



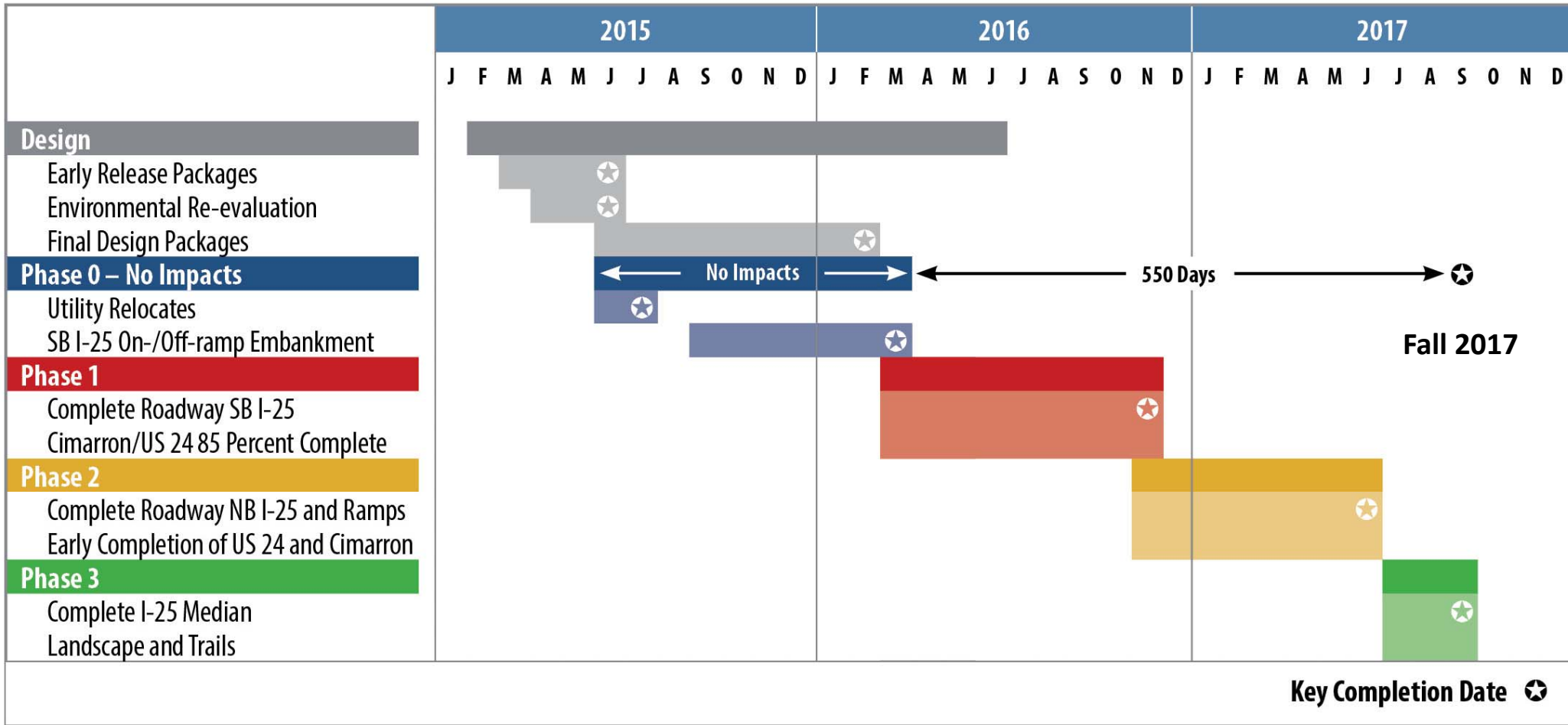
I-25/Cimarron Interchange

Project Overview

- Rebuilding the I-25 interchange between Colorado Avenue on the north and South Nevada Avenue on the south, 8th Street on the west and the Cimarron Street bridge over Fountain Creek on the east.
- Project will improve operations, correct existing safety and design deficiencies, and serve the anticipated short- and long-term travel demands in this area.
- When complete, the interchange will handle more vehicles with enhanced multi-modal travel for those using I-25, Cimarron Street and the trail system.
- The project will provide:
 - Improved interstate and interchange operations and safety
 - Improved highway alignment, ramps, acceleration/deceleration lanes
 - New I-25 and US 24 bridge structures
 - Improved trail connectivity and aesthetic treatments
 - New connection between 8th Street and Cimarron Street
- Trail, stream and offline work will begin summer 2015 with roadway construction scheduled to begin spring 2016.
- Operational completion is planned by fall 2017.



I-25/Cimarron Interchange

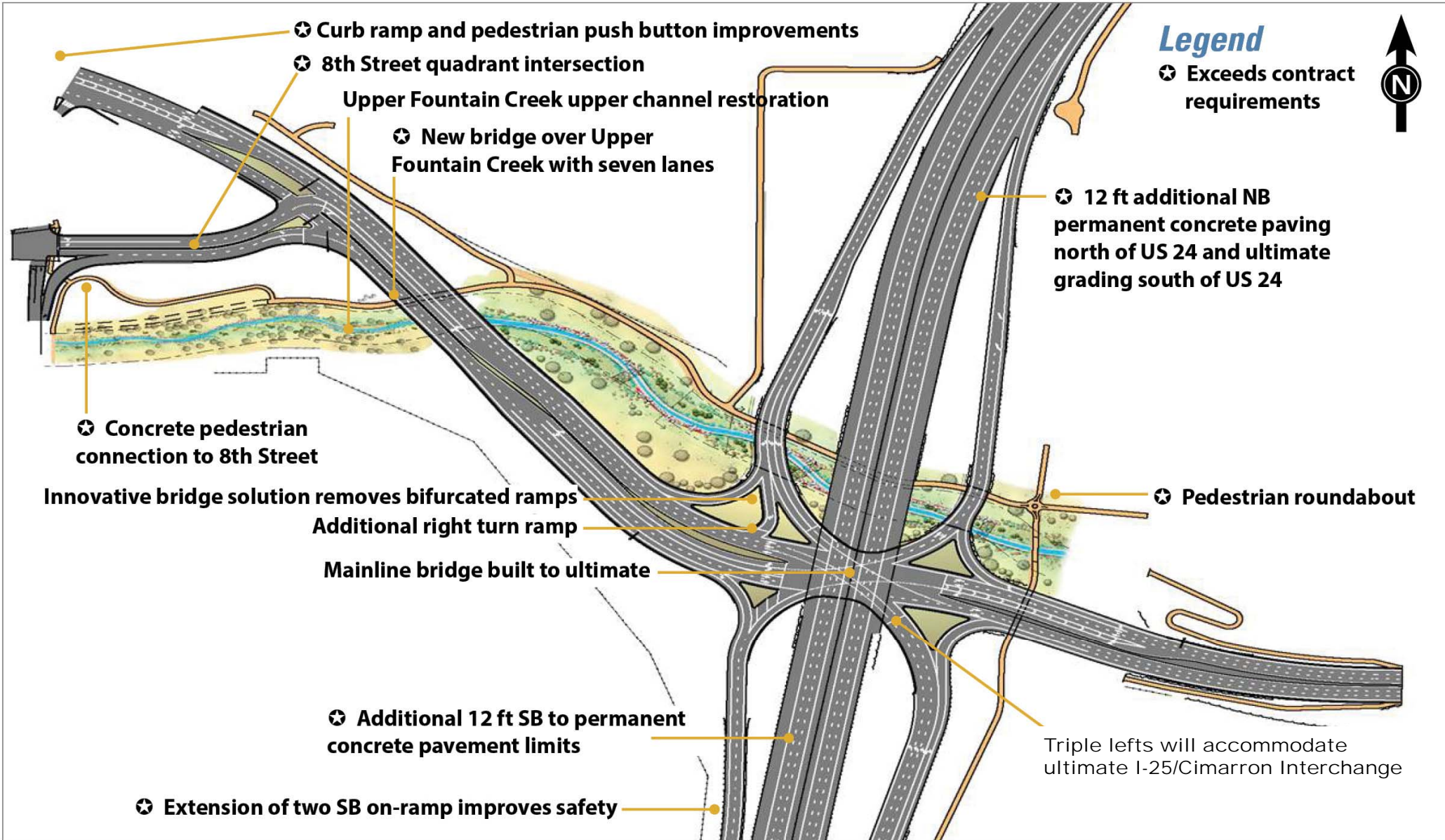


Project Schedule

Project Enhancements



I-25/Cimarron Interchange





I-25/Cimarron Interchange

Working Team Stakeholders

- Dave Watt - CDOT Resident Engineer
- Lesley Mace - CDOT Project Manager
- Susan Edmondson - Downtown Partnership, Mayor's Downtown Leadership Team
- Keith King - City of Colorado Springs City Council
- Bob Cope – City of Colorado Springs Economic Vitality
- Jim Rees - Southwest Downtown Urban Renewal Authority
- Gary Bradley - Business Stakeholder
- Larry Small - Fountain Creek Watershed, Flood Control & Greenway District
- David Lord - Greenway Fund
- Welling Clark - Organization of Westside Neighbors (OWN)
- Dave Van Ness - Old Colorado City Associates (OCCA)
- Les Gruen - Transportation Commissioner
- Craig Casper - Pikes Peak Area Council of Government



I-25/Cimarron Interchange

Aesthetics/Landscaping/Trails Technical Team

- Allen Beauchamp, Al Brody, Dave Van DerWege – Bicycle Community
- Tasha Brackin – El Paso County
- Victoria Chavez, Jennifer Irvine – El Paso County Traffic
- Stuart Coppedge – RTA Architects
- Brett Iverson – Colorado Springs Police Dept. Homeless Outreach Team
- Bill Koerner – Trails and Open Space Coalition
- Chris Lieber – City of Colorado Springs
- Connie Perry – City of Colorado Springs
- Dave Munger – Council of Neighbors and Organizations
- Elena Nunez – Colorado Springs Utilities
- Jim Rees – City of Colorado Springs Urban Renewal Authority
- Brian Risley – CRP Architects
- Nolan Schriener – Land Planner / Architect
- Jon Severson – Urban Single Track Project
- Larry Small – Fountain Creek Watershed Flood Control & Greenway District
- Kevin Shanks – THK



I-25/Cimarron Interchange

Executive Oversight Committee

- Karen Rowe – CDOT Region 2 Director
- Randy Jenson – Federal Highway Administration
- Steve Cox – City of Colorado Springs
- Travis Easton – City of Colorado Springs
- Sallie Clark – El Paso County
- Richard Zamora – CDOT Headquarters

Project Management Team

- Doug Lollar – CDOT Program Engineer
- Dave Watt – CDOT Resident Engineer
- Lesley Mace – CDOT Project Manager
- Nabil Haddad – CDOT Innovative Contracting
- Rob MacDonald – PPACG
- Dahir Egal, George Jones – FHWA
- Kathleen Krager – City of Colorado Springs
- Andre Brackin – El Paso County
- Mark Scholfield – Consultant PM
- Dave Poling – Corridor Manager
- Scott Asher – Design Development Manager
- Lisa Streisfeld – CDOT Environmental
- Robert Frei – CDOT Environmental
- Lisa Bachman – Public Involvement



I-25/Cimarron Interchange

Phase 0

June 2015 to March 2016

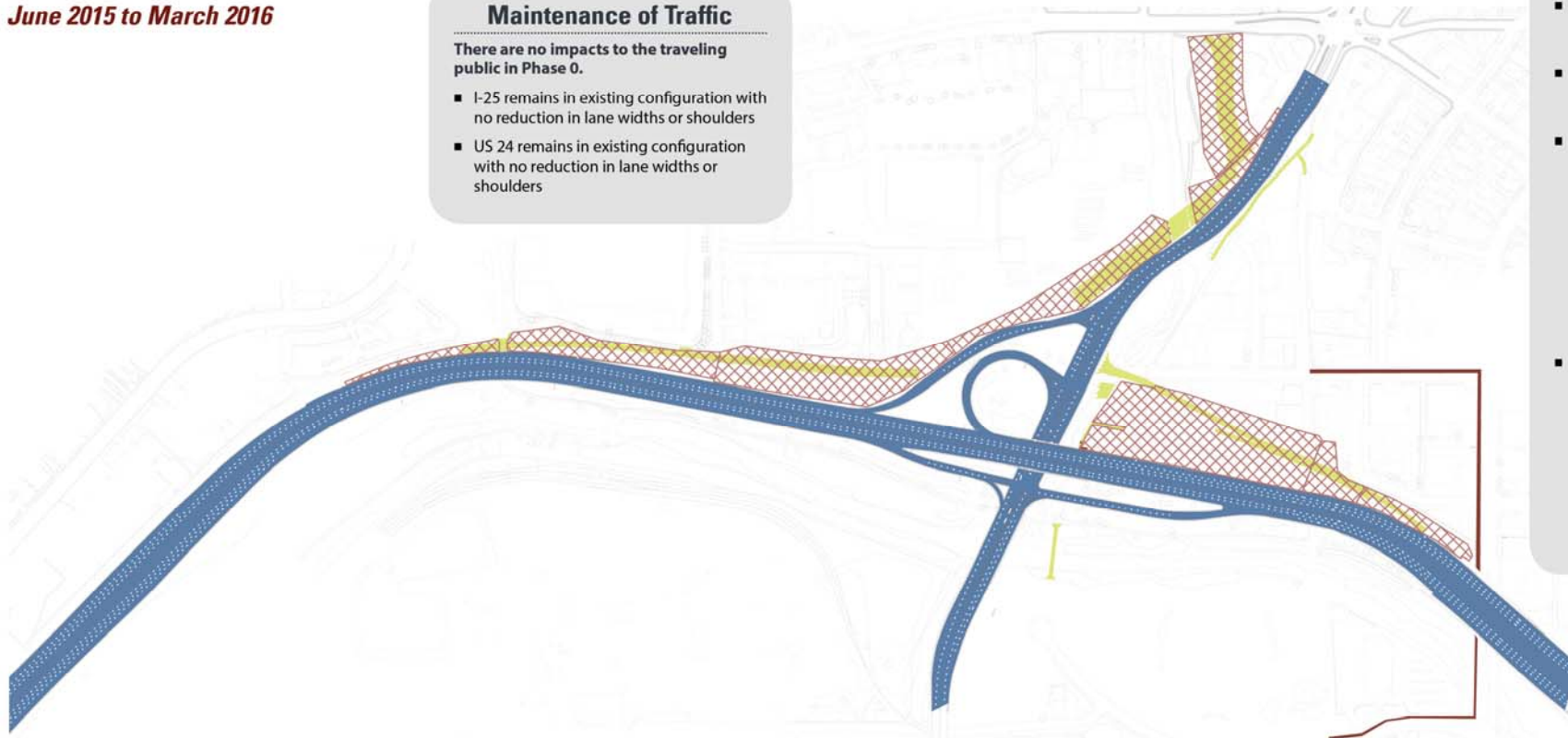
Maintenance of Traffic

There are no impacts to the traveling public in Phase 0.

- I-25 remains in existing configuration with no reduction in lane widths or shoulders
- US 24 remains in existing configuration with no reduction in lane widths or shoulders

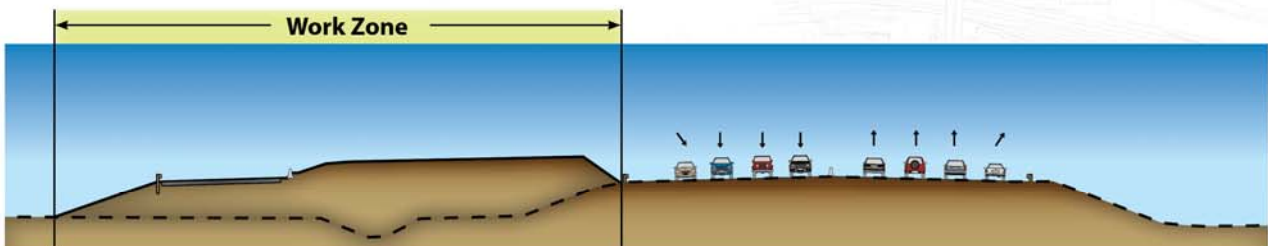
Key Construction Activities

- **Utilities** – construct 3000 feet of electric duct bank plus multiple water, gas, and fiber optic lines
- **Drainage** – install 60-inch storm sewer line, directionally jack and bore under I-25 with no traffic impacts
- **Roadway and Embankment**
 - SB I-25 off-ramp embankment, cut and fill
 - SB I-25 on-ramp embankment, fill
 - I-25 SB – off- and on-ramp; install walls and paving
 - ARE 4a – 8th Street quadrant intersection – embankment and paving
 - US 24 – build off line pavement
- **Bridges and Structures**
 - Temporary SB I-25 off-ramp over Upper Fountain Creek
 - EB US 24 bridge over Upper Fountain Creek
 - Pedestrian bridge over Monument Creek
 - Bear Creek Concrete Box Culvert extension



Legend

- Construction
- Location of Traffic
- Pedestrian/Bicycle Route
- Embankment



Phase 0 Typical Section of I-25 South of Upper Fountain Creek



I-25/Cimarron Interchange

Phase 1

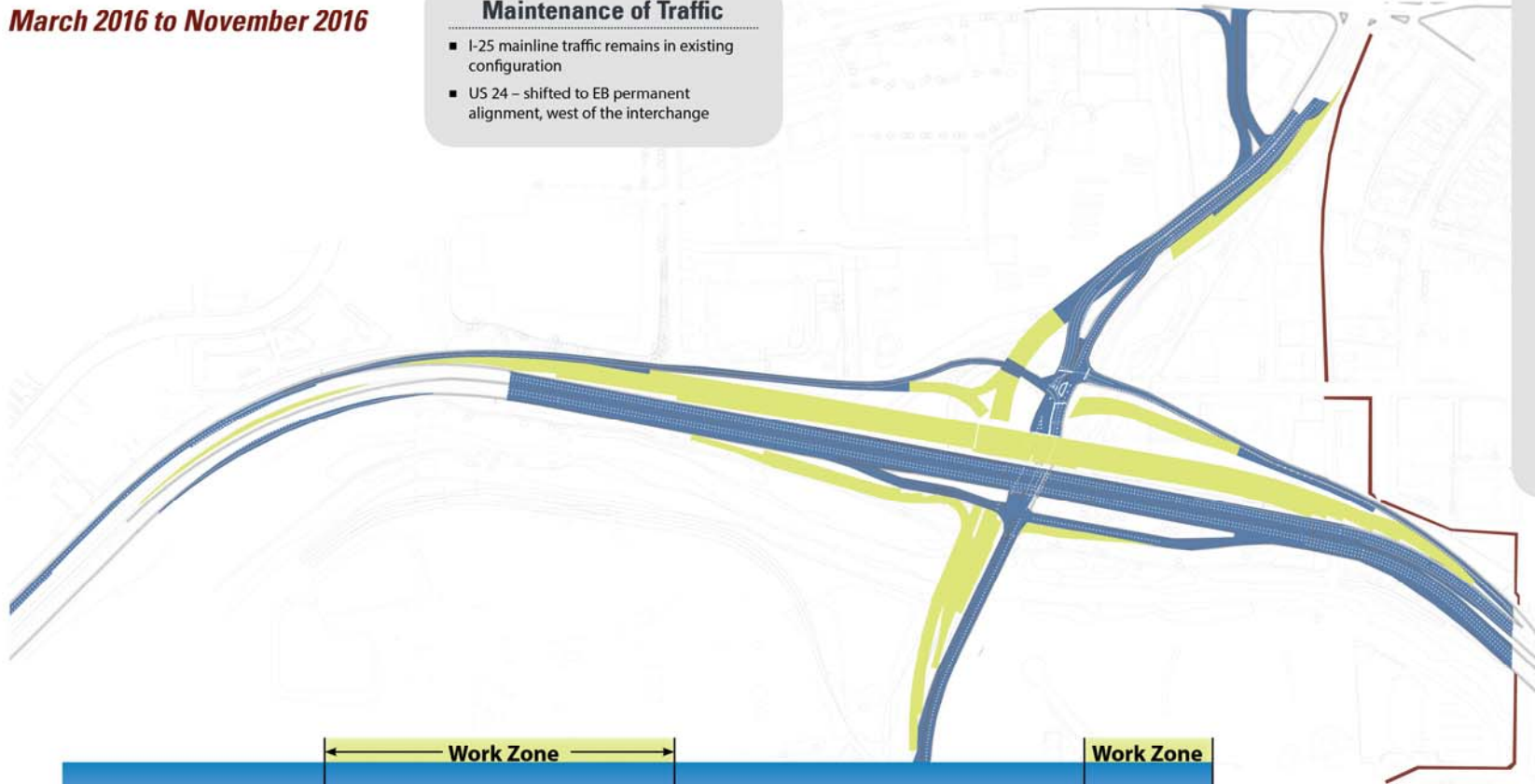
March 2016 to November 2016

Maintenance of Traffic

- I-25 mainline traffic remains in existing configuration
- US 24 – shifted to EB permanent alignment, west of the interchange

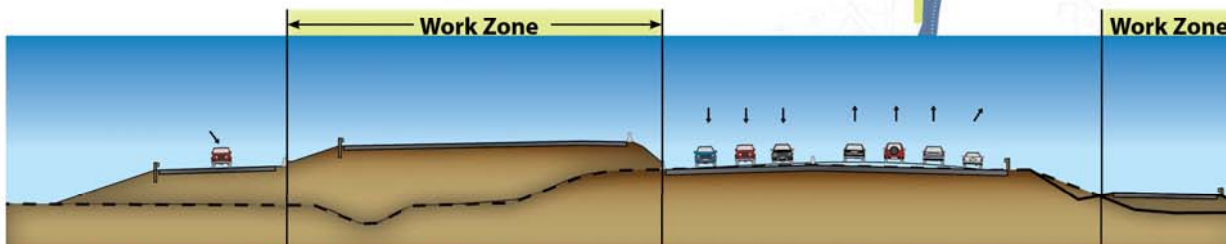
Key Construction Activities

- **Roadway and Embankment**
 - NB I-25 off-ramp fill south of US 24
 - Fill under new I-25 SB on-ramp
 - SB I-25 south end – fill under new alignment
 - Pave US 24 and Cimarron tie-ins
 - South end overlay as needed for tie-in (cross slope corrections)
 - Fastrack crossovers north/south end to shift NB onto new SB alignment
- **Bridges and Structures**
 - SB I-25 bridge
 - Cimarron over Lower Fountain Creek
 - US 24 over Upper Fountain Creek (remaining WB structure)
 - SB I-25 off-ramp over Upper Fountain Creek
 - Pedestrian bridge over Upper Fountain Creek
 - Temporary NB I-25 on-ramp bridge over Upper Fountain Creek
- **Trails – Fountain Creek path**



Legend

- Construction
- Location of Traffic
- Pedestrian/Bicycle Route



Phase 1 Typical Section of I-25 South of Upper Fountain Creek



I-25/Cimarron Interchange

Figure 2.4 – Phase 2

Phase 2

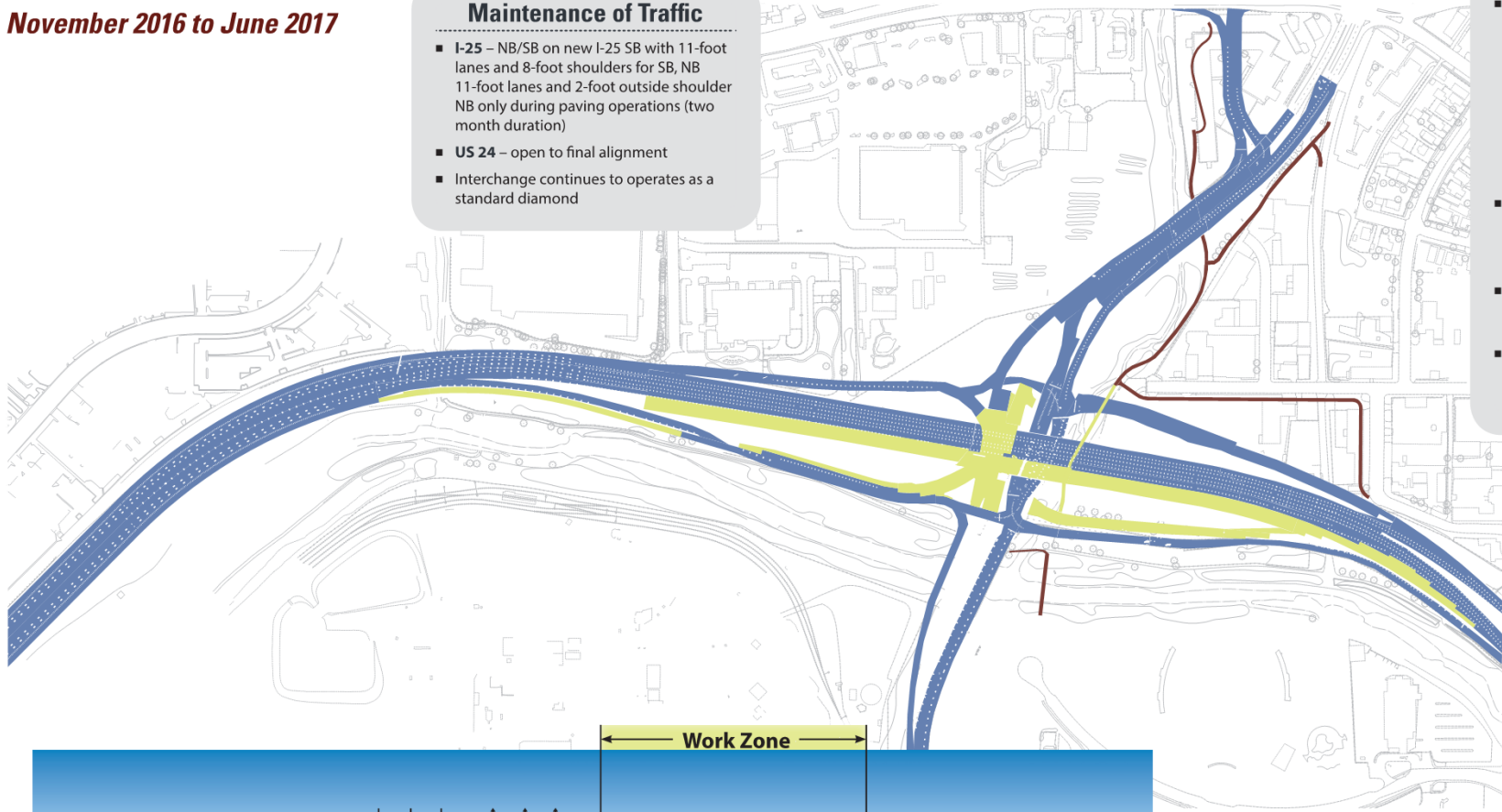
November 2016 to June 2017

Maintenance of Traffic

- I-25 – NB/SB on new I-25 SB with 11-foot lanes and 8-foot shoulders for SB, NB 11-foot lanes and 2-foot outside shoulder NB only during paving operations (two month duration)
- US 24 – open to final alignment
- Interchange continues to operate as a standard diamond

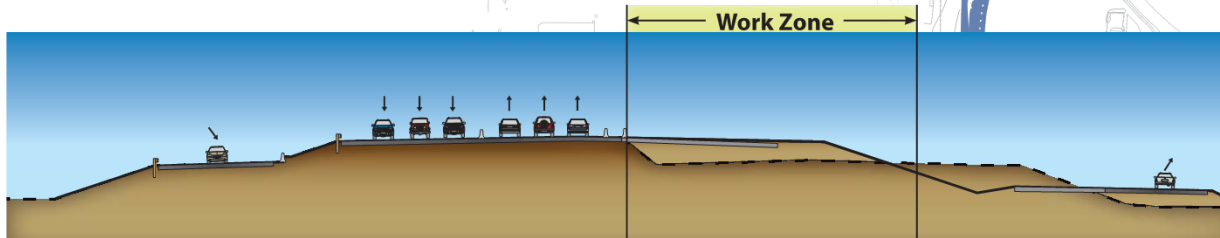
Key Construction Activities

- **Roadway and Embankment**
 - NB I-25 embankment/settlement
 - Pave NB I-25
 - NB I-25 On-ramp
 - NB I-25 off-ramp
 - US 24/ I-25 SB
 - Cimarron – install lighting, signals, and signs
 - ARE 2B
- **Bridges and Structures**
 - NB I-25 bridge
 - NB I-25 on-ramp bridge over Upper Fountain Creek
- **Trail and Stream** – ARE 3 stream channel reconstruction and multi-use trail extension
- **Aesthetics and Landscaping**
 - Landscaping starts
 - Final aesthetics



Legend

- Construction
- Location of Traffic
- Pedestrian/Bicycle Route



Phase 2 Typical Section of I-25 South of Upper Fountain Creek



I-25/Cimarron Interchange

Figure 2.5 – Phase 3

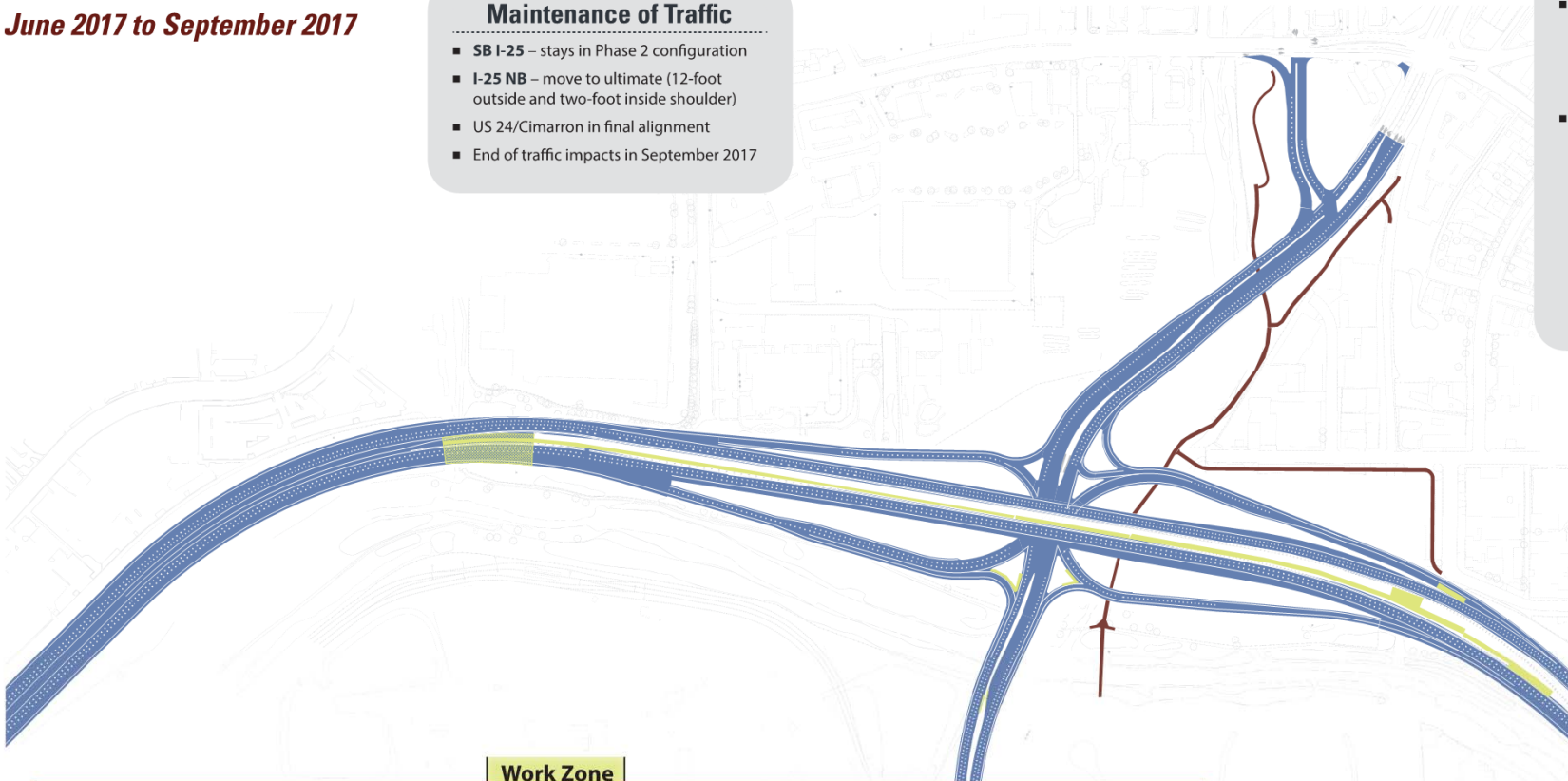
Phase 3 June 2017 to September 2017

Maintenance of Traffic

- SB I-25 – stays in Phase 2 configuration
- I-25 NB – move to ultimate (12-foot outside and two-foot inside shoulder)
- US 24/Cimarron in final alignment
- End of traffic impacts in September 2017

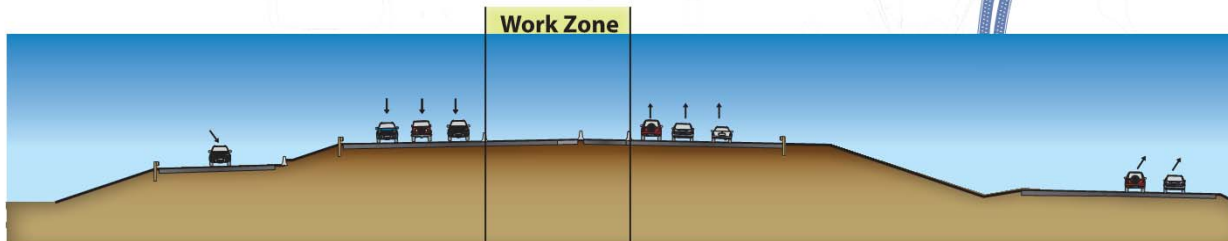
Key Construction Activities

- **Aesthetics and Landscaping**
 - Landscaping – finish selective prunings, ground cover
 - Finish aesthetics – concrete coatings and staining
- **Median Work/Final Completion**
 - Install median barrier, lighting, conduit, glare screen barrier
 - Final striping
 - Laterals/inlet tie-ins
 - Finish island/median curbs
 - Complete remaining trails and stream improvements; restoration at the confluence and under new NB on ramp bridge



Legend

-  Construction
-  Location of Traffic
-  Pedestrian/Bicycle Route



Phase 3 Typical Section of I-25 South of Upper Fountain Creek



I-25/Cimarron Interchange

I-25 Bridge over Cimarron

The I-25 bridge over Cimarron will be designed with monuments as detailed in Sheet 1.3, 1.5, 3.0, and 3.1 of the Aesthetic Plans and Details, Book 4.

We have included the following CDOT-approved aesthetic modifications to the I-25 bridge over Cimarron, which meet or exceed the aesthetic guidelines:

- Intermediate pylon refinements will continue to the bottom of the deck, and pier pylon refinements will continue to the bottom of the girder. The circle pattern will be adjusted to deck and girder heights as needed. (Figure 3.10 – Pylon Refinements)
- Bridge rail, monument accent band, and accent columns inset were created to receive ornamental steel cut rings that enhance the desired aesthetic with more shadowing. Space between rings and rail face are minimized to prevent bird nesting. (Figure 3.12 – Decorative Rail Section and Elevation)
- Post tensioned spliced precast girders with a cast-in-place deck provides a thin bridge profile minimizing heaviness of structure. (Figure 3.15 – I-25 Bridge Structure over Cimarron)

Figure 3.10 – Pylon Refinements

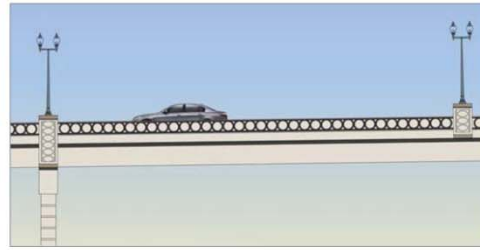
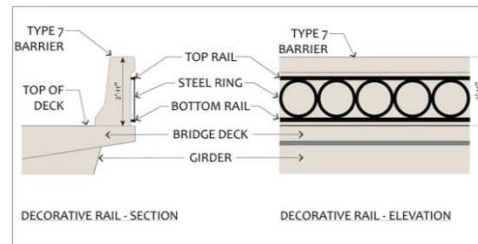


Figure 3.12 – Decorative Rail Section and Elevation



- Monument face skewed to follow geometry of abutment face and reduce sight distance problems. Circle pattern will be wrapped around monument with no partial circles. (Figure 3.11 – Monument Face).
- Vertical coping added to corners between wing walls and bridge.
- Custom formliner panels will match bridge panels at 5'x10' (Figure 3.13 – Custom Creekside Panel Dimensions)
- Random ashlar pattern will be placed on east facing wing walls past horizontal panels and fractured fin pattern will be placed on west facing wing walls past horizontal panels to the end of the wall at the gore.
- Wing walls will be reduced by maximizing 3:1 slopes from the abutment faces where possible for consistency.

Figure 3.13 – Custom Creekside Panel Dimensions

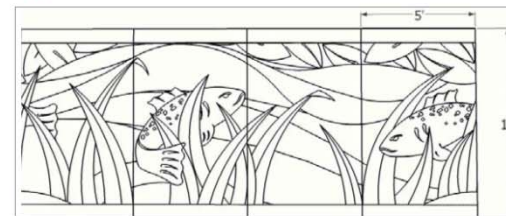
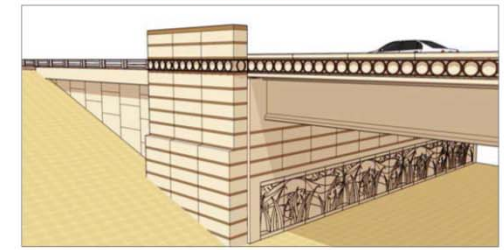


Figure 3.11 – Monument Face



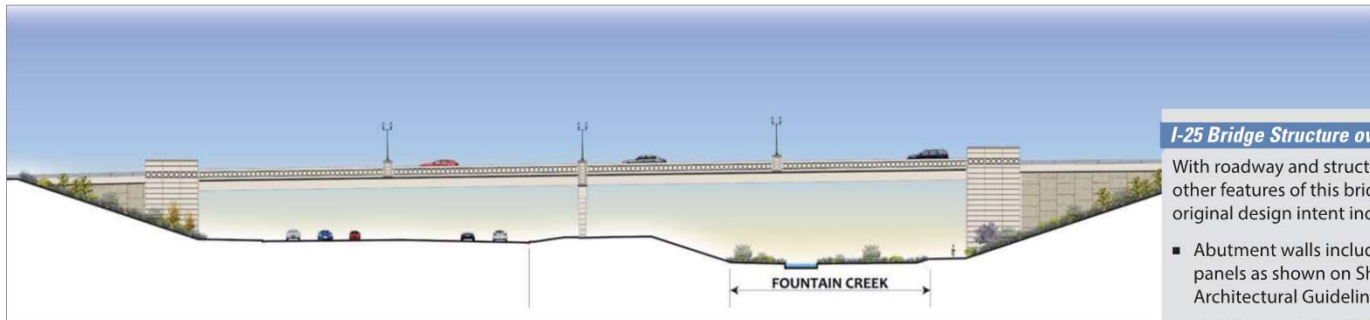
Value-Added Benefit

We upgraded the rings on the monuments to steel in lieu of formliner to provide a consistent aesthetic look along the bridge.

Figure 3.14 – Wing Walls



Figure 3.15 – I-25 Bridge Structure over Cimarron



I-25 Bridge Structure over Cimarron

With roadway and structure design refinements, other features of this bridge will follow the original design intent including:

- Abutment walls including stained horizontal panels as shown on Sheet 1.5 of the Architectural Guidelines
- Retaining walls and caps on the east side of I-25 will receive stain and an ashlar pattern

formliner, and retaining walls and caps on the west side of I-25 will match the COSMIX walls

- All girders will receive stain
- Each monument will receive LED uplighting
- Lighting under the bridges to wash the custom panels and trail
- Decorative rock mulches will be placed under the bridges – no slope paving added



I-25/Cimarron Interchange

Cimarron Bridge over Fountain Creek

The Cimarron Bridge over Fountain Creek is an important structure which is part of a larger future vision in this area. Its features will remain intact with the following minor modifications:

- 3:1 slopes will begin at the abutment face to minimize walls. Refinements made to monument heights to maintain appropriate proportions. (Figure 3.18 - Bridge Monument and Wing Wall)
- Formliner applied on pier accent column that supports ornamental light
- Intermediate pylons will extend to bottom of deck

- Custom creekside architectural panels placed on abutment faces along the trail with hand stained ashlar pattern formliner above up to bottom of structure. North/south faces of abutments will match hand-stained ashlar pattern formliner (Figure 3.19 - Cimarron Bridge Corner)

Decorative columns, lighting, piers and pier caps will follow the original intent of the aesthetic vision for this bridge following guidance on Sheets 1.4 and 3.2 of the Architectural Guidelines. Pier columns will include hand stained stone formliner at their base and above as shown in Figure 3.20 - Cimarron over Fountain Creek. Bridge rail will receive contrasting stain and fractured fin formliner. Pigmented stain will be applied to the girders for a finished look.

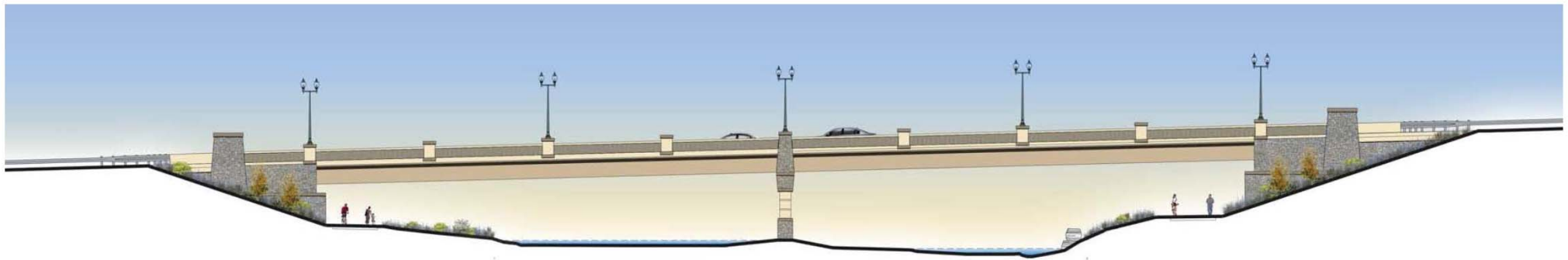
Figure 3.18 – Bridge Monument and Wing Wall



Figure 3.19 – Cimarron Bridge Corner



Figure 3.20 – Cimarron over Fountain Creek





I-25/Cimarron Interchange



Conceptual Rendering of Interchange



I-25/Cimarron Interchange

Stream Restoration

We have included stream restoration and trail enhancements to the project that improve the multi-modal user's experience and connectivity, and improve the riparian and aquatic environment of Upper Fountain Creek. We will reconstruct approximately 1,300 linear feet of Upper Fountain Creek west of the confluence to the US 24 bridge for the base configuration; and, another 800 linear feet up to 8th Street for ARE 3. Proposed channel improvements include lowering the channel invert and redefining the channel section to pass food flows under the new US 24 bridge. By incorporating ARE 3, we will maintain a stream gradient of 0.7 percent from the confluence to 8th Street.



Monument Creek river bedpro looking north



ARE 3 Meandering Stream Channel

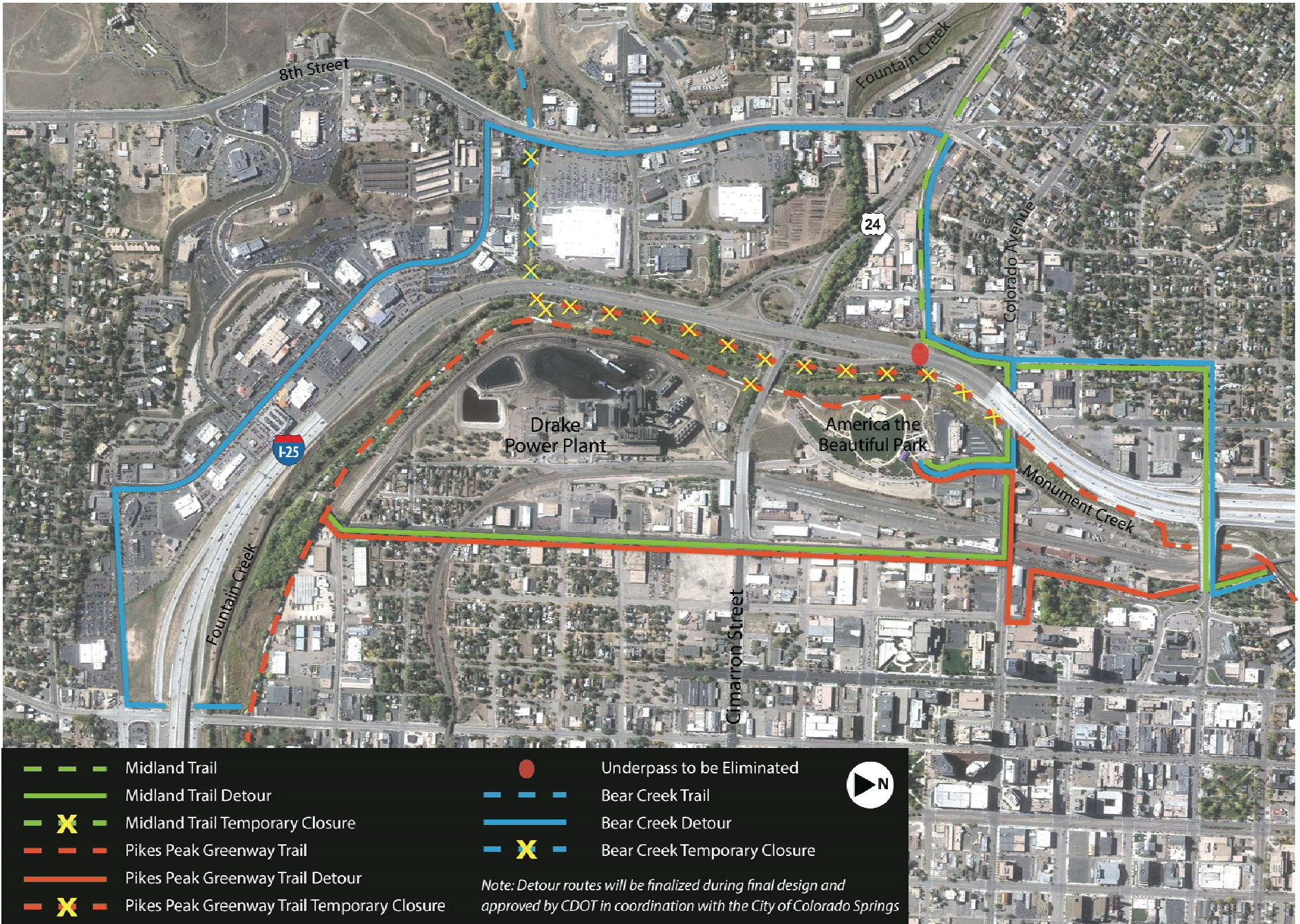
Table 3.16 – Stream Restoration Technical Challenges, Solutions, and Benefits

Technical Challenge	Our Approach/Solution	Benefit to the Project
Natural meandering alignment	<ul style="list-style-type: none"> Design grading based on natural sinuosity for both large peak flows and normal bankfull flows. 	<ul style="list-style-type: none"> Best channel alignment relative to I-25 ramps and mainline
Integration of highway bridges with the stream channel	<ul style="list-style-type: none"> Incorporate large-scale meandering design into the channel alignment to minimize the skew at waterway openings for the I-25 ramp bridges and mainline. 	<ul style="list-style-type: none"> Minimizes the potential for local scour at the bridge foundations. Maintains the aesthetic qualities between the bridge and channel
Sustainable stream channel	<ul style="list-style-type: none"> Use a two-year bankfull flow to design channel section, sinuosity and profile grade 	<ul style="list-style-type: none"> Naturally stable channel requires minimal maintenance
Resilient aquatic habitats	<ul style="list-style-type: none"> Incorporate linked bendway and riffle pools in a natural meandering pattern based on bankfull flow 	<ul style="list-style-type: none"> Restored ecosystem helping to link aquatic habitats in upper to lower Fountain Creek
Construction within a live stream	<ul style="list-style-type: none"> Pay careful attention to the sequence of construction and timing of work 	<ul style="list-style-type: none"> Minimal risk of damage to downstream stream reaches and conformance to NPDES and USACE 404 permit requirements

Proposed Trail Detours



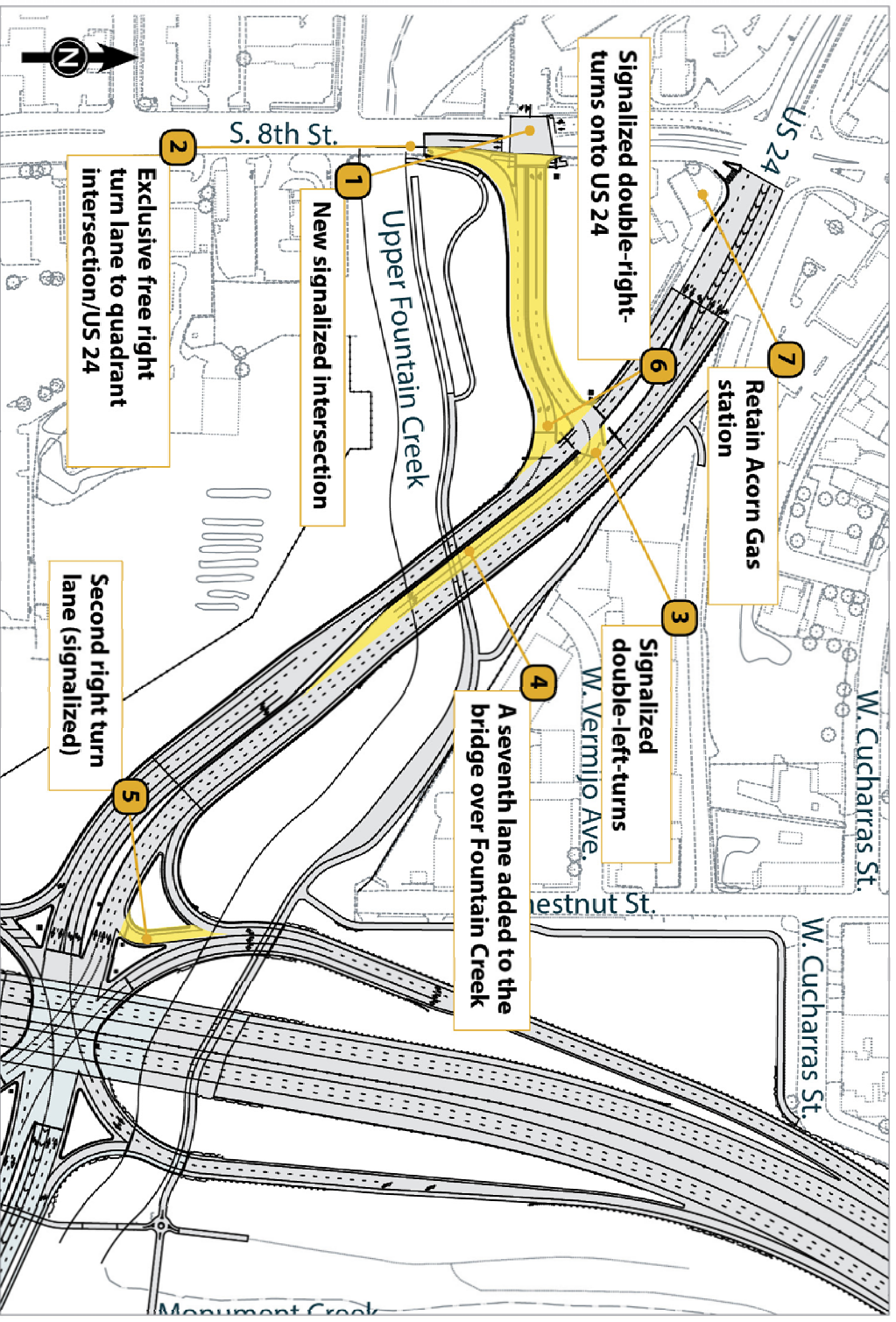
I-25/Cimarron Interchange



8th Street Connection to Cimarron Street



I-25/Cimarron Interchange



1. A new signalized intersection on 8th Street improves ingress/egress for Colorado Place Center retail area and serves the quadrant intersection.
2. Exclusive right turn lane from NB 8th to EB US 24 serves high right turn volume.
3. Signalized double lefts from WB US 24 to 8th Street (quadrant) allows more frequent green lights than lefts at existing signal, reducing delays and queuing. In addition, WB US 24 traffic always has a green light.
4. The seventh lane on the bridge over Upper Fountain Creek allows lane balance on US 24 and double left turns to the quadrant intersection.
5. A second SB I-25 to WB US 24 right turn lane will be signalized, reducing weaving traffic destined

for 8th Street from I-25—although not a project requirement, this is an important operational improvement.

6. Signalized double rights for NB 8th Street (quadrant) to EB US 24, which eliminates merging and weaving conflicts along US 24.
7. This design retains the Acorn Gas Station and improves access for drivers traveling WB on US 24.

Other distinguishing features include:

- Meets CDOT and City of Colorado Springs design criteria.
- A design that reduces congestion-related accidents.
- Accommodates traffic demand for nearly 20 years.



I-25/Cimarron Interchange

Ways To Stay Informed

- **Project Website**

www.codot.gov/projects/i25cimarronDB

- **Project Informational Hotline**

719-302-6781

- **Project Email**

Dot_i25cimarron@state.co.us

- ✓ Send us an email if you would like to sign up to receive the project e-newsletter and project notifications.

- **Comment Forms**

Fill out a comment form and leave it with a project representative.

- **Written Information Materials**

Pick up any of our printed handouts for more details on the project.