

# TYPICAL SECTIONS

**Mobility enhancements:**

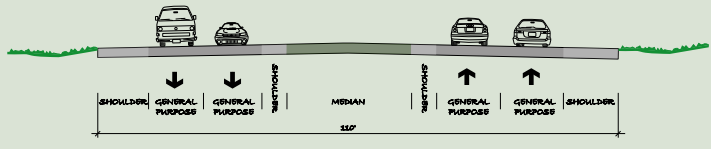
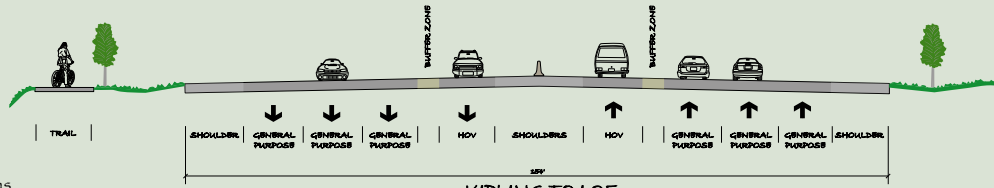
- Variable work hours
- Rideshare matching
- Teleworking
- Connective transit service
- Traffic Signals & ramp metering
- Incident management plans
- Advanced traveler information systems
- Telecommunications
- Traffic management
- Improved pedestrian/bicycle facilities
- Marketing and promotion
- TMO expansion/integration

**Bus:**

Express bus to run in HOV lanes

**Note:**

Widest desirable typical shown; dimensions may be reduced or refined at later dates.



**GENERAL PURPOSE LANES  
6 LANES PLUS HOV**

**Mobility enhancements:**

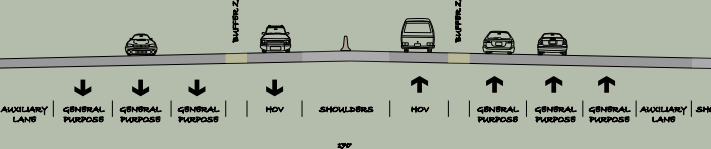
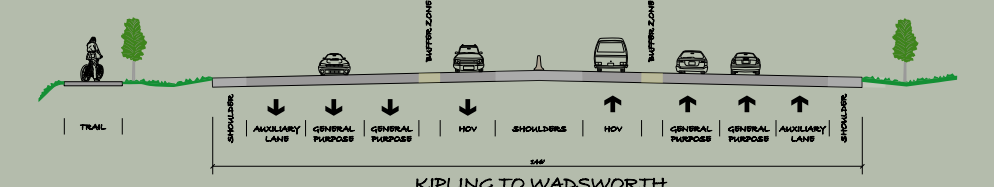
- Variable work hours
- Rideshare matching
- Teleworking
- Connective transit service
- Traffic Signals & ramp metering
- Incident management plans
- Advanced traveler information systems
- Telecommunications
- Traffic management
- Improved pedestrian/bicycle facilities
- Marketing and promotion
- TMO expansion/integration

**Bus:**

Express bus to run in HOV lanes

**Note:**

Widest desirable typical shown; dimensions may be reduced or refined at later dates.



**GENERAL PURPOSE LANES  
6 LANES PLUS HOV AND AUXILIARY**

**Mobility enhancements:**

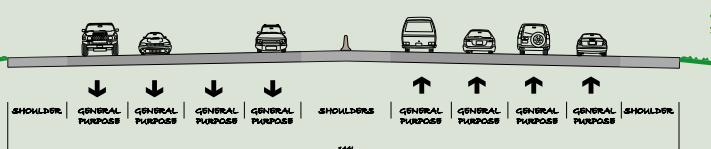
- Variable work hours
- Rideshare matching
- Teleworking
- Connective transit service (low potential)
- Traffic Signals & ramp metering
- Incident management plans
- Advanced traveler information systems
- Telecommunications
- Traffic management
- Improved pedestrian/bicycle facilities
- Marketing and promotion
- TMO expansion/integration

**Bus:**

Express bus has low potential to run in general purpose lanes.

**Note:**

Widest desirable typical shown; dimensions may be reduced or refined at later dates.



**GENERAL PURPOSE LANES  
8 LANES**

