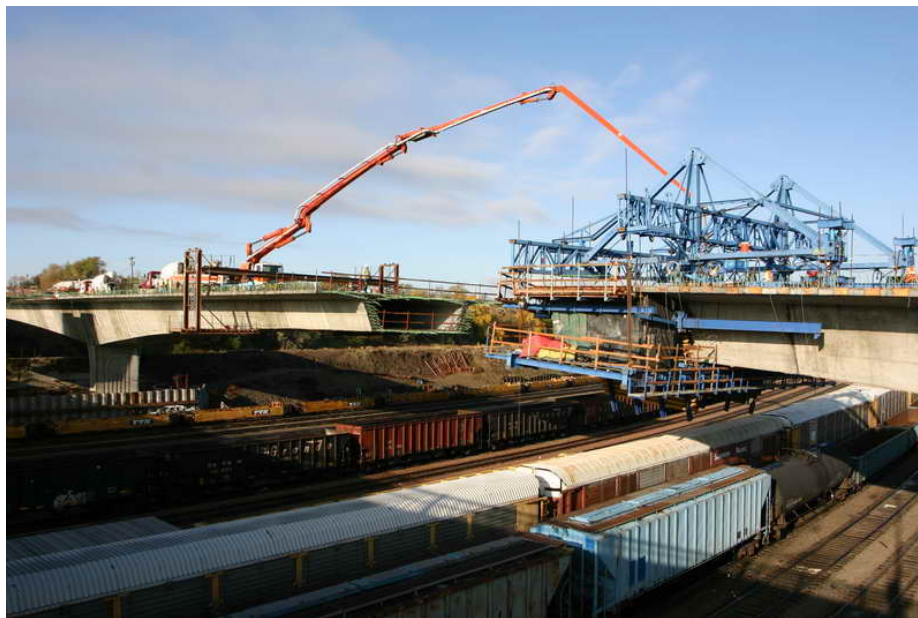


**Recent Construction Highlights**

Flatiron Constructors Intermountain completed segment construction at Cantilever 4 WB, with only the closure joints remaining. Also, pedestrian curb forming began in Span 1 WB and roadway approach work on each end is complete for traffic switch to the new bridge. The following is a summary of the construction progress for the last month.



**Figure 1 – Cantilever 4 WB Segmental Construction – October 22, 2009:**  
A 52-m pump truck is staged on Cantilever 3 WB to facilitate concrete placement for Segment W4-7W.



**Figure 2 – Cantilever 4 WB Segmental Construction – October 22, 2009:**  
The concrete pump placed above eliminates any disruption to the Railroads, since multiple track closures and crossings would be required for the pump, if staged below.



**Figure 3 – Cantilever 4 WB Segmental Construction – October 22, 2009:**  
A BNSF train passes under Cantilever 4 WB, as seen from below Span 5 WB. The form traveler is in position for Segment W4-7E.

**Figure 4 – Cantilever 4 WB Segmental Construction – October 27, 2009:**  
After casting Segment W4-7E, the web forms had to be cut prior to launching the traveler. (The web forms can be seen projecting well below the lower deck drive in Figure 3 above.) This was required to provide sufficient vertical clearance to the BNSF switch tracks.



**Figure 5 – Utility Work – October 28, 2009:**  
The conduits are installed in Span 1 WB in preparation for Qwest Utilities (Telecommunications). Qwest will install their cables after Span 3 WB closure pour is complete.



**Figure 6 – East End Roadway Approach Construction – November 11, 2009:**  
Paving begins at the entrance of the Midtown Shopping Center. The Contractor intends to complete paving operations before the winter season sets in.



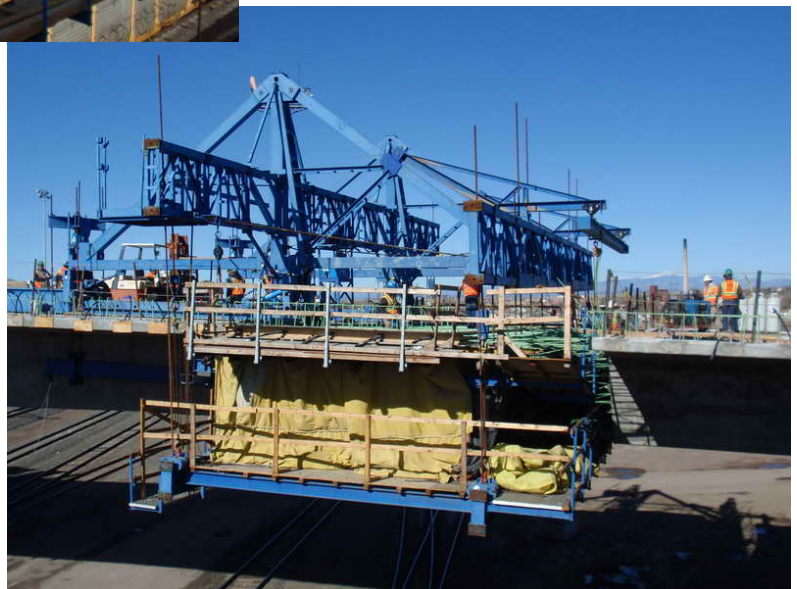
**Figure 7 – Cantilever 4 WB Segmental Construction – November 4, 2009:**  
Cantilever 4 WB (right) construction nears completion as it approaches the previously completed Cantilever 3 WB in the main-span. Pier Table 3 EB (left) will receive the form travelers when Cantilever 4 WB is complete. Roadway work occurs in the far background.

**Figure 8 – Bridge Finishing Work –  
November 4, 2009:**  
Crews work on repairs and pouring back  
form traveler tie-down holes from the  
finishing platform in Span 2 WB.



**Figure 9 – Cantilever 4 WB Segmental  
Construction – November 6, 2009:**  
The segment crew finishes the top slab for  
Segment W4-9W with the roller screed.

**Figure 10 – Cantilever 4 WB Segmental  
Construction – November 17, 2009:**  
Workers complete forming the last segment  
on Cantilever 4 WB. The total difference in  
deflection due to placing the wet weight of  
the concrete was over 5½”.







**Figure 11 – Span 1 WB Pedestrian Curb Construction – November 18, 2009:**  
Workers set up a 40' section of the pedestrian curb forms at Span 1 WB. Once Flatiron is comfortable with their formwork system, they plan to form and pour the curb in 120' sections.



**Figure 12 – West End Roadway Approach Construction – November 18, 2009:**  
Paving operations take place for the approach on the westbound structure. All four lanes will be switched to the westbound bridge in early 2010.



**Project Summary:**

November 24, 2009  
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<b>Substructure Construction</b>	<b><u>To</u> <u>Date</u></b>		<b><u>Total</u></b>	<b><u>Unit</u></b>	<b><u>% Complete</u></b>
48" Diameter Drilled Shafts (Monuments)	3	of	4	Each	75%
48" Diameter Drilled Shafts (Abutments)	11	of	14	Each	79%
60" Diameter Drilled Shafts (Pier 2 & 5)	6	of	8	Each	75%
96" Diameter Drilled Shafts (Pier 3 & 4)	8	of	8	Each	100%
Type I Footings (Pier 2 & 5)	3	of	4	Each	75%
Type II Footings (Pier 3 & 4)	4	of	4	Each	100%
3'-6" Piers (Pier 2 & 5)	3	of	4	Each	75%
7'-1" Piers (Pier 3 & 4)	4	of	4	Each	100%
Abutments	1 1/2	of	2	Each	75%

<b>Superstructure Construction</b>	<b><u>To</u> <u>Date</u></b>		<b><u>Total</u></b>	<b><u>Unit</u></b>	<b><u>% Complete</u></b>
<b>Westbound</b>					
End Span CIP Westbound	2	of	2	Each	100%
Abutment Diaphragm Westbound	2	of	2	Each	100%
Pier Diaphragm Westbound	2	of	2	Each	100%
Pier Table Westbound	2	of	2	Each	100%
Cantilever 3 Segments Westbound	22	of	22	Each	100%
Cantilever 4 Segments Westbound	20	of	20	Each	100%
Closure Segments Westbound	1	of	3	Each	33%
<b>Eastbound</b>					
End Span CIP Eastbound	1	of	2	Each	50%
Abutment Diaphragm Eastbound	1	of	2	Each	50%
Pier Diaphragm Eastbound	1	of	2	Each	50%
Pier Table Eastbound	2	of	2	Each	100%
Cantilever 3 Segments Eastbound	0	of	22	Each	0%
Cantilever 4 Segments Eastbound	0	of	20	Each	0%
Closure Segments Eastbound	0	of	3	Each	0%



**Project Summary:**

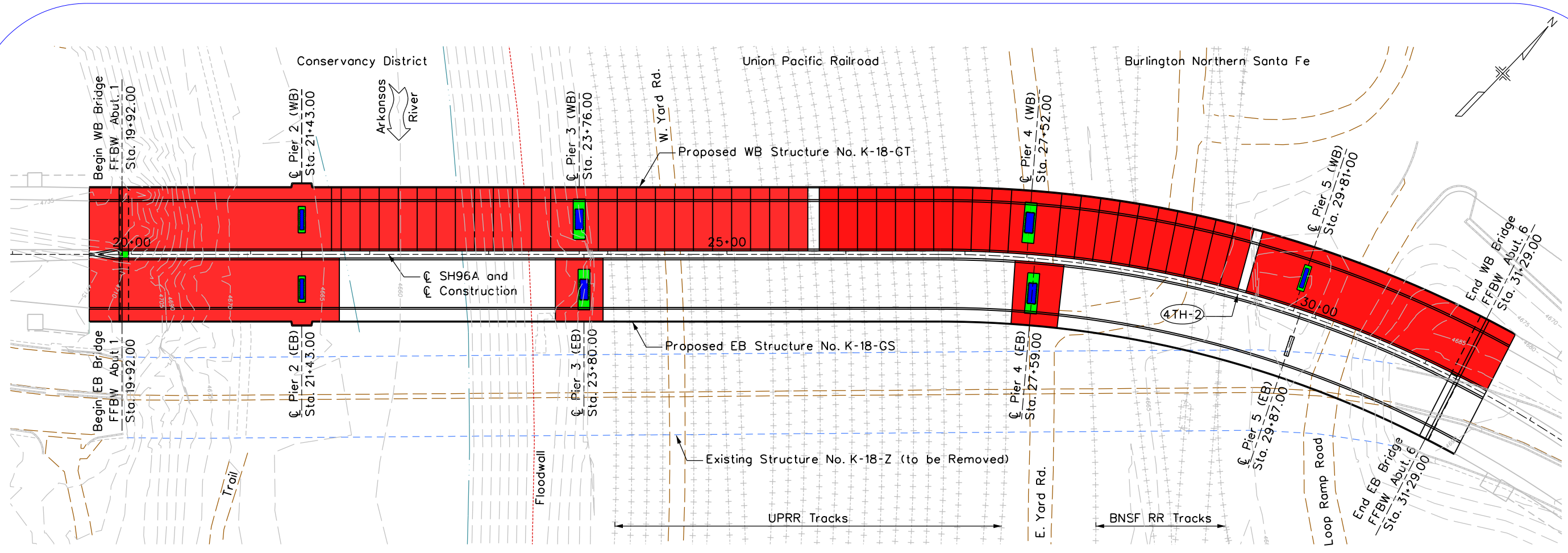
November 24, 2009  
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**Project Milestone Dates**

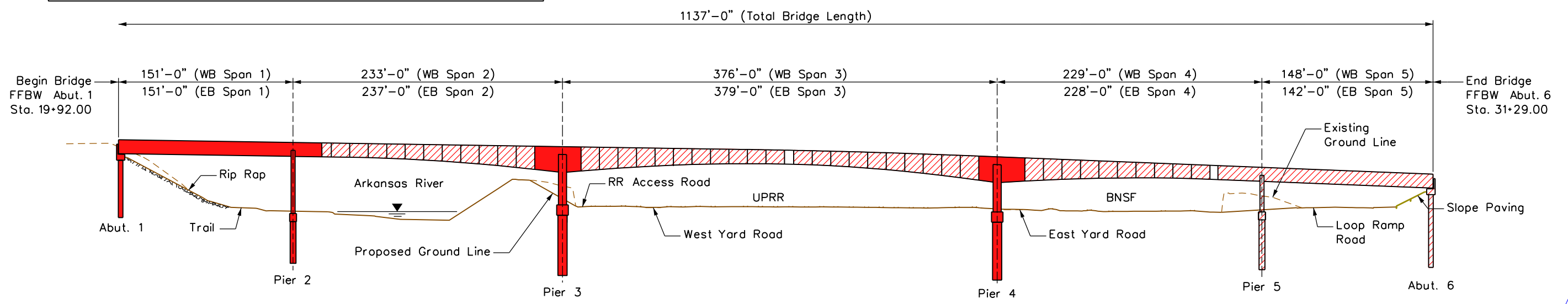
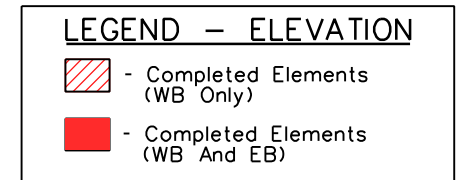
Milestone Event	April 2008 Baseline Finish Date	Actual
Project Award	October 18, 2007	October 18, 2007
Notice to Proceed	November 8, 2007	November 8, 2007
Form and Pour First Segment – W3-1E	November 19, 2008	February 16, 2009
Form and Pour First Closure – Span 2 WB	May 19, 2009	August 14, 2009
W4-10E Post Tension	October 20, 2009	November 19, 2009
Shift Traffic to New WB Structure	February 17, 2010	
Install Last Drilled Caissons – Abutment 6 (EB Only)	April 26, 2010	
Form and Pour Last Segment – E4-10E	October 12, 2010	
Form and Pour Last Closure – Span 3 EB	November 16, 2010	
Complete Structure and Final Traffic Configuration	March 4, 2011	

**All items are based on the April 2008 Baseline Schedule. All dates represent the “Finish” of the activity, unless otherwise noted. Refer to the October 2009 Project Updates for previous milestone dates.**

Cantilever construction has progressed at one pair of segments per week. Although casting of the first segment at Cantilever 3 WB was approximately 3 months behind schedule, it can be seen that Flatiron has made up time casting Cantilever 4 WB. Flatiron continues that project completion will occur within contractual requirements.



PLAN



ELEVATION

