	Not Eligible
5JF4573	8130 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4574	8126 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4575	8020 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4576	8010 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4577	8000 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4578	7996 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4579	7960 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4580	7950 6th Ave. Frontage Rd.	Residence	Not Eligible

ELIGIBILITY RECOMMENDATIONS			
US 6 and Wadsworth Environmental Assessment			
5JF4581	7707 West 5th Ave.	Residence	Not Eligible
5JF4582	7700 West 5th Ave.	Residence	Not Eligible
5JF4583	490 Yukon St.	Residence	Not Eligible
5JF4584	7610 West 5th Ave.	Office building	Not Eligible
5JF4585	495 Wadsworth Blvd.	Gas Station	Not Eligible
5JF4587	7655 West 4th Ave.	Residence	Not Eligible
5JF4588	7630 West 4th Ave.	Residence	Not Eligible
5JF4600	600 Field St.	Residence	Not Eligible
5JF4601	605 Everett St.	Residence	Not Eligible
5JF4602	600 Everett St.	Residence	Not Eligible
5JF4603	605 Estes St.	Residence	Not Eligible
5JF4604	600 Estes St.	Residence	Not Eligible
5JF4605	605 Dudley St.	Residence	Not Eligible
5JF4606	600 Dudley St.	Residence	Not Eligible
5JF4607	605 Cody Court	Residence	Not Eligible
5JF4608	610 Cody Court	Residence	Not Eligible
5JF4609	8401 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4614	7390 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4615	7200 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4616	7150 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4617	575 Saulsbury St.	Residence	Not Eligible
5JF4618	580 Saulsbury St.	Residence	Not Eligible
5JF817.7	13th Ave. Railroad	Railroad bed	Not Eligible
5JF2268 (JEF.036)	McIntyre Gulch Culvert	Concrete box culvert	Not Eligible
5JF3013.1	Lakewood Lateral	Trench and culvert	Not Eligible
5JF4508.1	Wadsworth Blvd.	Highway	Not Eligible
5JF4508.2	Lakewood Gulch Culvert	Concrete box culvert	Not Eligible
5JF4509.1	US 6	Highway	Not Eligible
5JF4509.2	US 6 and Wadsworth Blvd. Bridge	Bridge	Not Eligible
5JF2877	1398 Wadsworth Blvd.	Commercial	Not Eligible (in APE, part of RTD West Corridor)
5JF2878	1370 Wadsworth Blvd.	Residence	Not Eligible (in APE, part of RTD West Corridor)
5JF2881	1350 Wadsworth Blvd.	Residence	Not Eligible (in APE, part of RTD West Corridor)
5JF2882	1330 Wadsworth Blvd.	Residence	Not Eligible (in APE, part of RTD West Corridor)
5JF2883	7525 West 13th Ave.	Residence	Not Eligible (in APE, part of RTD West Corridor)

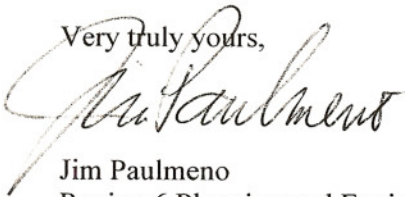
Request for Comments

As a consulting party with interest in these historic properties, we welcome your comments on the determination of eligibility outlined in the table above. Should you elect to respond, we request that you do so within 30 days of receipt of this letter.

This submittal has also been forwarded to the State Historic Preservation Office and the Jefferson County Planning and Zoning Department. We will forward their responses once we receive them.

Thank you in advance for your prompt attention to this matter. If you need further information, please contact CDOT Region 6 Senior Staff Historian, Dianna Litvak, at 303-757-9461.

Very truly yours,



Jim Paulmeno
Region 6 Planning and Environmental Manager

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cc: (without enclosures)
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STATE OF COLORADO

DEPARTMENT OF TRANSPORTATION

Region 6, Planning and Environmental
2000 South Holly Street
Denver, CO 80222
(303) 757-9385
(303) 757-9036 FAX



July 2, 2008

Mr. Dennis Dempsey
Jefferson County Planning and Zoning Department
100 Jefferson County Parkway
Suite 3550
Golden, CO 80419-3550

SUBJECT: Determination of Eligibility for Historic Properties – US 6 and Wadsworth Environmental Assessment, Lakewood, Colorado

Dear Mr. Dempsey:

This letter and the attached Historic Resources Survey Report constitute the Determinations of Eligibility under Section 106 of the National Historic Preservation Act for the project referenced above. The report was prepared as part of the Environmental Assessment (EA) for the proposed construction of transportation improvements at the US 6 and Wadsworth Boulevard interchange and along Wadsworth Boulevard between 4th and 14th Avenues in Lakewood, Colorado.

Project Description

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concerns and operational inefficiencies; increase infrastructure capacity to meet current and future traffic volumes; and support multi-modal connections.

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CDOT also proposes to widen Wadsworth Boulevard between 4th and 14th Avenues to provide an additional travel lane in each direction and detached sidewalks along both sides of the highway; these improvements would complement improvements north and south of the project area. In addition, access along Wadsworth would be controlled with a raised median.

Additional detail on the Recommended Alternative will be submitted as part of the Effects Determination for this project.

Eligibility Determinations

A total of 275 cultural resources are located within the boundaries of the project area. Of these, 156 resources were constructed in or before 1965. The date 1965 was chosen because it is CDOT's standard practice to evaluate historic resources that are 45 years or older in order to allow for a period for completion of final design and construction for the proposed project. Eighteen of the 156 pre-1966 historic resources have been previously surveyed and evaluated. Due to changes to the resources, or to augment or correct existing inventory forms, 13 of these 18 historic resources were resurveyed in this inventory. The remaining 5 resources not resurveyed have previously received official determinations of eligibility and did not warrant additional survey. Therefore, a total of 151 cultural resources (156 resources minus the 5 resources not resurveyed) were surveyed in this inventory. These 151 resources consist of 137 architectural resources, 7 landscape features, and 7 linear resources.

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5JF4513	1230 Wadsworth Blvd.	Residence	Eligible, Criterion C
5JF4536	700 Wadsworth Blvd.	Residence	Eligible, Criterion C
5JF4542	7433 6th Ave. Frontage Rd.	Residence	Eligible, Criterion C
5JF4563	8125 6th Ave. Frontage Rd.	Residence	Eligible, Criterion C
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5JF4512	1015 Wadsworth Blvd.	Structure 15: Storage Outbuilding	Noncontributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Structure 16: Bleachers	Noncontributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Structure 17: Ticket Booth	Noncontributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Baseball Field	Noncontributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Parking Areas	Noncontributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Playground	Noncontributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Landscaped areas adjacent to Buildings 4, 5, 6, and 8	Noncontributing, Lakewood School Historic District
5JF4512	1015 Wadsworth Blvd.	Fences	Noncontributing, Lakewood School Historic District
5JF3610	700 Park Ln.	Residence	Contributing, Green Acres Historic District
5JF4526	7445 Highland Dr.	Residence	Contributing, Green Acres Historic District
5JF4527	7415 Highland Dr.	Residence	Contributing, Green Acres Historic District
5JF4528	7385 Highland Dr.	Residence	Contributing, Green Acres Historic District
5JF4529	705 Emerald Ln.	Residence	Contributing, Green Acres Historic District
5JF4530	715 Emerald Ln.	Residence	Contributing, Green Acres Historic District
5JF4537	7500 Broadview Dr.	Residence	Contributing, Green Acres Historic District
5JF4541	7370 Broadview Dr.	Residence	Contributing, Green Acres Historic District
5JF4543	740 Emerald Ln.	Residence	Contributing, Green Acres Historic District
5JF4544	730 Emerald Ln.	Residence	Contributing, Green Acres Historic District
5JF4545	701 Park Ln.	Residence	Contributing, Green Acres Historic District
5JF4546	721 Park Ln.	Residence	Contributing, Green Acres Historic District
5JF4547	741 Park Ln.	Residence	Contributing, Green Acres Historic District
5JF4548	761 Park Ln.	Residence	Contributing, Green Acres Historic District
5JF4549	781 Park Ln.	Residence	Contributing, Green Acres Historic District
5JF4550	790 Park Ln.	Residence	Contributing, Green Acres Historic District

ELIGIBILITY RECOMMENDATIONS			
US 6 and Wadsworth Environmental Assessment			
5JF4551	660 Park Ln.	Residence	Contributing, Green Acres Historic District
5JF4552	680 Park Ln.	Residence	Contributing, Green Acres Historic District
5JF4553	720 Park Ln.	Residence	Contributing, Green Acres Historic District
5JF4636	710 Emerald Ln.	Residence	Contributing, Green Acres Historic District
5JF4531	725 Emerald Ln.	Residence	Noncontributing, Green Acres Historic District
5JF4532	765 Emerald Ln.	Residence	Noncontributing, Green Acres Historic District
5JF4538	7474 Broadview Dr.	Residence	Noncontributing, Green Acres Historic District
5JF4539	7460 Broadview Dr.	Residence	Noncontributing, Green Acres Historic District
5JF4540	7400 Broadview Dr.	Residence	Noncontributing, Green Acres Historic District
5JF4610	7393 6th Ave. Frontage Rd.	Residence	Noncontributing, Green Acres Historic District
5JF4611	7373 6th Ave. Frontage Rd.	Residence	Noncontributing, Green Acres Historic District
5JF4612	7225 6th Ave. Frontage Rd.	Residence	Noncontributing, Green Acres Historic District
5JF4613	7199 6th Ave. Frontage Rd.	Residence	Noncontributing, Green Acres Historic District
5JF4637	720 Emerald Ln.	Residence	Noncontributing, Green Acres Historic District
5JF4590	545 Meadowlark Dr.	Residence	Contributing, Meadowlark Hills Historic District
5JF4591	540 Meadowlark Dr.	Residence	Contributing, Meadowlark Hills Historic District
5JF4592	555 Everett St.	Residence	Contributing, Meadowlark Hills Historic District
5JF4593	560 Everett St.	Residence	Contributing, Meadowlark Hills Historic District
5JF4594	585 Estes St.	Residence	Contributing, Meadowlark Hills Historic District
5JF4595	580 Estes St.	Residence	Contributing, Meadowlark Hills Historic District
5JF4596	585 Dudley St.	Residence	Contributing, Meadowlark Hills Historic District
5JF4597	580 Dudley St.	Residence	Contributing, Meadowlark Hills Historic District
5JF4598	585 Dover St.	Residence	Contributing, Meadowlark Hills Historic District
5JF4599	590 Dover St.	Residence	Contributing, Meadowlark Hills Historic District
5JF4589	9050 6th Ave.	Shopping Center	Noncontributing, Meadowlark Hills Historic District
5JF2907	7550 West 13th Ave.	Residence	Not Eligible
5JF2908	1296 Wadsworth Blvd.	Residence	Not Eligible
5JF2911	1370 Yukon St.	Residence	Not Eligible
5JF2912	1355 Wadsworth Blvd.	Residence	Not Eligible
5JF2913	1345 Wadsworth Blvd.	Residence	Not Eligible
5JF2914	1360 Yukon St.	Residence	Not Eligible
5JF4510	Northwest Cloverleaf Ruin	Residence	Not Eligible
5JF4514	1224 Wadsworth Blvd.	Residence	Not Eligible
5JF4515	1220 Wadsworth Blvd.	Residence	Not Eligible
5JF4516	1208 Wadsworth Blvd.	Residence	Not Eligible
5JF4517	1204 Wadsworth Blvd.	Residence	Not Eligible
5JF4518	1200 Wadsworth Blvd.	Residence	Not Eligible
5JF4519	1190 Wadsworth Blvd.	Residence	Not Eligible

ELIGIBILITY RECOMMENDATIONS			
US 6 and Wadsworth Environmental Assessment			
5JF4520	1050 Wadsworth Blvd.	Office building	Not Eligible
5JF4521	1000 Wadsworth Blvd.	Auto service station	Not Eligible
5JF4522	7585 West 10th Ave.	Converted residence	Not Eligible
5JF4523	900 Wadsworth Blvd.	Residence	Not Eligible
5JF4524	850 Wadsworth Blvd.	Residence	Not Eligible
5JF4525	820 Wadsworth Blvd.	Commercial equipment sales / rental	Not Eligible
5JF4533	730 Wadsworth Blvd.	Residence	Not Eligible
5JF4534	720 Wadsworth Blvd.	Residence	Not Eligible
5JF4535	710 Wadsworth Blvd.	Residence	Not Eligible
5JF4554	999 Wadsworth Blvd.	Residence	Not Eligible
5JF4555	7630 West 10th Ave.	Residence	Not Eligible
5JF4556	975 Wadsworth Blvd.	Residence	Not Eligible
5JF4557	965 Wadsworth Blvd.	Residence	Not Eligible
5JF4558	829 Wadsworth Blvd.	Vehicle Maintenance building	Not Eligible
5JF4559	785 Wadsworth Blvd.	Restaurant	Not Eligible
5JF4560	659 Wadsworth Blvd.	Office building	Not Eligible
5JF4561	699 Wadsworth Blvd.	Strip mall	Not Eligible
5JF4562	7707 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4564	8149 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4565	8159 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4566	7400 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4567	7420 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4568	7430 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4569	594 Webster St.	Residence	Not Eligible
5JF4570	550 Webster St.	Residence	Not Eligible
5JF4571	500 Carr St.	Residence	Not Eligible
5JF4572	485 Balsam St.	Residence	Not Eligible
5JF4573	8130 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4574	8126 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4575	8020 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4576	8010 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4577	8000 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4578	7996 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4579	7960 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4580	7950 6th Ave. Frontage Rd.	Residence	Not Eligible

ELIGIBILITY RECOMMENDATIONS			
US 6 and Wadsworth Environmental Assessment			
5JF4581	7707 West 5th Ave.	Residence	Not Eligible
5JF4582	7700 West 5th Ave.	Residence	Not Eligible
5JF4583	490 Yukon St.	Residence	Not Eligible
5JF4584	7610 West 5th Ave.	Office building	Not Eligible
5JF4585	495 Wadsworth Blvd.	Gas Station	Not Eligible
5JF4587	7655 West 4th Ave.	Residence	Not Eligible
5JF4588	7630 West 4th Ave.	Residence	Not Eligible
5JF4600	600 Field St.	Residence	Not Eligible
5JF4601	605 Everett St.	Residence	Not Eligible
5JF4602	600 Everett St.	Residence	Not Eligible
5JF4603	605 Estes St.	Residence	Not Eligible
5JF4604	600 Estes St.	Residence	Not Eligible
5JF4605	605 Dudley St.	Residence	Not Eligible
5JF4606	600 Dudley St.	Residence	Not Eligible
5JF4607	605 Cody Court	Residence	Not Eligible
5JF4608	610 Cody Court	Residence	Not Eligible
5JF4609	8401 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4614	7390 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4615	7200 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4616	7150 6th Ave. Frontage Rd.	Residence	Not Eligible
5JF4617	575 Saulsbury St.	Residence	Not Eligible
5JF4618	580 Saulsbury St.	Residence	Not Eligible
5JF817.7	13th Ave. Railroad	Railroad bed	Not Eligible
5JF2268 (JEF.036)	McIntyre Gulch Culvert	Concrete box culvert	Not Eligible
5JF3013.1	Lakewood Lateral	Trench and culvert	Not Eligible
5JF4508.1	Wadsworth Blvd.	Highway	Not Eligible
5JF4508.2	Lakewood Gulch Culvert	Concrete box culvert	Not Eligible
5JF4509.1	US 6	Highway	Not Eligible
5JF4509.2	US 6 and Wadsworth Blvd. Bridge	Bridge	Not Eligible
5JF2877	1398 Wadsworth Blvd.	Commercial	Not Eligible (in APE, part of RTD West Corridor)
5JF2878	1370 Wadsworth Blvd.	Residence	Not Eligible (in APE, part of RTD West Corridor)
5JF2881	1350 Wadsworth Blvd.	Residence	Not Eligible (in APE, part of RTD West Corridor)
5JF2882	1330 Wadsworth Blvd.	Residence	Not Eligible (in APE, part of RTD West Corridor)
5JF2883	7525 West 13th Ave.	Residence	Not Eligible (in APE, part of RTD West Corridor)

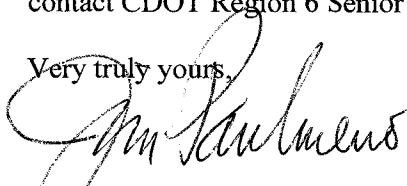
Request for Comments

As a consulting party with interest in these historic properties, we welcome your comments on the determination of eligibility outlined in the table above. Should you elect to respond, we request that you do so within 30 days of receipt of this letter.

This submittal has also been forwarded to the State Historic Preservation Office and the Lakewood Historical Society. We will forward their responses once we receive them.

Thank you in advance for your prompt attention to this matter. If you need further information, please contact CDOT Region 6 Senior Staff Historian, Dianna Litvak, at 303-757-9461.

Very truly yours,



Jim Paulmeno
Region 6 Planning and Environmental Manager

Enclosures:

Historic Resources Report
Site Forms

cc: (without enclosures)
Dianna Litvak, CDOT Region 6
Kirk Webb, CDOT Region 6
Lisa Schoch, CDOT EPB
Seyed Kalantar, CDOT Region 6
Marcee Allen, FHWA
Edward Nichols, SHPO
Mandy Whorton, CH2M HILL
File



OFFICE of ARCHAEOLOGY and HISTORIC PRESERVATION

August 7, 2008

Jim Paulmeno
Region 6 Planning and Environmental Manager
Colorado Department of Transportation, Region 6
2000 South Holly Street
Denver, CO 80222

Re: US 6 and Wadsworth Environmental Assessment, Lakewood, CO (CHS #50636)

Dear Mr. Paulmeno,

Thank you for your correspondence dated July 2, 2008 and received by our office on that same date regarding the consultation of the above-mentioned project under Section 106 of the National Historic Preservation Act (Section 106).

After review of the provided additional information, we concur with the recommended finding of eligible for the National Register of Historic Places (NRHP) for the resources listed below.

- 5JF.3548
- 5JF.3549
- 5JF.3554
- 5JF.4511. The site form under item 40 states that the period of significance for the resource is 1920-50 (National Register Criterion A) and under item 43 the period of significance is stated as being from 1948-49. Please more clearly define the period of significance(s) under the National Register criteria.
- 5JF.4513
- 5JF.4536
- 5JF.4542
- 5JF.4563
- 5JF.4586. The site form under item 40 states that the period of significance for the resource is 1958, the date of construction. We agree that this date is appropriate for National Register Criterion C (architecture), but not for National Register Criterion A (transportation/automobile growth). We recommend that the period of significance be reconsidered for National Register Criterion A and be extended beyond one year, 1958, as appropriate.
- 5JF.4512. We concur that the resource is eligible as a historic district, but request additional information regarding the period of significance. According to item 40 on the site form, the period of significance is 1927 to 1958. Item 45 states that the 1958 date is justified due to landscape changes of the mid-1950 and the 50-year rule. A period of significance for a historic district can extend beyond the 50-year rule cutoff date and not have to meet the exceptional significance threshold of National Register Criterion Consideration G. Please provide a clearer justification for the end date for the period of significance. The end date should be based on a significant event, such as the end of construction of buildings, a fundamental change in school administration, or the school closing or changing ownership. It is not uncommon to have the end period of significance extend into the 1960s or even 1970s if appropriate.

- After review of the Green Acres Historic District, we concur that the historic district is eligible to the NRHP. We have provided comments below on the survey properties that we have additional questions. We concur with the other contributing and noncontributing recommendations.
 - 5JF.4610. The site form states that this resource is not a good example of a split level type. However, the site form does not include a justification or explanation why the property is not a good example. Item 43 states that the property has good historic physical integrity but noncontributing is marked under item 45. Please provide additional information on why this property is not a good example of the split level type and noncontributing to the historic district.
- After review of the Meadowlark Hills Historic District, we concur that the historic district is eligible to the NRHP. We concur with the recommended contributing and noncontributing status of the surveyed properties within the historic district. Staff attached copies of Table 8 from the survey report to each site form because the site forms did not state which model was being surveyed. In future submissions, we recommend that you include information regarding the surveyed models in each site form.
 - 5JF.4596. The text in item 45 of the site form describes the property as contributing to the historic district, but the contributing/noncontributing section is not marked. Staff marked the property as contributing.

After review of the submitted information, we concur with the recommendation of not eligible for the NRHP for the properties listed below.

- | | | |
|------------|------------|------------|
| • 5JF.4566 | • 5JF.4604 | • 5JF.4518 |
| • 5JF.4567 | • 5JF.4605 | • 5JF.4519 |
| • 5JF.4568 | • 5JF.4606 | • 5JF.4520 |
| • 5JF.4569 | • 5JF.4607 | • 5JF.4521 |
| • 5JF.4570 | • 5JF.4608 | • 5JF.4522 |
| • 5JF.4571 | • 5JF.4609 | • 5JF.4523 |
| • 5JF.4572 | • 5JF.4614 | • 5JF.4524 |
| • 5JF.4575 | • 5JF.4615 | • 5JF.4525 |
| • 5JF.4576 | • 5JF.4616 | • 5JF.4533 |
| • 5JF.4577 | • 5JF.4617 | • 5JF.4534 |
| • 5JF.4578 | • 5JF.4618 | • 5JF.4535 |
| • 5JF.4579 | • 5JF.2907 | • 5JF.4554 |
| • 5JF.4580 | • 5JF.2908 | • 5JF.4555 |
| • 5JF.4581 | • 5JF.2911 | • 5JF.4556 |
| • 5JF.4582 | • 5JF.2912 | • 5JF.4557 |
| • 5JF.4584 | • 5JF.2913 | • 5JF.4558 |
| • 5JF.4585 | • 5JF.2914 | • 5JF.4559 |
| • 5JF.4587 | • 5JF.4510 | • 5JF.4560 |
| • 5JF.4588 | • 5JF.4514 | • 5JF.4561 |
| • 5JF.4600 | • 5JF.4515 | • 5JF.4562 |
| • 5JF.4601 | • 5JF.4516 | • 5JF.4564 |
| • 5JF.4603 | • 5JF.4517 | • 5JF.4565 |
| • 5JF.4602 | | |

Please refer to our comments below in regards to the linear resources.

- 5JF.817.7. We are unable to concur with the recommendation that the entire Denver & Intermountain Railroad is not eligible to the NRHP. The site forms provide information on segment 5JF.817.7 and not for the entire railroad. We also are not able to concur with the finding of not eligible to the NRHP for the segment 5JF.817.7. A segment of a resource can not be eligible or not eligible. A segment can support or not support the overall eligibility of the entire linear resource. In our opinion, the entire linear resource of 5JF.817 is eligible for the NRHP and segment 5JF.817.7 lacks integrity and does not support the overall eligibility of the entire linear resource.
- 5JF.2268. We concur that this culvert is not eligible for the NRHP.
- 5JF.3013.1. The entire linear resource has not been evaluated for NRHP eligibility. In our opinion, the entire linear resource is eligible to the NRHP and the surveyed segment lacks integrity and does not support the overall eligibility of the entire linear resource.

Please refer to our comments below for resources that we were unable to concur on the recommended eligibility finding.

- 5JF.4509.1 and 5JF.4509.2. US 6 has been determined eligible by CDOT and our office in past projects, but item 17 on the Linear Component Form recommends the entire linear resource as not eligible to the NRHP. In our opinion, there is not enough information on the site form to concur with the recommended finding of not eligible for US 6. 5JF.4509.2 is an interchange located within the surveyed segment of 5JF.4509.1. We recommend that the interchange be evaluated within the segment 5JF.4509.1. In our opinion, the surveyed segment, 5JF.4509.1, (which would encompass 5JF.4509.2) does not support the overall eligibility of the entire linear resource of US 6.
- 5JF.4508.1 and 5JF.4508.2. In our opinion, the culvert surveyed as 5JF.4508.2 should be incorporated into the site form for 5JF.4508.1 because it is located within that surveyed segment. The site form for 5JF.4508.1 states that the entire linear feature of Wadsworth Boulevard is not eligible for the NRHP, but the site form only provides detailed information on the surveyed segment of Wadsworth Boulevard, 5JF.4508.1. In our opinion, the surveyed segment 5JF.4508.1 (which would encompass 5JF.4508.2) would not support the overall eligibility of Wadsworth Boulevard.
- 5JF.4573 and 5JF.4574. The provided photographs are difficult to review because the surveyed properties are hidden behind tall fences.
- 5JF.4583. We do not concur that this property is not individually eligible for the NRHP. In our opinion, the property is significant under National Register Criterion C for architecture at the local level of significance as a good example of the Contemporary style which was popular from the 1940s to 1980s. The property, built in 1955, retains the character-defining features of the Contemporary style including a low-pitched side gabled roof with exposed roof beams, brick façade (roman brick), large window openings lacking in detail, and strong horizontal appearance.

We request being involved in the consultation process with the local government, which as stipulated in 36 CFR 800.3 is required to be notified of the undertaking, and with other consulting parties. Additional information provided by the local government or consulting parties might cause our office to re-evaluate our eligibility and potential effect findings.

Please note that our compliance letter does not end the 30-day review period provided to other consulting parties. If we may be of further assistance, please contact Amy Pallante, our Section 106 Compliance Coordinator, at (303) 866-4678.

Sincerely,



Edward C. Nichols
State Historic Preservation Officer

STATE OF COLORADO

DEPARTMENT OF TRANSPORTATION

Region 6, Planning and Environmental
2000 South Holly Street
Denver, CO 80222
(303) 757-9385
(303) 757-9036 FAX



October 10, 2008

Mr. Edward C. Nichols
State Historic Preservation Officer
Colorado Historical Society
1300 Broadway
Denver, CO 80203

SUBJECT: Additional Eligibility Issues, US6 and Wadsworth Environmental Assessment, Lakewood, CO
(CHS #50636)

Dear Mr. Nichols,

This letter and the attached materials constitute CDOT's response to your questions regarding eligibility determinations for several properties within the project area in correspondence dated August 7, 2008. We have made revisions to the original site forms and are returning these to you. We are also submitting the Final Historic Resources Survey Report.

5JF4511: We have revised the period of significance for the resource as 1948-1949 to be consistent with the discussion under item 43.

5JF4586: We have revised the period of significance for the resource as 1958-1961. The building was constructed in 1958, and Wadsworth was widened in 1961, providing a significant event to frame a period of significance.

5JF4512: The period of significance for the NRHP-Eligible Lakewood School Historic District has been extended from 1927 through 1977. This period now includes the 1968-70 Administration and Arts Building (Building 5) as a contributing building to the historic district. In 1977, the Lakewood School was converted into the Jefferson County Open School and the school underwent an administrative change. After that point, new construction took on a different form on the school property. New buildings were subsequently built behind the earlier buildings and setback from view from 10th Avenue, the school's primary street front. This is seen in Building 4 (Cafeteria and Library) and Building 6 (Connecting Building), both of which were built assuming a deferential position in terms of minimalist style and their placement behind the earlier buildings on the property.

5JF4610: We maintain that this house is not a Split Level house, but a two story house because there are only two levels present. The one-car garage underneath the second story is at the same level as the main portion of the house. However, because of the house's odd form due to the small, off-center second story, the architectural style is designated "Other Style." Building permit research suggests that if this second story was a later addition, it was added sometime before 1970. Regardless, however, the house's unusual shape does not conform to any one of the accepted residential architectural styles as described in the OAHP's *Field Guide to Colorado's*

Historic Architecture & Engineering or in Virginia and Lee McAlester's *A Field Guide to American Houses*, and thus is not recommended eligible for the National Register under Criterion C.

5JF.817.7: We have made the changes to items 36 and 37 of the Management Data Form stating that 5JF8717 is eligible for the NRHP and 5JF817.7 lacks integrity and does not support the overall eligibility of the entire linear resource.

5JF3013.1. We have made the following changes to item 13 of the re-evaluation form, stating that this segment of the Lakewood Lateral does not support the eligibility of the overall resource, which, in the absence of enough information, has been determined to be eligible to the NRHP.

5JF4508.1/5JF4509.1. We have revised these site forms from our previous submission. We have included 5JF4508.2 in the form for 5JF4508.1 and 5JF4509.2 in the form for 5JF4509.1.

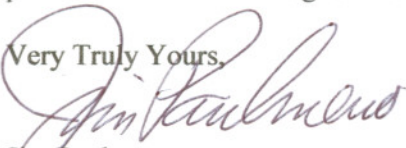
5JF4573 and 5JF4574: Your staff requested that these two properties be re-photographed from within the property in order to see the houses more clearly. However, due to restricted access, we changed our recommendation from not eligible to "Needs Data." Neither of these properties will be impacted by the Preferred Alternative. If plans should change, resulting in indirect or direct impacts, CDOT will request permission to access the properties to make eligibility determinations.

5JF4583. After conducting further research at the Jefferson County Assessor and Tax Records, we continue to recommend that this house is not a good example of a 1950s Contemporary style house due to the numerous alterations to its 5th Avenue façade, at the north side of the house. Numerous garage and carport additions to the sides and rear of the house have resulted in an odd secondary, side facade. The north facade where the original carport was later enclosed projects in a slight angle. It is covered by an extension of the main roof, but this is recessed slightly the trim does not project in the same manner as the rest of the roofline. An additional photograph showing this addition in detail, and a sketch plan from Jefferson County Tax Records that shows the alterations to the house have been added to the inventory form to provide further explanation of these changes, and the resulting diminished integrity to the house.

We hereby request your concurrence with these additional eligibility issues. Your response is necessary for the Federal Highway Administration's compliance with Section 106 of the National Historic Preservation Act, and the Advisory Council on Historic Preservation regulations.

We have also copied this request to the consulting parties that are participating in this consultation: the Lakewood Historical Society and the Jefferson County Historical Commission. We will forward any comments we receive from them.

Thank you in advance for your prompt attention to this matter. If you require additional information, please contact CDOT Region 6 Senior Historian Dianna Litvak at (303)757-9461.

Very Truly Yours,

Jim Paulmeno
Region 6 Planning and Environmental Manager

Enclosures: Revised Historic Resources Report
Site Forms

cc: without enclosures

Dianna Litvak, CDOT Region 6
David Singer, CDOT Region 6
Lisa Schoch, CDOT EPB
Marcee Allen, FHWA
Mandy Whorton, CH2MHill
Karen Zoltenko, Lakewood Historical Society
Dennis Dempsey, Jefferson County



October 21, 2008

Jim Paulmeno
Region 6 Planning and Environmental Manager
Colorado Department of Transportation, Region 6
2000 South Holly Street
Denver, CO 80222

Re: US 6 and Wadsworth Environmental Assessment, Lakewood, CO (CHS #50636)

Dear Mr. Paulmeno,

Thank you for your correspondence dated October 10, 2008 and received by our office on October 16, 2008 regarding the consultation of the above-mentioned project under Section 106 of the National Historic Preservation Act (Section 106).

After review of the submitted information, we concur with the changes made to the site forms for the resources listed below:

- 5JF.4511
- 5JF.4586
- 5JF.4512
- 5JF.4610
- 5JF.817.7
- 5JF.3013.1
- 5JF.4508.1 and 5JF.4508.2
- 5JF.4509.1 and 5JF.4509.2
- 5JF.4573
- 5JF.4574
- 5JF.4583

We request being involved in the consultation process with the local government, which as stipulated in 36 CFR 800.3 is required to be notified of the undertaking, and with other consulting parties. Additional information provided by the local government or consulting parties might cause our office to re-evaluate our eligibility and potential effect findings. Please note that our compliance letter does not end the 30-day review period provided to other consulting parties. If we may be of further assistance, please contact Amy Pallante, our Section 106 Compliance Manager, at (303) 866-4678.

Sincerely,

Edward C. Nichols
State Historic Preservation Officer

STATE OF COLORADO

DEPARTMENT OF TRANSPORTATION

Region 6, Planning and Environmental
2000 South Holly Street
Denver, CO 80222
(303) 757-9385
(303) 757-9036 FAX



December 9, 2008

Mr. Edward Nichols
State Historic Preservation Officer
Colorado Historical Society
1300 Broadway Avenue
Denver, CO 80203

SUBJECT: Determination of Effects – US 6 and Wadsworth Environmental Assessment, Lakewood, Colorado

Dear Mr. Nichols:

This letter and the attached Determination of Effects to Historic Properties Report constitute a request for concurrence on the Determination of Effects for the project referenced above. The report was prepared as part of the Environmental Assessment (EA) for the proposed reconstruction of the US 6 and Wadsworth Boulevard interchange and roadway widening along Wadsworth Boulevard between 4th and 14th Avenues in Lakewood, Colorado.

Project Description

The purpose of the US 6 and Wadsworth Boulevard project is to improve traffic flow and safety, accommodate high traffic volumes, and increase multi-modal travel options and connections at the US 6 and Wadsworth Boulevard interchange and along Wadsworth Boulevard between 4th Avenue and 14th Avenue.

The project area includes US 6 (also designated as 6th Avenue) and Wadsworth Boulevard (also designated as State Highway 121). The east-west limits along US 6 are from the eastern interchange ramps with Wadsworth Boulevard west to Garrison Street. On Wadsworth Boulevard, the project limits are 4th Avenue to 14th Avenue. Exhibit 1 in the attached report provides a map of the project area.

Description of the Recommended Alternative

The existing design and configuration of the interchange and roadway within the project limits have not kept pace with traffic and multi-modal travel demands. Improvements are needed to improve safety for motorists, pedestrians, and bicyclists; correct design deficiencies that contribute to safety concerns and operational inefficiencies; increase infrastructure capacity to meet current and future traffic volumes; and support multi-modal connections.

To address these needs, CDOT proposes to reconstruct the existing cloverleaf interchange and replace it with a tight diamond with a loop in the northwest quadrant. The loop provides the greatest capacity for the highest volume traffic movement: westbound to southbound in the evening rush hour. The new interchange would lengthen acceleration and deceleration lanes, improve intersection spacing, and remove weaving conflicts. To mitigate high traffic noise that exceeds FHWA's noise abatement criteria, existing noise walls on the east side of Wadsworth near the interchange would be reconstructed where needed, and new noise walls would be constructed between US 6 and the US 6 frontage road west of

Wadsworth to commercial developments at Garrison Street. The existing US 6 bridge over Wadsworth would be replaced, and its profile would be raised to accommodate a longer span across Wadsworth.

CDOT also proposes to widen Wadsworth Boulevard between 4th and 14th Avenues to provide an additional travel lane in each direction and detached sidewalks along both sides of the highway; the additional travel lanes and sidewalks would complement improvements north and south of the project area. In addition, access along Wadsworth would be controlled with a raised median.

The frontage roads on the north side US 6 within the project area would be reconfigured to address neighborhood access and traffic management. Accesses would be shifted, and portions of the existing one-way roads would be changed to two-way operations to improve access and reduce neighborhood cut-through traffic. Changes in frontage road operations were developed in consultation with residents of the adjacent neighborhoods.

Chapter 2 in the attached report provides additional detail about the Recommended Alternative and the alternatives evaluation process.

Eligibility Determinations

An Historic Resources Survey completed as part of the EA assessed eligibility for 151 historic resources within the Area of Potential Effects (APE) for the US 6 / Wadsworth project. Of the 151 properties surveyed, nine architectural resources are individually eligible for nomination to the National Register of Historic Places (NRHP) and 39 architectural resources are contributing elements to three NRHP-eligible historic districts within or partially within the APE. Your office concurred with these eligibility determinations in correspondence dated August 7, 2008 and October 21, 2008.

CDOT conducted additional analysis of the historic property at 1215 Wadsworth Blvd. (5JF4511). A revised Architectural Inventory Form is attached to this submittal. The form was revised to clearly delineate the contributing landscape features, which include the extant trees and the circular driveway, and the noncontributing landscape features, which include the Dry Gulch culvert at the southwest area of the property and the dirt walking path along Wadsworth Boulevard. These noncontributing elements were created after the property's period of significance. This revision allowed better evaluation of the impacts to the historic, contributing features of this property.

Effect Determinations

No Action Alternative

There would be no direct impacts to any historic properties with the No Action Alternative.

Proposed Action (Recommended Alternative)

CDOT has determined the Proposed Action will have an Adverse Effect to four individually NRHP eligible historic properties and no effect or no adverse effect to the remaining historic properties. The following tables summarize the effects to historic properties within the APE. The attached report provides additional discussion of effects and the efforts that CDOT undertook to avoid or minimize effects to historic properties.

TABLE 1
Effects to Individually Eligible Historic Resources in the APE

Site No.	Address	Proposed Action Impact	Effect Determination
5JF3548	7395 W. 6th Ave. Frontage Rd.	Demolition of structure (full acquisition)	Adverse Effect
5JF3549	7423 W. 6th Ave. Frontage Rd.	Demolition of structure (full acquisition)	Adverse Effect
5JF3554	7558 W. 9th Ave.	No direct or indirect impact (no	No Historic Properties Affected

TABLE 1
Effects to Individually Eligible Historic Resources in the APE

Site No.	Address	Proposed Action Impact	Effect Determination
		change to setting)	
5JF4511	1215 Wadsworth Blvd.	Partial acquisition of historic property frontage	No Adverse Effect
5JF4513	1230 Wadsworth Blvd.	Acquisition of portion of property that does not contribute to historic significance	No Adverse Effect
5JF4536	700 Wadsworth Blvd.	Demolition of structure (full acquisition)	Adverse Effect
5JF4542	7433 W. 6th Ave. Frontage Rd.	Demolition of structure (full acquisition)	Adverse Effect
5JF4563	8125 W. 6th Ave. Frontage Rd.	No direct or indirect impact (no adverse change to setting); beneficial noise reduction	No Adverse Effect
5JF4586	401 Wadsworth Blvd.	Roadway project ties into existing condition north of property boundary	No Adverse Effect

TABLE 2
Effects to Lakewood School Historic District and Contributing Buildings and Landscape Features, Site 5JF4512

Building	Proposed Action Impact	Effect Determination
Lakewood School Historic District	Acquisition of a portion of property adjacent to Wadsworth that does not contribute to historic significance; no buildings or contributing landscape features affected	No Adverse Effect
Building 1: New America School	Acquisition of portion of parking lot in front of building that does not contribute to historic significance	No Adverse Effect
Building 2: Elementary School	No Impact	No Historic Properties Affected
Building 3: Gymnasium	No Impact	No Historic Properties Affected
Building 5: Arts and Administration	No Impact	No Historic Properties Affected
Building 7: Junior High	No Impact	No Historic Properties Affected
Structure 10: World War II Memorial	No Impact	No Historic Properties Affected

Structure 12: Storage Outbuilding	No Impact	No Historic Properties Affected
Memorial Field	No Impact	No Historic Properties Affected
Landscaped areas adjacent to Buildings 1, 2, 3, 7	No Impact	No Historic Properties Affected

TABLE 3

Effects to Green Acres Historic District and Contributing Buildings within the APE

Site No.	Address	Proposed Action Impact	Effect Determination
	Green Acres Historic District	Construction near the southwestern edge of the historic district; most contributing resources unaffected; minor effects to three contributing resources (see 5JF4529, 5JF4530, and 5JF4537, below)	No Adverse Effect
5JF3610	700 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4526	7445 Highland Dr.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4527	7415 Highland Dr.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4528	7375 Highland Dr.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4529	705 Emerald Ln.	Indirect visual impact from construction of a sound barrier wall along rear/side of property	No Adverse Effect
5JF4530	715 Emerald Ln.	Indirect visual impact from construction of a sound barrier wall along rear of property	No Adverse Effect
5JF4537	7500 Broadview Dr.	Partial acquisition (approximately 180 square feet) required in southwest corner of property. Indirect visual impact from construction of a sound barrier wall along rear/side of property	No Adverse Effect
5JF4541	7370 Broadview Dr.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4543	740 Emerald Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4544	730 Emerald Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect

5JF4545	701 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4546	721 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4547	741 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4548	761 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4549	781 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4550	790 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4551	660 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4552	680 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4553	720 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4636	710 Emerald Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect

TABLE 4
Effects to Meadowlark Hills Historic District and Contributing Buildings within the APE

Site No.	Address	Proposed Action Impact	Effect Determination
Meadowlark Hills Historic District		Construction of a noise wall along frontage road north of the historic district	No Adverse Effect
5JF4590	545 Meadowlark Dr.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4591	540 Meadowlark Dr.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4592	555 Everett St.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4593	560 Everett St.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4594	585 Estes St.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4595	580 Estes St.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4596	585 Dudley St.	Beneficial indirect effect from reduced noise	No Adverse Effect

TABLE 4

Effects to Meadowlark Hills Historic District and Contributing Buildings within the APE

Site No.	Address	Proposed Action Impact	Effect Determination
5JF4597	580 Dudley St.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4598	585 Dover St.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4599	590 Dover St.	Beneficial indirect effect from reduced noise	No Adverse Effect

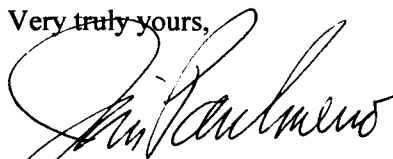
Request for Concurrence

We request your concurrence with the Determination of Effect outlined in the above tables. Your response is necessary for FHWA's compliance with Section 106 of the National Historic Preservation Act and the Advisory Council on Historic Preservation regulations.

This submittal has also been forwarded to the Jefferson County Historical Commission, Lakewood Historical Society, and Lakewood Heritage, Culture and the Arts Division. We will forward their responses once we receive them.

Thank you in advance for your prompt attention to this matter. If you need further information, please contact CDOT Region 6 Historian, Dianna Litvak, at (303) 757-9461.

Very truly yours,



Jim Paulmeno
Region 6 Planning and Environmental Manager

Enclosures: Determination of Effects to Historic Properties Report
5JF4511 (1215 Wadsworth) Architectural Inventory Form

cc: Dianna Litvak, Lisa Streisfeld, David Singer, Seyed Kalantar – CDOT Region 6
Marcee Allen – FHWA
Mandy Whorton – CH2M HILL
file

STATE OF COLORADO

DEPARTMENT OF TRANSPORTATION

Region 6, Planning and Environmental
2000 South Holly Street
Denver, CO 80222
(303) 757-9385
(303) 757-9036 FAX



December 9, 2008

Ms. Karen Zoltenko
Lakewood Historical Society
801 S. Yarrow Street
Lakewood, CO 80226

SUBJECT: Determination of Effects – US 6 and Wadsworth Environmental Assessment, Lakewood, Colorado

Dear Ms. Zoltenko:

This letter and the attached Determination of Effects to Historic Properties Report constitute the Determination of Effects under Section 106 of the National Historic Preservation Act for the project referenced above. The report was prepared as part of the Environmental Assessment (EA) for the proposed reconstruction of the US 6 and Wadsworth Boulevard interchange and roadway widening along Wadsworth Boulevard between 4th and 14th Avenues in Lakewood, Colorado.

Project Description

The purpose of the US 6 and Wadsworth Boulevard project is to improve traffic flow and safety, accommodate high traffic volumes, and increase multi-modal travel options and connections at the US 6 and Wadsworth Boulevard interchange and along Wadsworth Boulevard between 4th Avenue and 14th Avenue.

The project area includes US 6 (also designated as 6th Avenue) and Wadsworth Boulevard (also designated as State Highway 121). The east-west limits along US 6 are from the eastern interchange ramps with Wadsworth Boulevard west to Garrison Street. On Wadsworth Boulevard, the project limits are 4th Avenue to 14th Avenue. Exhibit 1 in the attached report provides a map of the project area.

Description of the Recommended Alternative

The existing design and configuration of the interchange and roadway within the project limits have not kept pace with traffic and multi-modal travel demands. Improvements are needed to improve safety for motorists, pedestrians, and bicyclists; correct design deficiencies that contribute to safety concerns and operational inefficiencies; increase infrastructure capacity to meet current and future traffic volumes; and support multi-modal connections.

To address these needs, CDOT proposes to reconstruct the existing cloverleaf interchange and replace it with a tight diamond with a loop in the northwest quadrant. The loop provides the greatest capacity for the highest volume traffic movement: westbound to southbound in the evening rush hour. The new interchange would lengthen acceleration and deceleration lanes, improve intersection spacing, and remove weaving conflicts. To mitigate high traffic noise that exceeds FHWA's noise abatement criteria, existing noise walls on the east side of Wadsworth near the interchange would be reconstructed where needed, and new noise walls would be constructed between US 6 and the US 6 frontage road west of Wadsworth to commercial developments at Garrison Street. The existing US 6 bridge over Wadsworth would be replaced, and its profile would be raised to accommodate a longer span across Wadsworth.

CDOT also proposes to widen Wadsworth Boulevard between 4th and 14th Avenues to provide an additional travel lane in each direction and detached sidewalks along both sides of the highway; the

additional travel lanes and sidewalks would complement improvements north and south of the project area. In addition, access along Wadsworth would be controlled with a raised median.

The frontage roads on the north side US 6 within the project area would be reconfigured to address neighborhood access and traffic management. Accesses would be shifted, and portions of the existing one-way roads would be changed to two-way operations to improve access and reduce neighborhood cut-through traffic. Changes in frontage road operations were developed in consultation with residents of the adjacent neighborhoods.

Chapter 2 in the attached report provides additional detail about the Recommended Alternative and the alternatives evaluation process.

Eligibility Determinations

As detailed in the Historic Resources Survey completed as part of the EA (sent to you for review in July 2008), CDOT surveyed 151 historic resources within the Area of Potential Effects (APE) for the US 6 / Wadsworth project. Of these, nine architectural resources have been determined individually eligible for nomination to the National Register of Historic Places (NRHP) and 39 architectural resources have been determined to be contributing elements to three NRHP-eligible historic districts within or partially within the APE.

Effect Determinations

No Action Alternative

There would be no direct impacts to any historic properties with the No Action Alternative.

Proposed Action (Recommended Alternative)

CDOT has determined the Proposed Action will have an Adverse Effect to four individually NRHP eligible historic properties and no effect or no adverse effect to the remaining historic properties. The following tables summarize the effects to historic properties within the APE. The attached report provides additional discussion of effects and the efforts that CDOT undertook to avoid or minimize effects to historic properties.

TABLE 1
 Effects to Individually Eligible Historic Resources in the APE

Site No.	Address	Proposed Action Impact	Effect Determination
5JF3548	7395 W. 6th Ave. Frontage Rd.	Demolition of structure (full acquisition)	Adverse Effect
5JF3549	7423 W. 6th Ave. Frontage Rd.	Demolition of structure (full acquisition)	Adverse Effect
5JF3554	7558 W. 9th Ave.	No direct or indirect impact (no change to setting)	No Historic Properties Affected
5JF4511	1215 Wadsworth Blvd.	Partial acquisition of historic property frontage	No Adverse Effect
5JF4513	1230 Wadsworth Blvd.	Acquisition of portion of property that does not contribute to historic significance	No Adverse Effect
5JF4536	700 Wadsworth Blvd.	Demolition of structure (full acquisition)	Adverse Effect
5JF4542	7433 W. 6th Ave. Frontage Rd.	Demolition of structure (full acquisition)	Adverse Effect
5JF4563	8125 W. 6th Ave. Frontage Rd.	No direct or indirect impact (no adverse change to setting); beneficial noise reduction	No Adverse Effect
5JF4586	401 Wadsworth Blvd.	Roadway project ties into existing condition north of property boundary	No Adverse Effect

TABLE 2
 Effects to Lakewood School Historic District and Contributing Buildings and Landscape Features, Site 5JF4512

Building	Proposed Action Impact	Effect Determination
Lakewood School Historic District	Acquisition of a portion of property adjacent to Wadsworth that does not contribute to historic significance; no buildings or contributing landscape features affected	No Adverse Effect
Building 1: New America School	Acquisition of portion of parking lot in front of building that does not contribute to historic significance	No Adverse Effect
Building 2: Elementary School	No Impact	No Historic Properties Affected
Building 3: Gymnasium	No Impact	No Historic Properties Affected
Building 5: Arts and Administration	No Impact	No Historic Properties Affected
Building 7: Junior High	No Impact	No Historic Properties Affected
Structure 10: World War II Memorial	No Impact	No Historic Properties Affected
Structure 12: Storage Outbuilding	No Impact	No Historic Properties Affected
Memorial Field	No Impact	No Historic Properties Affected
Landscaped areas adjacent to Buildings 1, 2, 3, 7	No Impact	No Historic Properties Affected

TABLE 3
 Effects to Green Acres Historic District and Contributing Buildings within the APE

Site No.	Address	Proposed Action Impact	Effect Determination
Green Acres Historic District		Construction near the southwestern edge of the historic district; most contributing resources unaffected; minor effects to three contributing resources (see 5JF4529, 5JF4530, and 5JF4537, below)	No Adverse Effect
5JF3610	700 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4526	7445 Highland Dr.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4527	7415 Highland Dr.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4528	7375 Highland Dr.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4529	705 Emerald Ln.	Indirect visual impact from construction of a sound barrier wall along rear/side of property	No Adverse Effect
5JF4530	715 Emerald Ln.	Indirect visual impact from construction of a sound barrier wall along rear of property	No Adverse Effect
5JF4537	7500 Broadview Dr.	Partial acquisition (approximately 180 square feet) required in southwest corner of property. Indirect visual impact from construction of a sound barrier wall along rear/side of property	No Adverse Effect
5JF4541	7370 Broadview Dr.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4543	740 Emerald Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect

TABLE 3
 Effects to Green Acres Historic District and Contributing Buildings within the APE

5JF4544	730 Emerald Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4545	701 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4546	721 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4547	741 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4548	761 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4549	781 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4550	790 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4551	660 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4552	680 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4553	720 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4636	710 Emerald Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect

TABLE 4
 Effects to Meadowlark Hills Historic District and Contributing Buildings within the APE

Site No.	Address	Proposed Action Impact	Effect Determination
Meadowlark Hills Historic District		Construction of a noise wall along frontage road north of the historic district	No Adverse Effect
5JF4590	545 Meadowlark Dr.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4591	540 Meadowlark Dr.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4592	555 Everett St.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4593	560 Everett St.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4594	585 Estes St.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4595	580 Estes St.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4596	585 Dudley St.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4597	580 Dudley St.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4598	585 Dover St.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4599	590 Dover St.	Beneficial indirect effect from reduced noise	No Adverse Effect

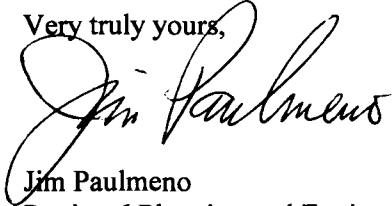
Request for Comments

As a consulting party with interest in these historic properties, we welcome your comments on the Determination of Effects outlined in the above tables. Should you elect to respond, we request that you do so within 30 days of receipt of this letter.

This submittal has also been forwarded to the State Historic Preservation Office, Jefferson County Planning and Zoning Department, and Lakewood Heritage, Culture and the Arts Division. We will forward their responses once we receive them.

Thank you in advance for your prompt attention to this matter. If you need further information, please contact CDOT Region 6 Historian, Dianna Litvak, at (303) 757-9461.

Very truly yours,



Jim Paulmeno
Region 6 Planning and Environmental Manager

Enclosure: Determination of Effects to Historic Properties Report

cc: Dianna Litvak, CDOT Region 6
Lisa Streisfeld, CDOT Region 6
David Singer, CDOT Region 6
Seyed Kalantar, CDOT Region 6
Marcee Allen, FHWA
Mandy Whorton, CH2M HILL
file

STATE OF COLORADO

DEPARTMENT OF TRANSPORTATION

Region 6, Planning and Environmental
2000 South Holly Street
Denver, CO 80222
(303) 757-9385
(303) 757-9036 FAX



December 9, 2008

Mr. Dennis Dempsey
Jefferson County Planning and Zoning Department
100 Jefferson County Parkway, Suite 3550
Golden, CO 80419-3550

SUBJECT: Determination of Effects – US 6 and Wadsworth Environmental Assessment, Lakewood, Colorado

Dear Mr. Dempsey:

This letter and the attached Determination of Effects to Historic Properties Report constitute the Determination of Effects under Section 106 of the National Historic Preservation Act for the project referenced above. The report was prepared as part of the Environmental Assessment (EA) for the proposed reconstruction of the US 6 and Wadsworth Boulevard interchange and roadway widening along Wadsworth Boulevard between 4th and 14th Avenues in Lakewood, Colorado.

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Effect Determinations

No Action Alternative

There would be no direct impacts to any historic properties with the No Action Alternative.

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 Effects to Lakewood School Historic District, Site 5JF4512

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5JF4537	7500 Broadview Dr.	Partial acquisition (approximately 180 square feet) required in southwest corner of property. Indirect visual impact from construction of a sound barrier wall along rear/side of property	No Adverse Effect
5JF4541	7370 Broadview Dr.	Beneficial indirect effect from reduced noise	No Adverse Effect

TABLE 3
 Effects to Green Acres Historic District and Contributing Buildings with in the APE

Site No.	Address	Proposed Action Impact	Effect Determination
5JF4543	740 Emerald Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4544	730 Emerald Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4545	701 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4546	721 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4547	741 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4548	761 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4549	781 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4550	790 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4551	660 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4552	680 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4553	720 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4636	710 Emerald Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect

TABLE 4
 Effects to Meadowlark Hills Historic District and Contributing Buildings with the APE

Site No.	Address	Proposed Action Impact	Effect Determination
Meadowlark Hills Historic District		Construction of a noise wall along frontage road north of the historic district	No Adverse Effect
5JF4590	545 Meadowlark Dr.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4591	540 Meadowlark Dr.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4592	555 Everett St.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4593	560 Everett St.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4594	585 Estes St.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4595	580 Estes St.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4596	585 Dudley St.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4597	580 Dudley St.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4598	585 Dover St.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4599	590 Dover St.	Beneficial indirect effect from reduced noise	No Adverse Effect

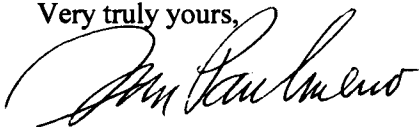
Request for Comments

As a consulting party with interest in these historic properties, we welcome your comments on the Determination of Effects outlined in the above tables. Should you elect to respond, we request that you do so within 30 days of receipt of this letter.

This submittal has also been forwarded to the State Historic Preservation Office, Lakewood Historical Society, and Lakewood Heritage, Culture and the Arts Division. We will forward their responses once we receive them.

Thank you in advance for your prompt attention to this matter. If you need further information, please contact CDOT Region 6 Historian, Dianna Litvak, at (303) 757-9461.

Very truly yours,



Jim Paulmeno
Region 6 Planning and Environmental Manager

Enclosure: Determination of Effects to Historic Properties Report

cc: Dianna Litvak, CDOT Region 6
Lisa Streisfeld, CDOT Region 6
David Singer, CDOT Region 6
Seyed Kalantar, CDOT Region 6
Marcee Allen, FHWA
Mandy Whorton, CH2M HILL
file

STATE OF COLORADO

DEPARTMENT OF TRANSPORTATION

Region 6, Planning and Environmental
2000 South Holly Street
Denver, CO 80222
(303) 757-9385
(303) 757-9036 FAX



Ms. Michelle Nierling
Lakewood Heritage, Culture and the Arts Manager
City of Lakewood
480 S Allison Pkwy
Lakewood, CO 80226

SUBJECT: Determination of Effects – US 6 and Wadsworth Environmental Assessment, Lakewood, Colorado

Dear Ms. Nierling:

This letter and the attached Determination of Effects to Historic Properties Report constitute the Determination of Effects under Section 106 of the National Historic Preservation Act for the project referenced above. The report was prepared as part of the Environmental Assessment (EA) for the proposed reconstruction of the US 6 and Wadsworth Boulevard interchange and roadway widening along Wadsworth Boulevard between 4th and 14th Avenues in Lakewood, Colorado.

Project Description

The purpose of the US 6 and Wadsworth Boulevard project is to improve traffic flow and safety, accommodate high traffic volumes, and increase multi-modal travel options and connections at the US 6 and Wadsworth Boulevard interchange and along Wadsworth Boulevard between 4th Avenue and 14th Avenue.

The project area includes US 6 (also designated as 6th Avenue) and Wadsworth Boulevard (also designated as State Highway 121). The east-west limits along US 6 are from the eastern interchange ramps with Wadsworth Boulevard west to Garrison Street. On Wadsworth Boulevard, the project limits are 4th Avenue to 14th Avenue. Exhibit 1 in the attached report provides a map of the project area.

Description of the Recommended Alternative

The existing design and configuration of the interchange and roadway within the project limits have not kept pace with traffic and multi-modal travel demands. Improvements are needed to improve safety for motorists, pedestrians, and bicyclists; correct design deficiencies that contribute to safety concerns and operational inefficiencies; increase infrastructure capacity to meet current and future traffic volumes; and support multi-modal connections.

To address these needs, CDOT proposes to reconstruct the existing cloverleaf interchange and replace it with a tight diamond with a loop in the northwest quadrant. The loop provides the greatest capacity for the highest volume traffic movement: westbound to southbound in the evening rush hour. The new interchange would lengthen acceleration and deceleration lanes, improve intersection spacing, and remove weaving conflicts. To mitigate high traffic noise that exceeds FHWA's noise abatement criteria, existing noise walls on the east side of Wadsworth near the interchange would be reconstructed where needed, and new noise walls would be constructed between US 6 and the US 6 frontage road west of Wadsworth to commercial developments at Garrison Street. The existing US 6 bridge over Wadsworth would be replaced, and its profile would be raised to accommodate a longer span across Wadsworth.

CDOT also proposes to widen Wadsworth Boulevard between 4th and 14th Avenues to provide an additional travel lane in each direction and detached sidewalks along both sides of the highway; the

additional travel lanes and sidewalks would complement improvements north and south of the project area. In addition, access along Wadsworth would be controlled with a raised median.

The frontage roads on the north side US 6 within the project area would be reconfigured to address neighborhood access and traffic management. Accesses would be shifted, and portions of the existing one-way roads would be changed to two-way operations to improve access and reduce neighborhood cut-through traffic. Changes in frontage road operations were developed in consultation with residents of the adjacent neighborhoods.

Chapter 2 in the attached report provides additional detail about the Recommended Alternative and the alternatives evaluation process.

Eligibility Determinations

As detailed in the Historic Resources Survey completed as part of the EA (sent to you for review in July 2008), CDOT surveyed 151 historic resources within the Area of Potential Effects (APE) for the US 6 / Wadsworth project. Of these, nine architectural resources have been determined individually eligible for nomination to the National Register of Historic Places (NRHP) and 39 architectural resources have been determined to be contributing elements to three NRHP-eligible historic districts within or partially within the APE. We understand that Lakewood has a list of properties that might already be listed or are being considered for a local landmark designation. Please provide us with the status of any of the properties that are in our report and your efforts to develop a local landmark list.

Effect Determinations

No Action Alternative

There would be no direct impacts to any historic properties with the No Action Alternative.

Proposed Action (Recommended Alternative)

CDOT has determined the Proposed Action will have an Adverse Effect to four individually NRHP eligible historic properties and no effect or no adverse effect to the remaining historic properties. The following tables summarize the effects to historic properties within the APE. The attached report provides additional discussion of effects and the efforts that CDOT undertook to avoid or minimize effects to historic properties.

TABLE 1
 Effects to Individually Eligible Historic Resources in the APE

Site No.	Address	Proposed Action Impact	Effect Determination
5JF3548	7395 W. 6th Ave. Frontage Rd.	Demolition of structure (full acquisition)	Adverse Effect
5JF3549	7423 W. 6th Ave. Frontage Rd.	Demolition of structure (full acquisition)	Adverse Effect
5JF3554	7558 W. 9th Ave.	No direct or indirect impact (no change to setting)	No Historic Properties Affected
5JF4511	1215 Wadsworth Blvd.	Partial acquisition of historic property frontage	No Adverse Effect
5JF4513	1230 Wadsworth Blvd.	Acquisition of portion of property that does not contribute to historic significance	No Adverse Effect
5JF4536	700 Wadsworth Blvd.	Demolition of structure (full acquisition)	Adverse Effect
5JF4542	7433 W. 6th Ave. Frontage Rd.	Demolition of structure (full acquisition)	Adverse Effect
5JF4563	8125 W. 6th Ave. Frontage Rd.	No direct or indirect impact (no adverse change to setting); beneficial noise reduction	No Adverse Effect

TABLE 1
 Effects to Individually Eligible Historic Resources in the APE

Site No.	Address	Proposed Action Impact	Effect Determination
5JF4586	401 Wadsworth Blvd.	Roadway project ties into existing condition north of property boundary	No Adverse Effect

TABLE 2
 Effects to Lakewood School Historic District and Contributing Buildings and Landscape Features, Site 5JF4512

Building	Proposed Action Impact	Effect Determination
Lakewood School Historic District	Acquisition of a portion of property adjacent to Wadsworth that does not contribute to historic significance; no buildings or contributing landscape features affected	No Adverse Effect
Building 1: New America School	Acquisition of portion of parking lot in front of building that does not contribute to historic significance	No Adverse Effect
Building 2: Elementary School	No Impact	No Historic Properties Affected
Building 3: Gymnasium	No Impact	No Historic Properties Affected
Building 5: Arts and Administration	No Impact	No Historic Properties Affected
Building 7: Junior High	No Impact	No Historic Properties Affected
Structure 10: World War II Memorial	No Impact	No Historic Properties Affected
Structure 12: Storage Outbuilding	No Impact	No Historic Properties Affected
Memorial Field	No Impact	No Historic Properties Affected
Landscaped areas adjacent to Buildings 1, 2, 3, 7	No Impact	No Historic Properties Affected

TABLE 3
 Effects to Green Acres Historic District and Contributing Buildings within the APE

Site No.	Address	Proposed Action Impact	Effect Determination
Green Acres Historic District		Construction near the southwestern edge of the historic district; most contributing resources unaffected; minor effects to three contributing resources (see 5JF4529, 5JF4530, and 5JF4537, below)	No Adverse Effect
5JF3610	700 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4526	7445 Highland Dr.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4527	7415 Highland Dr.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4528	7375 Highland Dr.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4529	705 Emerald Ln.	Indirect visual impact from construction of a sound barrier wall along rear/side of property	No Adverse Effect
5JF4530	715 Emerald Ln.	Indirect visual impact from construction of a sound barrier wall along rear of property	No Adverse Effect

5JF4537	7500 Broadview Dr.	Partial acquisition (approximately 180 square feet) required in southwest corner of property. Indirect visual impact from construction of a sound barrier wall along rear/side of property	No Adverse Effect
5JF4541	7370 Broadview Dr.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4543	740 Emerald Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4544	730 Emerald Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4545	701 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4546	721 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4547	741 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4548	761 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4549	781 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4550	790 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4551	660 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4552	680 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4553	720 Park Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4636	710 Emerald Ln.	Beneficial indirect effect from reduced noise	No Adverse Effect

TABLE 4
 Effects to Meadowlark Hills Historic District and Contributing Buildings within the APE

Site No.	Address	Proposed Action Impact	Effect Determination
Meadowlark Hills Historic District		Construction of a noise wall along frontage road north of the historic district	No Adverse Effect
5JF4590	545 Meadowlark Dr.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4591	540 Meadowlark Dr.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4592	555 Everett St.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4593	560 Everett St.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4594	585 Estes St.	Beneficial indirect effect from reduced noise	No Adverse Effect
5JF4595	580 Estes St.	Beneficial indirect effect from reduced noise	No Adverse Effect
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5JF4598	585 Dover St.	Beneficial indirect effect from reduced noise	No Adverse Effect
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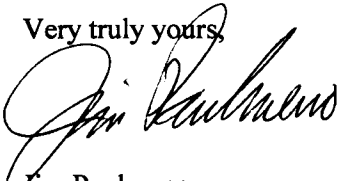
Request for Comments

As a consulting party with interest in these historic properties, we welcome your comments on the Determination of Effects outlined in the above tables. Should you elect to respond, we request that you do so within 30 days of receipt of this letter.

This submittal has also been forwarded to the State Historic Preservation Office, Jefferson County Planning and Zoning Department, and Lakewood Historical Society. We will forward their responses once we receive them.

Thank you in advance for your prompt attention to this matter. If you need further information, please contact CDOT Region 6 Historian, Dianna Litvak, at (303) 757-9461.

Very truly yours,



Jim Paulmeno
Region 6 Planning and Environmental Manager

Enclosure: Determination of Effects to Historic Properties Report

cc: Dianna Litvak, Lisa Streisfeld, David Singer, and Seyed Kalantar, CDOT Region 6
Marcee Allen, FHWA
Mandy Whorton, CH2M HILL
file

December 19, 2008

Jim Paulmeno
Region 6 Planning and Environmental Manager
Colorado Department of Transportation, Region 6
2000 South Holly Street
Denver, CO 80222

Re: US 6 and Wadsworth Environmental Assessment, Lakewood, CO (CHS #50636)

Dear Mr. Paulmeno:

Thank you for your correspondence dated December 9, 2008 and received by our office on that same date regarding the consultation of the above-mentioned project under Section 106 of the National Historic Preservation Act (Section 106).

After review of the assessment of adverse effect, we concur with the recommended finding of *no historic properties affected* [36 CFR 800.4(d)(1)] for the resources listed below.

- 5JF.3554

After review of the assessment of adverse effect, we concur with the recommended finding of *no adverse effect* [36 CFR 800.5(b)] for the resources listed below.

- 5JF.4512/Lakewood School Historic District
- 5JF.4685/Green Acres Historic District
- 5JF.4684/Meadowlark Hills Historic District
- 5JF.4513
- 5JF.4563
- 5JF.4586


After review of the assessment of adverse effect, we concur with the recommended finding of *adverse effect* [36 CFR 800.5(a)(1)] for the resources listed below.

- 5JF.3548
- 5JF.3549
- 5JF.4542
- 5JF.4536

Staff appreciates the detailed alternatives analysis provided for review in regards to avoidance and minimization measures to resolve the adverse effects. We agree that OAHF Level II documentation would be appropriate, but we recommend that FHWA and CDOT consult with all consulting parties to determine if any creative mitigation measures to resolve the adverse effects are feasible.

We request being involved in the consultation process with the local government, which as stipulated in 36 CFR 800.3 is required to be notified of the undertaking, and with other consulting parties. Additional information provided by the local government or consulting parties might cause our office to re-evaluate our eligibility and potential effect findings. Please note that our compliance letter does not end the 30-day review period provided to other consulting parties. If we may be of further assistance, please contact Amy Pallante, our Section 106 Compliance Manager, at (303) 866-4678.

Sincerely,



 Edward C. Nichols
State Historic Preservation Officer