

Historic Resources
Inventory and
Determination of
Effects and
Historic Section
4(f) Report

I-270 Environmental Assessment

STU 2706-043 (Subaccount 23198)

Prepared for

Jacobs Engineering

and

**Colorado Department of
Transportation**

and

Federal Highway Administration

Prepared by

**Mead
& Hunt**

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Abstract

In August 2019 and July 2020 Mead & Hunt, Inc. (Mead & Hunt) undertook an intensive-level historic resources survey of 51 historic architectural and linear properties adjacent to Interstate Highway 270 (I-270) in Commerce City and unincorporated areas of Adams County for the I-270 Environmental Assessment (EA). Properties selected for the survey were built in 1976 or earlier. An Area of Potential Effects (APE) was delineated for the EA and includes all of the properties surveyed as part of this project as well as previously surveyed properties that are officially not eligible for the National Register of Historic Places (National Register) and did not require field survey.

The historic survey was split into two phases. The first phase consisted of a preliminary scoping phase and initial survey work to determine whether any properties had the potential to be determined eligible for listing in the National Register to better inform the schedule for the historic clearance during the EA. The second phase completed historic survey of the remaining properties within the APE as part of the EA. Field survey was conducted August 8-9, 2019, and July 1-2, July 17, and October 16, 2020. Mead & Hunt staff evaluated properties for significance and potential eligibility for listing in the National Register and recorded historic properties on Office of Archaeology and Historic Preservation (OAHP) inventory forms. In addition to the properties recorded by Mead & Hunt, one linear segment property (5AM.3924.3) and two architectural properties (5AM.4047, 5AM.4070) were recorded by Kristi Miniello in April 2020.

Of the eleven linear properties, five support the overall eligibility of their respective resources for listing in the National Register, four do not support the overall eligibility of their respective resources, and one is determined not eligible for listing in the National Register in its entirety (5AM.4121.1). One railroad junction is also included with the linear properties and is determined eligible for listing in the National Register.

Of the 40 non-linear properties, including 38 individual architectural properties, one historic subdivision and one bridge, 38 are determined not eligible for listing in the National Register, one is determined eligible (5701 Dexter Street, 5AM.4047), and one is determined as "Needs Data" (Suncor Energy, 5AM.4044). Suncor Energy possesses potential National Register significance under *Criterion A* for its association with the history of oil refineries and the development of the oil and gas industry in Colorado and is the only remaining operating refinery in the state of Colorado. However, access was not granted to the property and the integrity of the associated buildings, structures, and resources could not be assessed. One additional architectural property, located at 6680 York Street, is located in the APE and is over 50 years old but was not surveyed because it is buffered from all work by surrounding properties and no direct or indirect impacts are anticipated for this property.

A total of 51 properties were recorded in this survey.

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- A Project Plans
- B Area of Potential Effects Maps
- C Right-of-way Acquisition or Easement Map
- D OAHP Site Forms (submitted separately)

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-Not CDOT Approved-

1. Introduction

This report documents historic properties contained in the study area for the purposes of complying with Section 106 of the National Historic Preservation Act (Section 106), governed by 36 CFR 800, for the I-270 Environmental Assessment. The Colorado Department of Transportation (CDOT) and Federal Highway Administration (FHWA) are administering the project with the City of Commerce City and Adams County. The project would implement transportation solutions that improve the I-270 corridor to accommodate transportation demands. The 7-mile-long I-270 corridor serves as an inner beltway for the Denver region, providing a direct connection between I-25 and I-70. It is a corridor of commerce, where the region's key industry clusters of energy, advanced manufacturing, and logistics companies reside. An Area of Potential Effects (APE) has been defined that encompasses properties adjacent to the proposed improvements within the I-270 corridor in Commerce City and unincorporated areas of Adams County, Colorado. The APE is described below.

Consultant Work Product - Jacobs Engineering
-Not CDOT Approved-

2. Project Description

A. Project Setting

I-270 is a 6.5-mile-long controlled-access Interstate Highway with two through lanes in each direction, providing a direct connection from I-25 to I-70 between the northern and eastern Denver metro communities (see Figure 4). I-270 is a key link to the Denver International Airport and large business clusters from the energy, manufacturing, and freight distribution centers, and is a major truck corridor, providing access to adjacent industrial areas. Between I-25 and I-70, I-270 has partial interchanges at I-76, York Street, Vasquez Boulevard, and Quebec Street. The posted speed limit on the freeway is 55 mph. The highway crosses over both the UPRR and BNSF railroads, as well as the South Platte River, Clear Creek, and Burlington Ditch, and parallels Sand Creek.

B. Project description

The I-270 Corridor Improvements project would improve the I-270 corridor and address the safety, reliability, and freight movement needs through a combination of roadway infrastructure and technology improvements. Along the 6.5-mile corridor extending from the I-270/I-25/US-36 interchange to the I-270/I-70 interchange, the I-270 mainline would be reconstructed and widened in both directions to accommodate one additional travel lane, full width (8-foot or greater) shoulders, and a 4-foot buffer for a potential express lane operating option. Twelve-foot-wide auxiliary lanes may also be placed between interchanges to provide more merging distance for accelerating and decelerating traffic. The existing grassy median, which varies from 5 to 25 feet wide where present, would be graded and paved to accommodate the roadway widening. However, widening to the outside of the existing pavement edge would also be required in some areas, requiring minor amounts of right-of-way for construction and operation of the improved interstate. Most of the I-270 bridge structures would be replaced with new bridges to meet vertical clearance requirements. The structures not being replaced have been constructed more recently and are still within their expected service life.

The four interchanges within the corridor (not including I-270's interchange with I-25) would be improved through construction of new on- and off-ramps that would increase the acceleration and deceleration lengths, increase turning radius and superelevation (i.e., banking), and reduce the number of weave points between interstate traffic and local traffic accessing and entering I-270. Auxiliary lanes between the interchanges would further reduce weaving by separating interstate traffic from local traffic and providing more time for heavy trucks to accelerate to interstate speed before merging. The full cloverleaf interchange at I-270/Vasquez Boulevard would be replaced with a partial cloverleaf interchange design to improve safety and connectivity with the local roadway network.

The project would also improve multimodal travel and the local roadway network at York Street and 56th Avenue. Where it ties into the I-270 eastbound on-ramp, York Street would be widened to accommodate an expanded roadway template, including addition travel lanes and a multi-use trail, as identified in the Adams County York Street Phase III Project. Additionally, 56th Avenue would be improved via widening, curve flattening, and sidewalk extension.

To facilitate drainage of the widened interstate and protect the adjacent watercourses, the project would include permanent water quality features such as sediment vaults, drop inlets, outfalls, and water quality

ponds. Intelligent transportation system infrastructure would be installed to provide driver information and equip the roadway to leverage current and future technology, such as variable message signs that provide drivers with accident and roadway condition information. The express lane operating option would also include tolling-related technology and signage that is not required for the general purpose lane operating option. Conceptual project plans are provided in Appendix A.

(1) Mainline I-270

The Proposed Action roadway template consists of:

- 10- to-12-foot inside shoulders,
- A 12-foot travel lane,
 - This inside lane would be added to the entire length of I-270 from I-25 to I-70. CDOT is evaluating two options to operate this additional lane—either as an express or general purpose lane
- Two 12-foot general purpose lanes, and
- A 12-foot outside shoulder (see Figure 1).

Figure 1 and Figure 2 show the proposed typical sections with the express lane operational option. (Because any difference in the proposed roadway width between the operating options would be negligible, a consistent template width is assumed for both options).

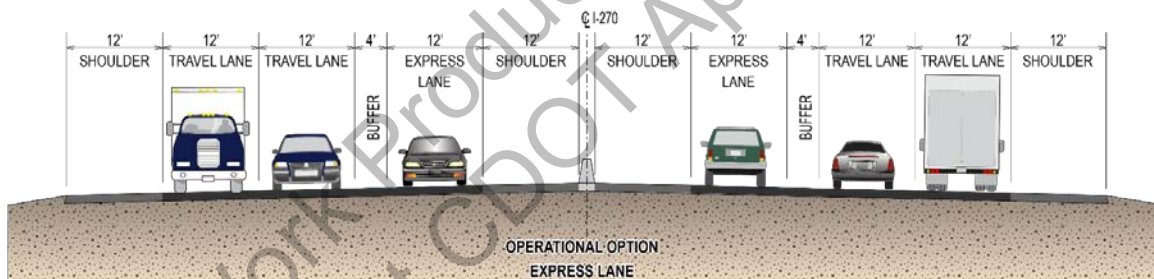


Figure 1. Typical roadway section without auxiliary lane.

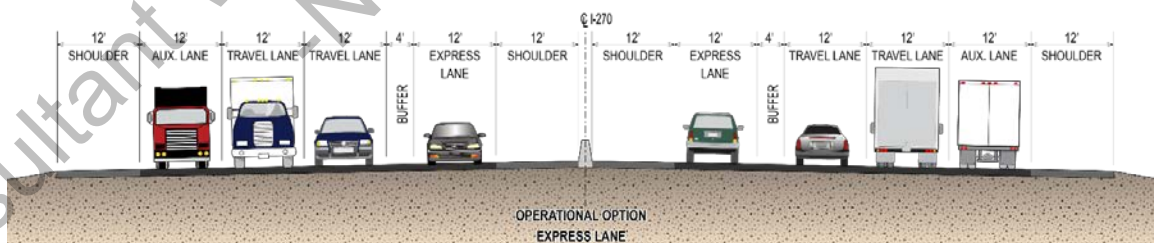


Figure 2. Typical roadway section with auxiliary lane.

Because some areas of I-270 were originally constructed on top of landfills whose settlement over time has created a “roller-coaster” effect to drivers and pavement cracking. The Proposed Action may include reconstruction of the full roadway prism as needed to ensure the reconstructed interstate is not impacted by the same settlement issues in the future.

(2) Staging area locations

Staging areas would be needed for materials, equipment, and access during construction of the Proposed Action. The following areas, none of which have historic structures that required survey, have been identified as staging areas:

- I-270/Vasquez Boulevard interchange (approximately 3.2 acres)
- North of I-270 just east of the Sand Creek Railroad junction (approximately 0.6 acres)
- North of I-270 just west of the Quebec Street Interchange and south of North Quebec Street (approximately 4.9 acres)
- I-270/I-76 interchange area (approximately 3.2 acres)

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I-270/York



I-270/Vasquez Blvd

<p>Legend:</p> <ul style="list-style-type: none"> — Mainline I-270 Widening — Collector Road — Auxiliary Lane Bridge Replacement Local Road/Multi-Modal Improvement 		
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Figure 3. Proposed Action.

C. Project purpose and need

The purpose of the I-270 Corridor Improvements project is to implement transportation solutions that improve the I-270 corridor to accommodate transportation demands. The project needs and goals are summarized below:

Needs:

- Improve safety by reducing the rate of vehicle crashes
- Improve travel time reliability and reduce delays
- Update obsolete and deficient bridges and highway infrastructure
- Improve truck freight movement efficiency

Goals:

- Accommodation of existing and planned multimodal routes
- Consideration of the natural and human environment
- Consideration of approved local and regional transportation plans

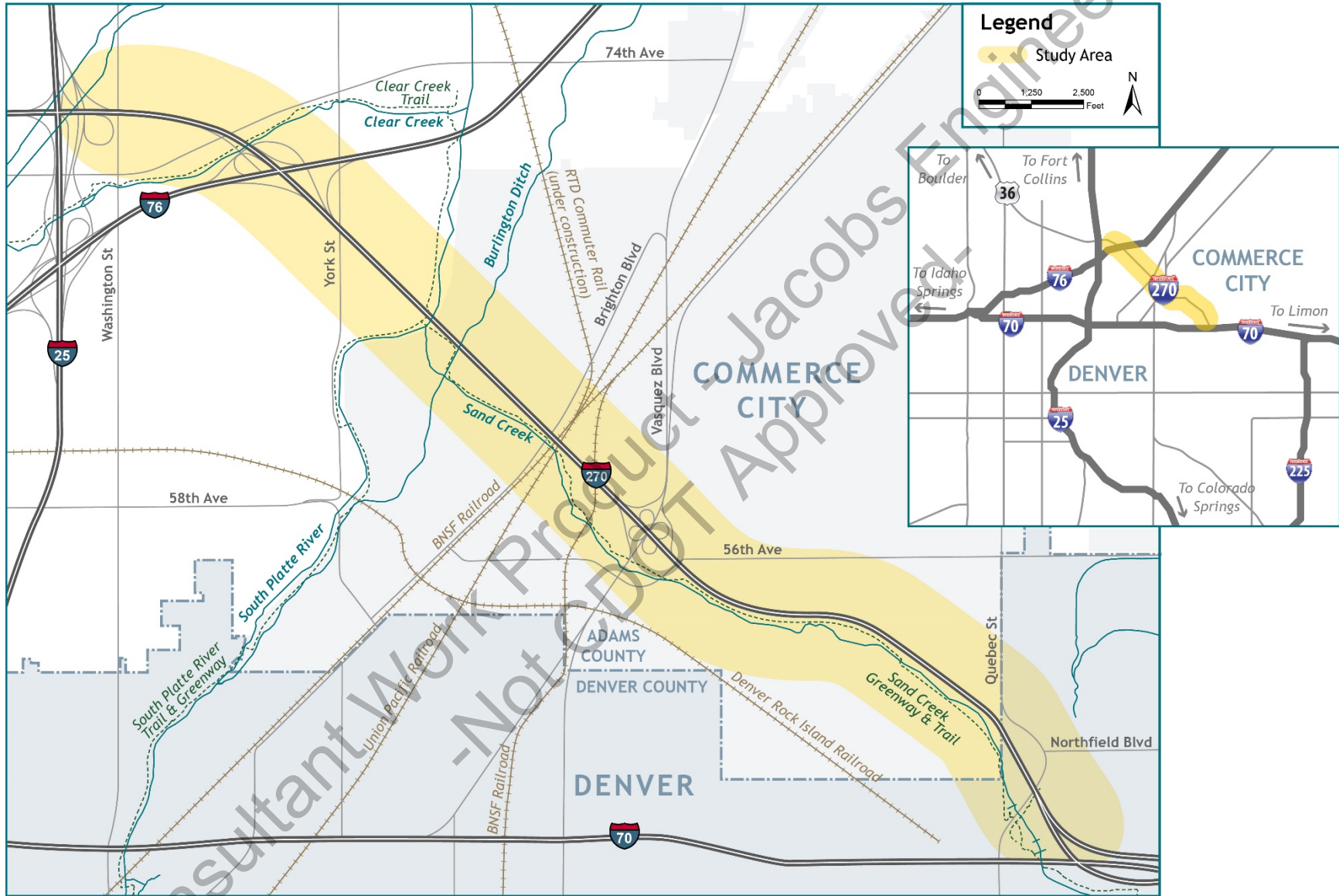


Figure 4. Project location map.

D. Area of Potential Effects

The APE is the geographic area within which an undertaking may directly or indirectly cause alterations in the character or use of historic properties, if any such properties exist. The APE includes the 6.5-mile corridor of I-270 between I-25 on the north and I-70 on the south. The APE was delineated to include age-eligible properties (built in 1976 or earlier) adjacent to the I-270 right-of-way that may be subject to direct and indirect effects as a result of the project activities. Maps showing the APE are provided in Appendix B.

The APE was developed to include the following elements:

- The footprint of the proposed design, including the associated right-of-way of I-270 encompassing 7 miles between the project start and end, and including interchanges where I-270 connects with I-76, I-25 on the north, and I-70 on the south.
- Locations where the project would result in changes to ramps between interchanges, including difference in heights, width, alignment, in comparison to current ramps.
- Locations where anticipated design is outside of or on the right-of-way line. The full parcel boundaries for properties with the potential to be subject to indirect or direct effects are included within the APE. The APE also includes vacant parcels and parcels with properties built in 1977 or later.
- Anticipated impacts to railroads within right-of-way (e.g., constructing new bridge piers within I-270 right-of-way).
- Anticipated impacts to irrigation ditches, including constructing new bridge piers within I-270 right-of-way and other changes or modifications to ditch structures and associated features.
- Locations of embankments, noise walls, retaining walls, barriers, water quality sites, and staging areas.
- Other locations adjacent to I-270 where direct or indirect impacts to historic properties are possible.

The APE was also limited on the southeast side of the study area to the I-270 right-of-way where Sand Creek parallels I-270 because no impacts are expected south of I-270 or to Sand Creek.

Sand Creek Drive and I-270 were not surveyed within the APE. Both properties are over 50 years of age, built in the late 1960s. Sand Creek Drive and I-270 are both subject to the *Section 106 Exemption Regarding Historic Preservation Review Process for Effects to the Interstate Highway System* adopted by the Advisory Council on Historic Preservation in 2005. Sand Creek Drive was built at the same time as I-270 and is within the I-270 right-of-way. As such, Sand Creek Drive and I-270 are exempt from the Section 106 review process and are not discussed further in this report.

Mead & Hunt historians reviewed the APE with Barbara Stocklin, CDOT Region 1 Senior Historian. The APE will be discussed further with CDOT and the State Historic Preservation Office (SHPO) and reviewed periodically based on project design milestones and alternatives developed as part of the EA to determine whether additional properties should be included within the APE.

A total of 71 historic architectural and linear properties built in 1976 or earlier, adjacent to I-270 in Commerce City and unincorporated Adams County, are present within the project APE (this includes 51 properties recorded during this survey and 20 properties recorded as part of other surveys). One architectural property, located at 6680 York Street and constructed in 1950, is located in the APE and is over 50 years old but was not surveyed because it is buffered from all work by surrounding properties and no direct or indirect impacts are anticipated for this property. The location of this property has been noted on the APE maps with an asterisk.

E. Environment: Natural and cultural

The study area is located along the I-270 corridor with significant industrial, commercial, residential, and transportation development. The natural landscape is an open plain with river and creek valleys. The nearest major waterways are Sand Creek, which runs south of and roughly parallel to I-270 in the southeast half of the study area; the South Platte River, which passes under I-270 near the mid-point of the study area; and Clear Creek, which passes under I-270 just northwest of the interchange with I-76. Sand Creek and Clear Creek are both tributaries of the South Platte River. The Burlington Ditch/O'Brien Canal is also located within the study area, passing under I-270 just east of the South Platte River. Undeveloped areas along the project corridor are generally undeveloped fields representing the remains of abandoned agricultural properties.

The I-270 corridor is characterized by a swath of commercial and industrial properties extending along the entire length and on either side of the interstate. These properties include warehouses and distribution centers, small- to medium-sized manufacturing facilities, and retail facilities, as well as the large oil refining facility currently operated by Suncor Energy. Residential developments include a two-block area at the intersection E. 52nd Place and Oneida Street and the Tiffany Subdivision between Niagara Street and Kearny Street, both of which are located just north of I-270 in the southeastern half of the study area. Another residential area is located north and south of the interstate along York Street in the northwestern half of the study area. The study area also includes several major transportation corridors including the Union Pacific Railroad, Burlington Northern Santa Fe Railway, Quebec Street (State Highway [SH] 35), Vasquez Boulevard (U.S. Highway [US] 6/US 85), Brighton Boulevard (SH 265), and I-76.

F. Historic context

(1) Early settlement, immigrant communities, and agriculture

The confluence of Sand Creek and the South Platte River was settled by Euro-Americans as an agricultural area that supported the growing city of Denver in the nineteenth century. Denver was founded following gold discoveries in the Rocky Mountains in 1858. In the 1870s enterprising businessmen established large smelters to process gold, silver, and lead on the northeast side of Denver, which became the city's industrial hub, supported by the early railroad network.¹ Colorado's first railroad, the Denver Pacific (DP), was completed in 1870 to connect Denver with the Union Pacific (UP) line in Cheyenne. The Chicago, Burlington, and Quincy Railroad (CB&Q) completed a connection between

¹ Leonard and Noel, *Denver: Mining Camp to Metropolis* (University Press of Colorado, 1990) 39, 58; Abbott, Leonard, and Noel, *Colorado: A History of the Centennial State* (University Press of Colorado) 130, 199.

Denver and the Nebraska state line in 1882, which intersected the DP at Sand Creek Junction near the current intersection of Brighton Boulevard and E. 60th Avenue.² The smelters, meat packing plants, and other factories, largely removed from the rest of the city, required armies of laborers, many of whom immigrated from Russia and countries in Eastern and Western Europe. Family members and friends helped one another find work, and families from the same country typically settled near one another, establishing ethnic groupings within neighborhoods, centered around churches, to share and preserve their religious and cultural traditions.³

The Italian immigrants in Denver established a particularly close-knit community. Italians came to Colorado in search of jobs working on the railroads, as well as in the smelters and other industries. Unlike the Eastern European immigrants who settled near the factories, sometimes in company housing, the Italian community settled along the banks of the South Platte River in North Denver and further north in Arapahoe County (currently Adams County), in the area known as “The Bottoms.” These immigrants used the soil along the riverbanks to grow vegetables in private gardens, initially to support themselves and later selling their produce for additional income at Denver’s City Market, and later the Denargo Market. The City Market opened in 1899 near the intersection of West Colfax Avenue and Curtis Street. This large open-air market supplied city residents, grocery stores, restaurants, and wholesalers with locally grown fresh produce (see Figure 5). The City Market remained the city’s primary produce market for decades, although it was relatively far from rail yards and prone to flooding from the South Platte River. In 1939 the Growers Public Market Association, a local farmers organization, partnered with the UP to build the Denargo Market near Brighton Boulevard and 29th Street. Located adjacent to the UP rail yards, the Denargo Market provided local growers with easy access to national distribution and farmers in Adams County, north of Denver, had direct access to the new market along York Street and Brighton Boulevard.⁴

² Tivis E. Wilkins, *Colorado Railroads: Chronological Development* (Boulder, Colo.: Pruett Publishing Company, 1974), 3, 41; Kenton Forrest and Charles Albi, *Denver’s Railroads: The Story of Union Station and the Railroads of Denver* (Golden, Colo.: Colorado Railroad Museum, 1981), 196.

³ Rebecca A. Hunt, *Urban Pioneers: Continuity and Change in the Ethnic Communities in two Denver, Colorado neighborhoods: 1875-1998* (Ph.D. Dissertation, University of Colorado, 1999), 52; Daniel Doeppers, “The Globeville Neighborhood in Denver,” *The Geographical Review* 57, No. 4, 1967, 509.

⁴ Giovanni Perilli, *Colorado and the Italians in Colorado* (Dr. G. Perilli, 1922), 27–29; Christine A. Derose, “Inside ‘Little Italy’: Italian Immigrants in Denver,” *Colorado Magazine* 54, no. 3 (Summer 1977): 278; Alisa Zahler, *Italy in Colorado: Family Histories from Denver and Beyond* (Denver: Colorado Historical Society, 2008), 56–57, 195–98.



Figure 5. Photo of the City Market in Denver, c. 1900.⁵

In 1908 the Denver-Laramie Realty Company, a subsidiary of the Denver, Laramie & Northwestern Railroad, laid out the 20-block town of Welby just north of the confluence of Clear Creek and the South Platte River at York Street and E. 78th Avenue, which attracted numerous Italian families. Welby evolved into a significant Italian community on Denver's rural edge with over 300 families by 1920. The heart of the Welby community was the Assumption Roman Catholic Church (see Figure 6). In 1911 several local families petitioned the church for a local parish, and Father Joseph Bosetti was installed as the first priest. A small church was constructed in 1911-1912. The Assumption School was constructed next to the church in 1920. The Servite Sisters came to Welby to run the school, which offered grades 1-12. The Assumption High School was closed in 1952, but the school continues to teach lower grades. A new church building, which still stands, was constructed in 1947. Within the largely Catholic Italian community the church was the center of social life where most children went to school, bazaars celebrating the end of harvest season, and spaghetti dinners celebrating the feasts of saints. The Cline School, located across the street from Assumption Church, was another fixture in Welby. Built in 1884, it operated as a one-room schoolhouse until 1950 and was demolished in 2001. The intersection of York Street and E, 78th Avenue was also the commercial center of Welby. Dominic Rotolo opened the first store in Welby, the Welby Merchandise Company, which was later owned by the Cosimi Family (see Figure 7). Francisco

⁵ Zahler, *Italy in Colorado: Family Histories from Denver and Beyond*, 196.

“Frank” Ciancio opened the Welby Mercantile Company in the 1920s and later opened a restaurant, Ciancio’s Famous Dinners, next to the Assumption school (see Figure 8).⁶



Figure 6. Assumption Roman Catholic Church and rectory currently located at 2361 E. 78th Avenue in Welby. Mead & Hunt, Inc., 2020.



Figure 7. A group of Welby residents gathered outside Dominic Rotolo’s Welby Merchandise Company, c. 1920.⁷

⁶ Alwen Bledsoe, “Assumption Church in Welby Celebrates 90th Birthday, Servite Order Operates Parish in Rural North Denver Hamlet,” *Denver Catholic Register*, May 29, 2002, Assumption Church Welby Colorado, 3016, History Colorado; Agnes Domenico, “Our Lady of the Assumption, 1912-1987,” 1987, 9–11, Assumption Church Welby Colorado, 3016, History Colorado; Zahler, *Italy in Colorado: Family Histories from Denver and Beyond*, 58–59, 224–25; Albin Wagner and Carl Dorr, “Crossroads of the West: A History of Brighton and the Platte Valley,” in *The History of Brighton, Colorado and Surrounding Area* (Brighton, Colo.: Brighton Genealogy Society, Brighton Historic Preservation Commission, 2006), 87–89.

⁷ Zahler, *Italy in Colorado: Family Histories from Denver and Beyond*, 59.



Figure 8. *Ciancio's Famous Dinners* (nonextant) was located at the intersection of York Street and E 78th Avenue in Welby, next to the Assumption Church and School.⁸

Most of the Italian families in Welby established small five- and ten-acre truck farms. The farms in the Welby area on the west bank of the South Platte River supplied fresh produce in Denver until the 1950s, when farming declined in southwest Adams County, and larger grocery stores replaced the open public markets in Denver. These farmers sold their produce at individual stalls that they rented out at the Denver markets, but also organized to ensure the collective success of the community. In 1923 Caesar A. Gerali organized the Vegetable Producers Cooperative Association, also known as the Grower's Association, to ensure fair prices for all members. According to local historian Albin Wagner, the Growers Association was "one of the first endeavors at cooperative vegetable marketing in Colorado."⁹ After major floods in 1933, local farmers successfully petitioned the Department of the Interior to build a dyke along the west bank of the South Platte River to protect their farms from future flooding. The Welby Community Improvement Association was established in 1937, with Frank Ciancio as the first president. This association advocated for numerous public projects including the first paving of York Street, the community's primary route into Denver. In the early 1950s Welby residents also established a local chapter of the Knights of Columbus, a national fraternal organization founded in 1882 as a Catholic immigrant aide society. The Welby chapter built the Knights of Columbus Hall at 6910 York Street and rented out residences nearby to help fund the activities of the chapter. They were involved in local fundraisers and other campaigns, and the hall became an important community center for celebrations.¹⁰

⁸ Zahler, *Italy in Colorado: Family Histories from Denver and Beyond*, 225.

⁹ Wagner and Dorr, "Crossroads of the West: A History of Brighton and the Platte Valley," 89.

¹⁰ *The Knights of Columbus: Handbook for Councils Using Home Corporation Facilities* (New Haven, Conn.: Knights of Columbus, n.d.), 6–7, <https://www.louisianakc.org/legalinfo/home%20corp.pdf>; "History of Coronado Council No. 3268," 1988, Assumption Church Welby Colorado, 3016, History Colorado; Domenico, "Our Lady of the Assumption, 1912-1987," 11; Wagner and Dorr, "Crossroads of the West: A History of Brighton and the Platte Valley," 87–89.

The Mouat and Starke's Subdivision, laid out in 1902 along the east side of York Street between the present E. 66th Avenue and I-270 frontage road, exemplifies the small truck farms that proliferated in the Welby area. This subdivision consisted of eight narrow, ten-acre lots that stretched between York Street and the South Platte River, and were irrigated by Gardener's Ditch. Gardener's Ditch, first constructed in 1861, predated Italian settlement north of Denver and was one of the earliest irrigation ditches in the Denver area. Some of the lots in the subdivision were further divided into five-acre parcels, including 6810 York Street and 6820 York Street. The farmhouses on these parcels were built on the higher ground close to the road and the cultivated fields were on the floodplain adjacent to the river and irrigation ditch. The houses on these and other truck farms were generally modest structures with gable roofs, sometimes with columns and porches. Smaller agricultural outbuildings such as garages, workshops, barns, and sheds were also common.¹¹

Other immigrant groups settled on the rural plains north of Denver and east of the South Platte River. Danish settlers raised hogs on large farms along Sand Creek. Additional hog farms were established along the South Platte River in the twentieth century. North of Sand Creek, German and Scandinavian families started their own vegetable truck farms. Japanese immigrants who first came to Colorado to work on the railroads settled along the South Platte River between Denver and Brighton and raised vegetables and sugar beets into the mid-twentieth century. Many Japanese farmers sold their produce directly to early canneries in Brighton.¹²

Truck farmers also raised dairy cows to sell milk and cheese at the markets, and their small operations developed into successful dairies including Morning Sun Farm, White Star Dairy, Fairview Farms, and IXL Dairy (see Figure 9). The Garden Farms Dairy operated for a time at 4100 E. 60th Avenue. The many dairies in the current Commerce City area remained relatively small family operations; however, in other parts of Adams County several businessmen transformed their small operations into one of the largest industries in the county, establishing Adams County as the "milkshed of Denver." The Brighton Creamery, established in 1883, was the first large dairy operation in what would become Adams County. In 1899 O.E. Frink built a cheese factory in Brighton. Frink expanded his operation beyond Brighton and owned several creameries around Colorado, including locations in Lafayette, Alamosa, and other small towns. Frink later started a canning factory in Fort Lupton and his son, Clarence B. Frink, expanded to canning factory to include dairy operations. Clarence Frink started the Frink Dairy Company, which evolved into the Northern Colorado Dairy Company. David McIntosh started the McIntosh Dairy, which still operates in Thornton, in the early twentieth century. Jonas Washburn also owned a number of dairies in Adams County, including the Washburn Jersey Dairy Company based in Aurora. Industrial development in

¹¹ "Map of Mouat and Starke's Subdivision of Part of Section 1, Twp. 3 S., Rng. 68 W.," October 27, 1902, Adams County Clerk and Recorder; John F. Hoffecker, *OAHM Management Data Form 5AM.1292: Gardener's Ditch or Heller's Ditch* (Prepared by Pinyon Environmental Engineering Resources Inc., June 1999), Office of Archeology and Historic Preservation; *Commerce City Historic Preservation Plan* (Commerce City, Colo.: Commerce City, January 2015), 18.

¹² Wagner and Dorr, "Crossroads of the West: A History of Brighton and the Platte Valley," 79–80; Albin Wagner, *Adams County, Colorado: A Centennial History 1902-2002* (Brighton, Colo.: Adams County Board of County Commissioners, 2002), 126; Dorothy Miller, *A Time to Remember: Commerce City Area Memories* (Homespun Publication, 1997), 7.

Commerce City in the mid-twentieth century overran the smaller family owned dairies, but many of the larger operations remain active throughout Adams County.¹³



Figure 9. A milk truck for the IXL Dairy, one of the many dairies formerly located in the current Commerce City area.¹⁴

The Welby community is in unincorporated Adams County, which was established in 1902 and carved out of Arapahoe County. Arapahoe County had been established as part of the Kansas territory in 1855. When Colorado became a territory in 1861 and a state in 1876, Arapahoe County extended from the Kansas state line to the Rocky Mountains and included Denver. Unsuccessful motions were made in the 1880s and 1890s to create Adams County out of the northern half of Arapahoe County. In 1902 the City and County of Denver was established, along with Adams County to the north and the current Arapahoe County to the south. Brighton, the largest city in the new Adams County, was designated the county seat. Other small communities emerged in the area of southwest Adams County around the turn of the century. The town of Derby was laid out in 1889 around a station on the CB&Q Railroad near the current intersection of E. 72nd Avenue and SH 2. Irondale, also laid out in 1889 near the current intersection of E. 84th Avenue and Rosemary Street, was planned as a factory town for the Kibler Stove Co., which intended to hire up to 200 men for its new factory. Although the factory only operated until 1893, residents remained in the area and Irondale was incorporated in 1926. Declining population in subsequent years led to the unincorporation of Irondale in the 1930s. Adams City was laid out in 1903 near the current intersection of E. 70th Avenue and US 85 with the intention of becoming the seat of Adams County. After Brighton secured the county seat Adams City remained small, but several public schools were built near each other at E. 69th Avenue and Cherry Street, including the original Adams City High School. East of Adams City the Rose Hill community grew and became known for the Rose Hill Cemetery, a Jewish

¹³ Marjorie Christiansen and Estelle Hadley, *Commerce City Reminisces* (Commerce City, Colo.: Commerce City Department of Community Development, 1980), 8; Wagner and Dorr, "Crossroads of the West: A History of Brighton and the Platte Valley," 79–80; Wagner, *Adams County, Colorado: A Centennial History 1902-2002*, 126; Miller, *A Time to Remember: Commerce City Area Memories*, 7.

¹⁴ "Oldie-823x365," n.d., Commerce City Historical Society, <http://www.cchistoric.com/gallery-item/historic-images/>.

cemetery established in 1892. Each of these communities remained relatively small and agricultural in nature until the end of World War II.¹⁵ Figure 10 shows an overview of the area.

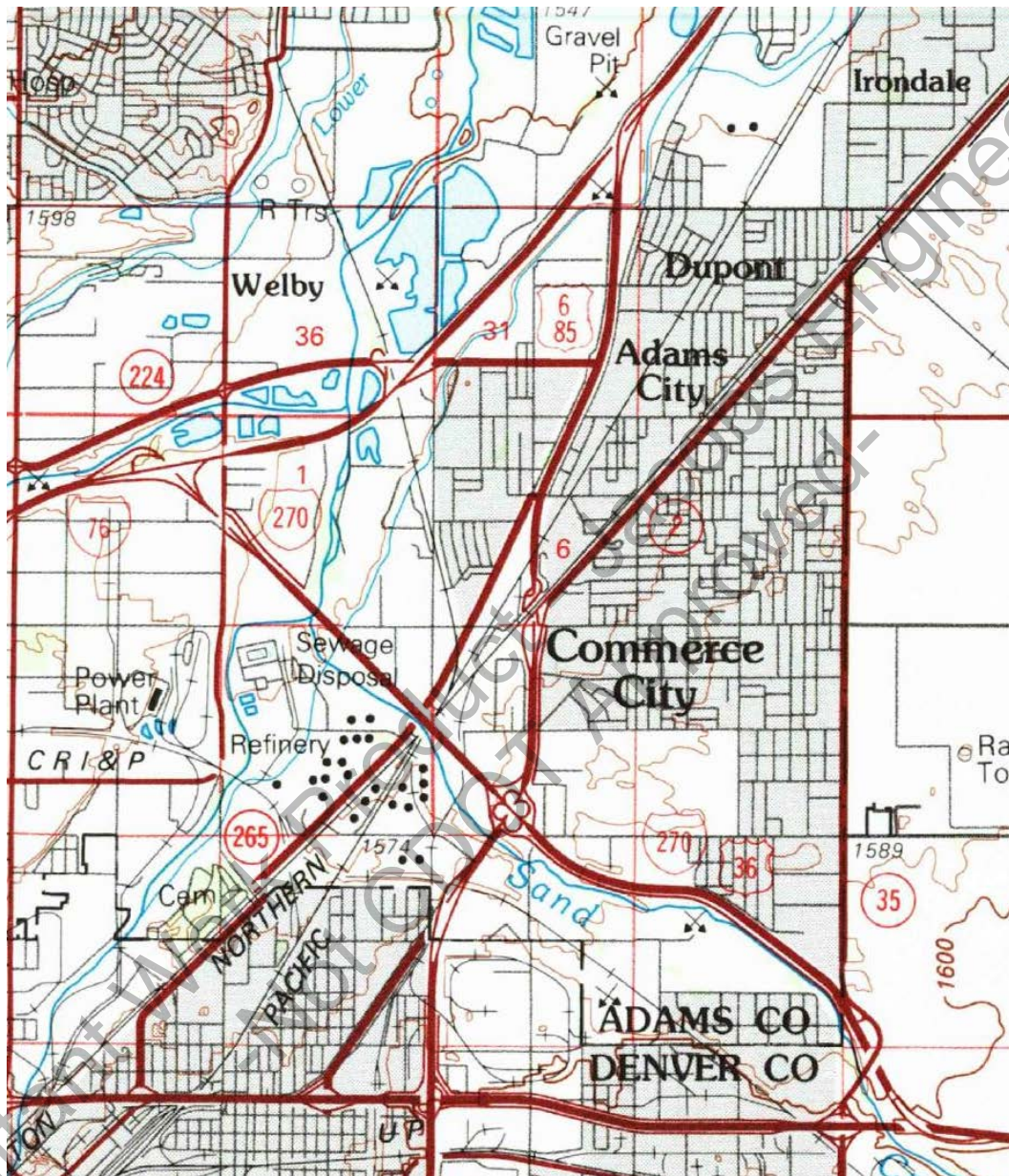


Figure 10. 1981 USGS map showing an overview of the Commerce City area including I-270 in relation to the communities of Welby, Adams City, Dupont, and Irondale.¹⁶

¹⁵ Wagner and Dorr, "Crossroads of the West: A History of Brighton and the Platte Valley," 58–59, 96–97; "Derby, South Adams County, Colorado, 1954" (New York: Sanborn Map & Publishing Company, 1954); "Adams City, South Adams County, Colorado, 1954" (New York: Sanborn Map & Publishing Company, 1954).

¹⁶ U.S. Geological Survey, "Denver East, CO" (Washington D.C.: U.S. Department of Interior, Geological Survey, 1981).

(2) Oil production in Colorado and Commerce City

Adams County's industrial transformation began in 1930, when Continental Oil opened its refinery on Brighton Boulevard. The first oil discoveries in Colorado were found near Canon City in 1860. Although this field was modest in comparison to later discoveries in the state, it produced a significant amount of oil for kerosene production. The Continental Oil and Transportation Company, which would become the largest oil company in Colorado, had its start in Ogden, Utah. Isaac Elder Blake, who had prospected in the Pennsylvania and West Virginia oil fields prior to his journey west, established the company in 1875 to transport kerosene by rail from Canon City to Utah. The following year Blake established the company's main office in Denver. In 1881 a far more productive oil discovery was uncovered in Florence, Colorado. While several small oil companies and refineries emerged in Southern Colorado, Continental Oil continued to specialize in the transportation of crude and refined oil products. Continental joined with Standard Oil in 1885, and by the late 1880s Continental was marketing nearly all of the oil products in Colorado. Continental Oil split with Standard Oil as part of an anti-trust Supreme Court ruling in 1913 but continued to operate in Colorado. At the time most of the oil production was occurring in western Colorado; however, transportation over the mountains proved to be costly and difficult. In 1923 the California-based Union Oil Company tapped the Wellington-Fort Collins oil field approximately 60 miles north of Commerce City, which proved to be one of the largest oil discoveries up to that time in Colorado.¹⁷

Continental Oil purchased the rights to the Fort Collins-Wellington field in 1929. That same year the company merged with Marland Oil based in Ponca City, Oklahoma. Ernest W. Marland, who later served as the governor of Oklahoma from 1935-1939, established Marland Oil in 1921. The two companies merged under the name Conoco. In 1930 Conoco built a new refinery on the west side of Brighton Boulevard at Sand Creek Junction (see Figure 11). The location was chosen due to the proximity to the UP and CB&Q rail lines and an available water source from Sand Creek. By 1940 Bay Petroleum, later Colorado Refining Co. (CRC), had also opened a refinery on the east side of Brighton Boulevard (see Figure 12). Colorado Oil Refining and Colorado Midland Refineries also had refining operations in the Denver area. Together the four companies produced three quarters of the state's oil products.¹⁸

¹⁷ Lee Scamehorn, *High Altitude Energy: A History of Fossil Fuels in Colorado* (Boulder, Colo.: University Press of Colorado, 2002), 43, 48, 65; "1909-1875," *ConocoPhillips*, accessed September 17, 2019, <http://www.conocophillips.com/about-us/our-history/1909-1875/>.

¹⁸ Scamehorn, *High Altitude Energy: A History of Fossil Fuels in Colorado*, 67-68, 121; "1929-1910," *ConocoPhillips*, accessed September 17, 2019, <http://www.conocophillips.com/about-us/our-history/1929-1910/>.



Figure 11. Continental/Conoco oil refinery on the west side of Brighton Boulevard just south of Sand Creek Junction, c. 1940.¹⁹



Figure 12. CRC oil refinery on the east side of Brighton Boulevard just south of Sand Creek Junction, c. 1940.²⁰

¹⁹ "Continental Oil Co.," 1940 1930, Western History Digital Collections, X-24471, Denver Public Library.

²⁰ "Continental Oil Refinery," 1940 1930, Western History Digital Collections, X-24470, Denver Public Library.

In 1949 Conoco modernized and expanded the refinery to the south and west, including expansion of the catalytic cracking facilities used to distill different petroleum products. By the early 1950s the Conoco and CRC refineries had largely grown to fill their current land parcels (see Figure 13). In the 1970s the Conoco and CRC facilities on Brighton Boulevard were the only two refineries remaining in the Denver area. Further expansions of the catalytic crackers at both facilities were completed in the late 1990s (see Figure 14).²¹ In 2002 Conoco merged with the Phillips Petroleum Company to form ConocoPhillips. In 2003 ConocoPhillips sold its refinery on the west side of Brighton Boulevard to Calgary (Canada)-based Suncor Energy Inc. Suncor also purchased the refinery on the east side of Brighton Boulevard from CRC in 2005. Currently, the combined facilities under Suncor Energy Inc. represent the only refinery in Colorado.²²



Figure 13. 1953 (left) and 1978 (right) aerial photographs of the Conoco and CRC refineries following the 1949 expansion.

²¹ Scamehorn, *High Altitude Energy: A History of Fossil Fuels in Colorado*, 123.

²² "2009-1990," *ConocoPhillips*, accessed September 18, 2019, <http://www.conocophillips.com/about-us/our-history/2009-1990/>; "Suncor Buys Denver Refinery, Stations," accessed September 18, 2019, <https://www.theglobeandmail.com/report-on-business/suncor-buys-denver-refinery-stations/article1160267/>; "Suncor Acquires Second Valero Refinery," *CSP Daily News*, June 1, 2005, <https://www.cspdailynews.com/fuels/suncor-acquires-second-valero-refinery>.



Figure 14. 2002 aerial photograph of Conoco and CRC refineries. The catalytic crackers, indicated with blue arrows, were significantly expanded at both facilities in the 1990s and 2000s.

(3) World War II and postwar development

During and after World War II the land use of southwest Adams County diversified from primarily agricultural to industrial and residential uses. Adams County became the location of the Mile High Greyhound Race Track (demolished in 2008), Rocky Mountain Arsenal chemical weapons plant (Arsenal), the Metropolitan Denver Sewage Disposal District (Metro Wastewater), and sand and gravel quarries.²³ Post-World War II (postwar) residential subdivisions proliferated throughout the county to provide housing for workers in these and other work places throughout the metro area.

Adams County experienced explosive growth between 1940 and 1970. The population of the county in 1940 was over 22,000, which grew to nearly 186,000 in 1970. The number of single-family homes built during this period also represents the shift from agricultural to residential land use in the county. Between 1940 and 1965, 32,097 residential dwellings were built in Adams County, with more than one third of these occupying parcels in unincorporated Adams County. The peak of the subdivision development occurred between 1955 and 1959, when 12,921 single-family residences were constructed in the county.²⁴

²³ Thomas Noel and Stephen J. Leonard, *Denver: Mining Camp to Metropolis* ([Boulder, Colo.]: University Press of Colorado, 1990), 345.

²⁴ Thomas H. Simmons, R. Laurie Simmons, and Dawn Bunyak, "Historic Residential Subdivisions of Metropolitan Denver, 1940-1965." (Colorado Department of Transportation, Region 6, 2011), J-213, J-218.

According to data provided in the Historic Residential Subdivisions of Metro Denver, 3,524 residential dwellings were constructed in Commerce City between 1940 and 1965. Four subdivisions with more than 100 parcels each accounted for 725 of these dwellings, or less than 20 percent of the overall number of dwellings constructed within the city during this time period. This indicates that most of the postwar dwellings built in Commerce City were either in neighborhoods with less than 100 parcels, or as single-family residences that were not built as part of subdivisions. Similar data on the total number of subdivisions built in Adams County during this period is not available.²⁵

The Federal Housing Administration (FHA), created in 1934 with the National Housing Act, had far-reaching guidelines and standards that controlled the financing and physical development of residential home building in neighborhoods across the U.S. The FHA insured loan programs for both home buyers and builders and developed standards for home construction, house inspections, and subdivisions, meant to ensure that homes and neighborhoods would meet aesthetic guidelines and be attractive to homebuyers. FHA guidelines recommended homebuilders place subdivisions near schools and shopping centers or build these amenities and other types of community centers. Infrastructure considerations included providing or utilizing adequate water, sewer, natural gas, electrical systems, and nearby transportation corridors. The FHA also recommended curvilinear street layouts and sidewalks, with cul de sacs, setbacks, and minimum lot sizes to provide ample front and back yards while at the same time maximizing the number of lots.²⁶

One of the biggest drivers of growth in Adams County came from the Arsenal. In 1940 the U.S. Army began to plan the construction of the plant and selected a 27-square-mile site in southwest Adams County, displacing approximately 200 family farms. Located between Quebec Street and Buckley Road and E. 56th and E. 86th Avenues, the Mountain Arsenal began to produce chemical and incendiary weapons in 1942. The Arsenal provided jobs to 3,000 men and women during the war and continued production during the Korean and Vietnam Wars, while also leasing sections to Shell Oil Company and other companies that made insecticides.²⁷

Much of the new development in Adams County after the war filled in the former farmlands between the Arsenal and the South Platte River. By the early 1950s the City of Denver had taken notice of the new industrial center on its northern edge and rumors began to circulate within the community that Denver was planning to annex this unincorporated corner of Adams County. In 1952 a group of citizens from small communities in Adams County voted to incorporate as Commerce Town. Commerce Town included the residential communities of Rose Hill and Adams City, and was originally bounded by Holly Street on the east, the South Platte River on the west, and the Denver city limits at 52nd Avenue on the south and approximately 67th Avenue on the north. The new community sought to avoid the zoning regulations that may have been implemented by Denver and advocated for the growth of commercial and industrial areas

²⁵ Simmons, Simmons, and Bunyak, "Historic Residential Subdivisions of Metropolitan Denver, 1940-1965.," J-234.

²⁶ Mead & Hunt, Inc. and Louis Berger Group, Inc., *A Model For Identifying and Evaluating the Historic Significance of Post-World War II Housing* (Prepared for the Transportation Research Board of the National Academies, National Cooperative Highway Research Program Report 723, 2012), 54–56, 73.

²⁷ Noel and Leonard, *Denver: Mining Camp to Metropolis*, 351–52; Simmons, Simmons, and Bunyak, "Historic Residential Subdivisions of Metropolitan Denver, 1940-1965.," E-31.

over residential development. In 1961 only 20 percent of Commerce Town was residential. Although the new community grew to the north through a series of annexations in the 1950s, it opted not to annex unincorporated areas west of the South Platte River where former farmland was being subdivided into residential developments, such as the Larusso Subdivision laid out in 1954. However, in 1962 Commerce Town annexed the larger community of Derby to its north, increasing the population from 4,000 to 16,000. Following the annexation of Derby, Commerce Town changed its name to Commerce City. The Arsenal hampered Commerce City's growth to the east, but the city continued to annex more land north and east of the Arsenal.²⁸

(4) Transportation and infrastructure

In the twentieth century, and particularly after World War II, automobile highways steadily supplanted the railroads as the backbone of commercial and industrial development in the Commerce City area. Brighton Boulevard (SH 265), Colorado Boulevard (SH 2), and US 6 became major transportation arteries in the area. Brighton Boulevard (SH 265) has served as a vehicle route for people entering Denver from the northeast since the 1880s. Vehicle traffic was significant enough in 1912 that the CB&Q Railroad built a railroad overpass to protect train and vehicle travel. Truck farmers, dairy farmers, and ranchers from Brighton and other northeastern Colorado communities used Brighton Boulevard to reach the Denver stockyards, processing plants, markets, and transportation centers. After World War II, the Conoco and CRC refineries also drew vehicle traffic along the artery. This route was an important, early, and direct link to transport agricultural goods between northeastern Adams County and Denver.²⁹ SH 2 also served as an important connection between Commerce City and the east side of Denver along Colorado Boulevard. SH 2 currently begins in the Denver metro area at Hampden Ave (US 285), travels north along Colorado Boulevard and connects with US 6 at Vasquez Boulevard, and then travels northeast through Commerce City to connect with I-76 near Brighton. The northeastern portion of SH 2 serves as a connector through Commerce City to I-76, traveling on the northwestern edge of the Rocky Mountain Arsenal, which became the Rocky Mountain Arsenal Wildlife Refuge in 1992.³⁰

US 6 and US 85 were important links to markets beyond Colorado prior to the construction of the Interstate Highway System. The segment of US 6 in northeastern Colorado was an early farm-to-market road that provided a vital connection between Colorado and Nebraska, providing access for rural farmers to transport goods and produce to markets and processing facilities in Denver and Commerce City.³¹ US 85 once ran the full length of the state of Colorado and facilitated early automobile travel between the agricultural lands of Colorado's high plains and mining regions along the Front Range during the first

²⁸ Wagner and Dorr, "Crossroads of the West: A History of Brighton and the Platte Valley," 97; Wagner, *Adams County, Colorado: A Centennial History 1902-2002*, 134-35; Nello Cassai, "Denver's No Nonsense Industrial Suburb," *Cervi's Rocky Mountain Journal*, January 25, 1961; "Larusso Subdivision," March 8, 1954, Adams County Clerk and Recorder.

²⁹ Mead & Hunt, Inc./Dill Historians LLC, *Colorado Historic Highway Inventory, Historical summary and Evaluation of Significance, State Highway (SH) 265*, 2017, 2.

³⁰ Mead & Hunt, Inc./Dill Historians LLC, *Colorado Historic Highway Inventory, Historical summary and Evaluation of Significance, State Highway (SH) 2*, 2017, 2.

³¹ Mead & Hunt, Inc./Dill Historians LLC, *Colorado Historic Highway Inventory, Historical summary and Evaluation of Significance, U.S. Highway (US) 6*, 2017, 5, 7.

several decades of the twentieth century. As such, US 85 was one of the state's most well-traveled routes prior to World War II and provided an important commercial link between Commerce City and the farms and markets in New Mexico and Wyoming.³²

In 1956 President Eisenhower passed the Federal-Aid Highway Act, which began construction of the Interstate Highway System. By the mid-1960s I-70, I-25, and I-76 (originally I-80S) were complete through Denver. These highways greatly increased commercial trucking traffic through Denver. Although these highways were close to the central Commerce City area, there was little direct access for Commerce City to the Interstate Highway System. In 1965 construction began on I-270, initially linking I-70 with I-76 through Commerce City. The section between I-70 and Vasquez Boulevard was completed in 1968 and a second section between US 85 and I-76 was completed in 1970. The construction of I-270 demolished older residential and industrial development east of the South Platte River and bisected a few remaining agricultural properties west of the river. An extension of I-270 between I-76 and I-25 was completed in 2000.³³

Another infrastructure development was the extension of Denver's sewage treatment facilities along the South Platte River. The Delgany Street Public Sanitary Sewer, constructed in 1892, was a significant public works project in Denver at the time it was built. The sewer began at Stout Street and 11th Street and followed 11th Street to its intersection with Delgany Street, at which point it followed Delgany Street until it discharged into the South Platte River at 31st Street. In 1895 it was extended as a combined sanitary and stormwater sewer to transport collected waste farther outside of the city.³⁴

By the 1930s new attention was focused on the health issues from having sources of drinking water contaminated by sewage disposal. By this time the Delgany Street Sewer Extension had dumped collected sewage and storm run-off into the South Platte River at 46th Street for 35 years. A 1932 report by the Colorado Department of Health linked the pollution of the river to cases of typhoid fever and dysentery in locations downstream of the sewage outlet and recommended that a sewage treatment plant be constructed. Eventually, the State Board of Public Health ordered the City to discontinue its present system of dumping sewage into the South Platte River. By October 1937 a sewage disposal plant was constructed on a 30-acre site west of the South Platte River at 51st Avenue and Franklin Street. By July 1938 the plant was processing approximately 38 million gallons of sewage per day.³⁵

In 1961 a new sanitation district formed: the Metropolitan Denver Sewage Disposal District Number 1, now known as the Metro Wastewater Reclamation District (Metro Wastewater), which serves the district of Denver and surrounding suburbs. In 1964 Metro Wastewater planned to construct a new wastewater

³² Mead & Hunt, Inc./Dill Historians LLC, *Colorado Historic Highway Inventory, Historical summary and Evaluation of Significance, U.S. Highway (US) 85*, 2017, 3, 5.

³³ Associated Cultural Resource Experts and Deborah Dobson-Brown, "Colorado State Roads and Highways" (National Register of Historic Places Multiple Property Submission, 2003), E42–43, E47, http://www.historycolorado.org/sites/default/files/files/OAHP/crforms_edumat/pdfs/645.pdf.

³⁴ Gail Keeley, "Denver's Brick Sewers, Historic Context" (Colorado Department of Transportation, Region 6, 2012), 58–61.

³⁵ Keeley, "Denver's Brick Sewers, Historic Context," 58–61.

treatment plant north of the South Platte treatment facility, which was eventually redeveloped as North Side Park. With the new treatment facility at 6450 York Street northwest of the Conoco oil refinery, Metro Wastewater started new and innovative programs for wastewater treatment when groundbreaking of the site occurred in 1964. In 1967 the new treatment plant opened and collected most of the wastewater in metro Denver. By 1972 the hydraulic capacity of the plant had been exceeded as it was treating 100 million gallons per day. In May 1974 Metro Wastewater began to construct a new plant next to the 1967 facility to increase capacity. This facility became known as the South Plant, which was also recognized as an innovative plant using recent technology, and a leader in research and development to use wastewater solids for soil amendments.³⁶ In the 2010s Metro Wastewater began purchasing parcels on the east side of York Street between E. 66th and E. 68th Avenues, including 6690 York Street.

(5) Further industrial development

Following the development of the oil refineries near Sand Creek, more industries were brought to the area by the multiple transportation networks. In 1937 Oscar Mallo of the Hungarian Flour Company constructed the large grain elevators that still stand just north of Sand Creek adjacent to the UP tracks. These grain elevators represent the connection of agriculture and industry as large-scale food processing facilities grew around the Denver area. The dairy at 4100 E. 60th Avenue was converted to a machine shop in the 1950s at the same time that similar small industrial businesses opened in Commerce City. US 6, US 85, and SH 2, served as major transportation arteries that advanced the industrial development north of Denver after World War II. As trucking came to replace locomotive transportation in the 1940s and 1950s, companies such as Beall Equipment of Colorado and Timpte, specializing in truck parts, snow plows, and trailers, set up shop in the new industrial corridor along Sand Creek.³⁷

These trucking companies used production sheds, a common industrial building type during this period, to produce their large equipment. The production shed is a generic term used to describe the single-story buildings that housed industrial machinery and were used to manufacture heavy equipment. The overall dimensions of production sheds varied based on the nature of the processes within but are generally characterized by their rectangular plan; wide, open bays; and sturdy construction. Heavy manufacturing required much larger tools and machinery, as well as equipment for lifting large components and products. Machinery and lifting equipment occupied the floor space of production sheds, and taller structures could include galleries or mezzanine levels in the side bays, which were used for storage or lighter assembly work. Materials-handling methods evolved over the course of the late nineteenth and early twentieth century, and these technologies in turn affected the layout and design of factory buildings. Early cranes used a jib mounted on a stationary pole, but by the late nineteenth century the traveling overhead crane was also a standard feature of many manufacturing facilities. These cranes required a structure that could support the weight of the crane and rails high above the shop floor and provided a

³⁶ Keeley, "Denver's Brick Sewers, Historic Context," 58–61.

³⁷ Wagner and Dorr, "Crossroads of the West: A History of Brighton and the Platte Valley," 97; Joseph E. King, *Colorado Engineering Context* (Denver: Prepared for Colorado Historical Society, 1984), 167–69; Cassai, "Denver's No Nonsense Industrial Suburb."

clear space free of columns that would impede its movement. This type of crane made it more practical to build long, narrow shops with a raised central crane-way, and gave the building type its distinctive form.³⁸

(6) Land use development in the I-270 corridor

Historic maps and aerial photographs are useful tools to portray the changes in land use along the I-270 corridor in the twentieth century. This section analyzes the changes in land use between the 1900s and the present with reference to specific areas east and west of the South Platte River.

(a) West of the South Platte River

West of the South Platte River, I-270 intersects York Street between E. 68th Avenue and E. 69th Avenue, just south of I-76. This area was first developed as part of the Welby community of small truck farms. The 1899 Willits Farm Map displays the early land subdivisions with only a handful of buildings indicated. Even at this early date, the distinctive narrow plots between York Street and the South Platte River can be seen (see Figure 15).

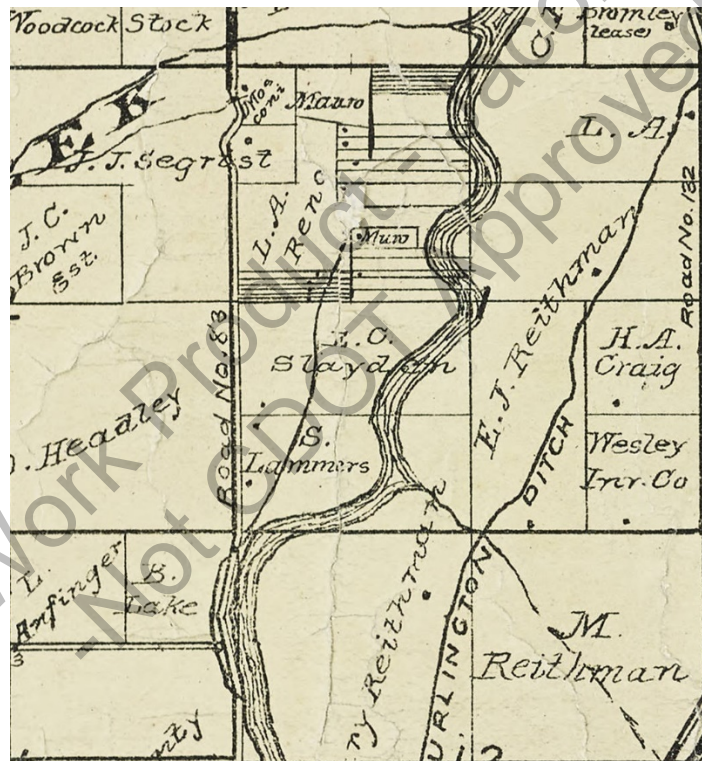


Figure 15. Section of the 1899 Willits Farm Map of Denver showing the subdivision of land and early settlement along York Street, marked Road No. 83, west of the South Platte River.³⁹

³⁸ Betsy H. Bradley, *The Works: The Industrial Architecture of the United States* (Oxford, England: Oxford University Press, 1999), 39, 47; Henry Grattan Tyrrell, *A Treatise on the Design and Construction of Mill Buildings, and Other Industrial Plants* (Chicago and New York: M.C. Clark Pub. Co., 1911), 5–11, 23, 35, <https://catalog.hathitrust.org/Record/100308187>.

³⁹ Warren C. Willits, "Willits Farm Map, Denver Colorado," 1899, Available at the Denver Public Library Western History Collection.

A 1938 U.S. Geological Survey (USGS) map indicates further agricultural development on the east side of York Street, with farmhouses and other buildings located on the high ground above the floodplains of the South Platte River and Clear Creek. The cultivated fields in these bottom lands are also seen in later aerial photographs. This map also indicates the presence of the White Cap Canal to the west of York Street, which extended from the South Platte River to Clear Creek and was most likely used for irrigation purposes. At this time there were only scattered buildings on the west side of York Street (see Figure 16). Prior to World War II this area was used almost entirely for agricultural production.

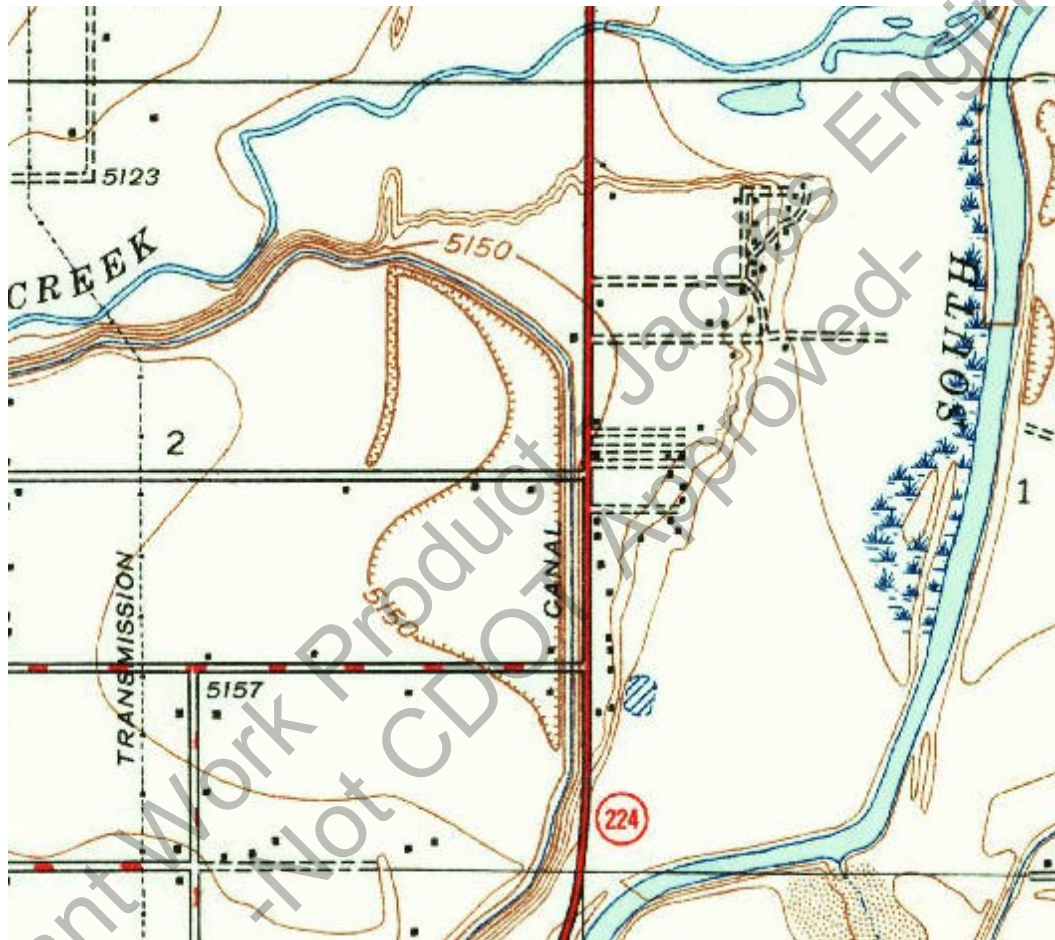


Figure 16. 1938 USGS map showing continuing agricultural development along York Street, labeled SH 224, above the floodplains of the Clear Creek and South Platte River. The White Cap Canal can be seen directly to the west of York Street.⁴⁰

Aerial photographs from the postwar period reveal that this area transitioned slowly from agricultural production to limited residential development, and ultimately to commercial and industrial uses. A 1953 aerial photograph shows that the area was still predominantly agricultural in the early 1950s, with cultivated fields on either side of York Street. A portion of the White Cap Canal is visible on this photograph; by 1957 the canal was abandoned and no longer appears on maps (see Figure 17).

⁴⁰ U.S. Geological Survey, "Derby, CO," 1:24000 (Washington DC: U.S. Department of Interior, Geological Survey, 1938).

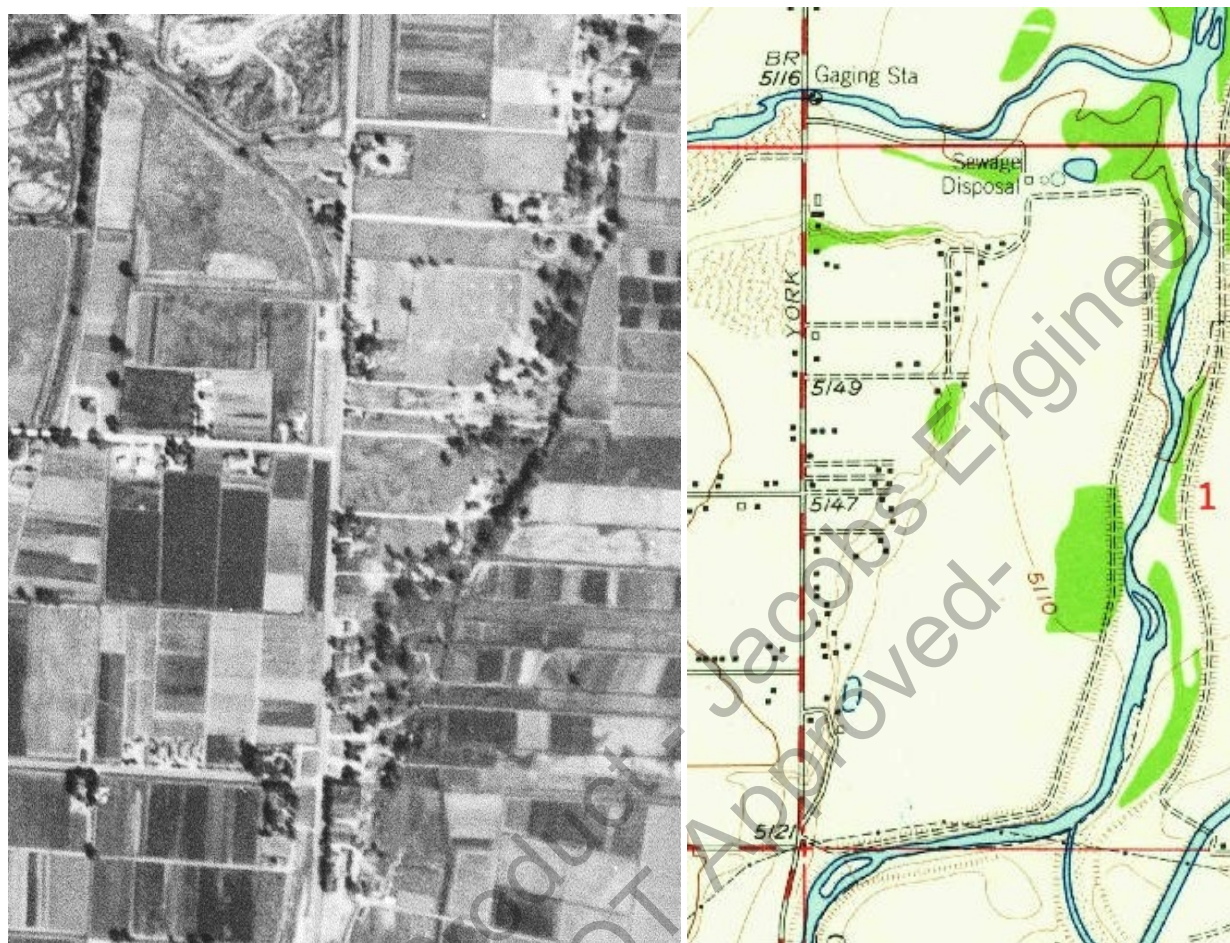


Figure 17. 1953 aerial photograph (left) and 1957 USGS map (right) depicting the continued presence of small truck farms and the abandonment of the White Cap Canal in the 1950s.⁴¹

By the 1970s the residential development of the area can be clearly seen. The Larusso and Harvest Acres subdivisions on E. 68th Avenue are built out, along with a grouping of mid-century houses to the east of York Street. While the early transition away from small farms is visible, the area remains dominated by agricultural fields. Significant changes began in the 1960s with the construction of I-270 and I-76. I-270 bisected the narrow farm properties to the east of York Street and cut through the northeast corner of the Larusso Subdivision, effectively cutting off future development of that small residential subdivision. The early stages of commercial development are also visible on York Street northeast of the Larusso subdivision and along E. 67th Avenue. The Denver Metro Wastewater plant at 6450 York Street was also constructed at this time (see Figure 18).

⁴¹ U.S. Geological Survey, "ARA001260574860" (U.S. Geological Survey, 1953), earthexplorer.usgs.gov, <https://earthexplorer.usgs.gov/metadata/4660/ARA001260574860/>; U.S. Geological Survey, "Derby, CO," 1:24000 (Washington DC: U.S. Department of Interior, Geological Survey, 1957).

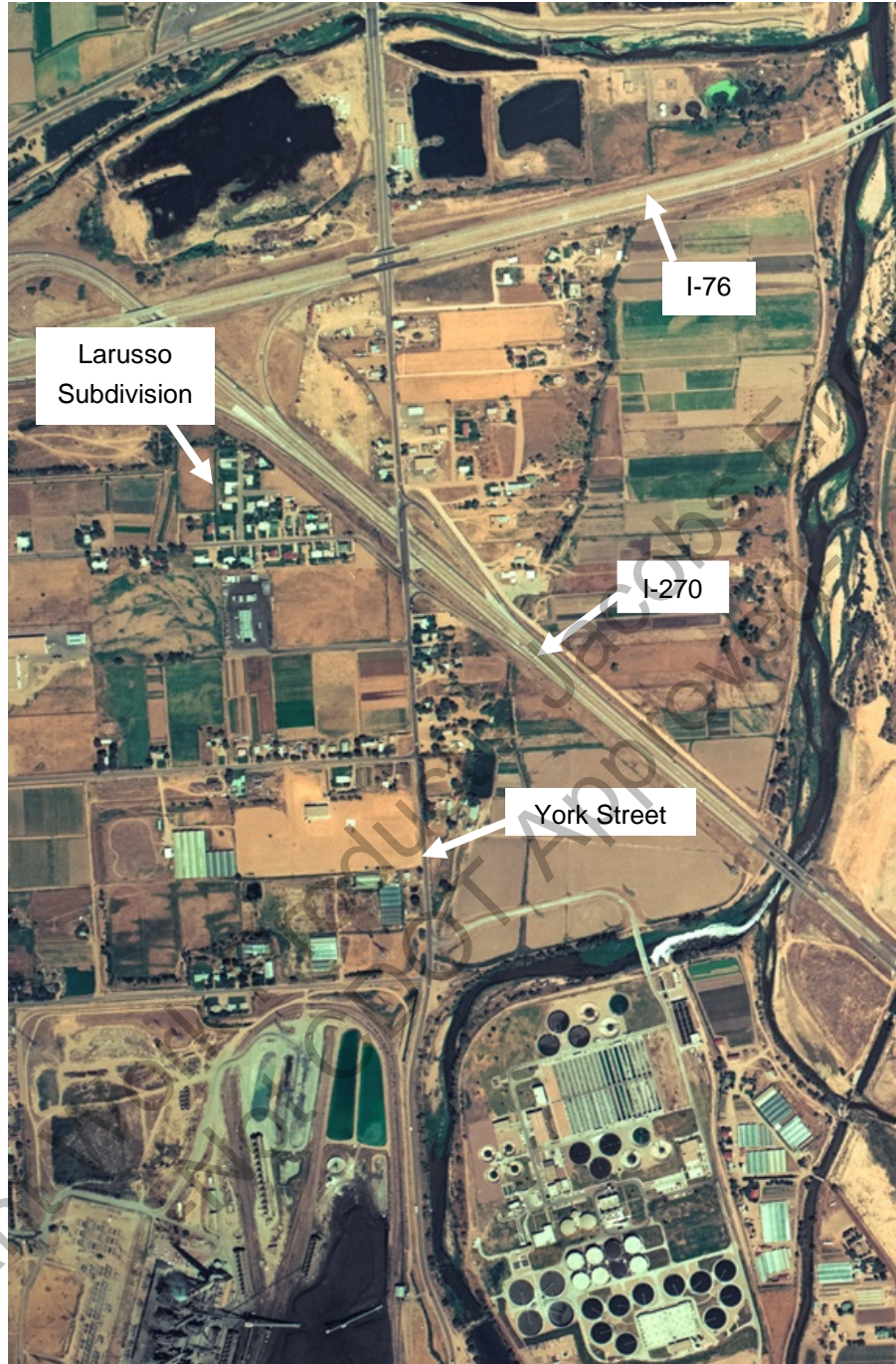


Figure 18. 1978 aerial photograph depicting the changes in land use from the mid-1950s to the mid-1970s. Agricultural fields remain to the east of York Street, but have been bisected by I-270 and I-76. Residential development can be seen east of York Street in the Lorusso Subdivision and Harvest Acres Subdivisions. The Metro Wastewater facility is visible in the bottom-right of the photograph.⁴²

⁴² U.S. Geological Survey, "AR1VEQCC0030054" (U.S. Geological Survey, 1978), earthexplorer.usgs.gov, <https://earthexplorer.usgs.gov/metadata/4660/AR1VEQCC0030054/>.

By the early 1990s commercial and industrial development west of York Street had continued to the point that is currently recognizable. The agricultural fields east of York Street were gone by 1999 and Cat Lake was constructed on that location in 2009 by the South Adams Water and Sanitation District and Denver Water Department. Metro Wastewater also expanded its property across the South Platte River in the late 1990s and 2000s and purchased former farm parcels on York Street between E. 64th and E. 67th Avenues.

(b) East of the South Platte River

East of the South Platte River, I-270 follows Sand Creek and crosses Vasquez Boulevard (US 6/US 85) between E. 56th and E. 60th Avenues. Maps and aerial photographs of this area indicate that the transition from agricultural to industrial land use occurred much more rapidly than on the west side of the South Platte. Land development in this area was similar to the area to the west until the 1930s. The 1938 USGS map indicates the start of an industrial shift with the presence of oil refineries at Sand Creek Junction, constructed in 1930, but there is little other development along Sand Creek (see Figure 19).

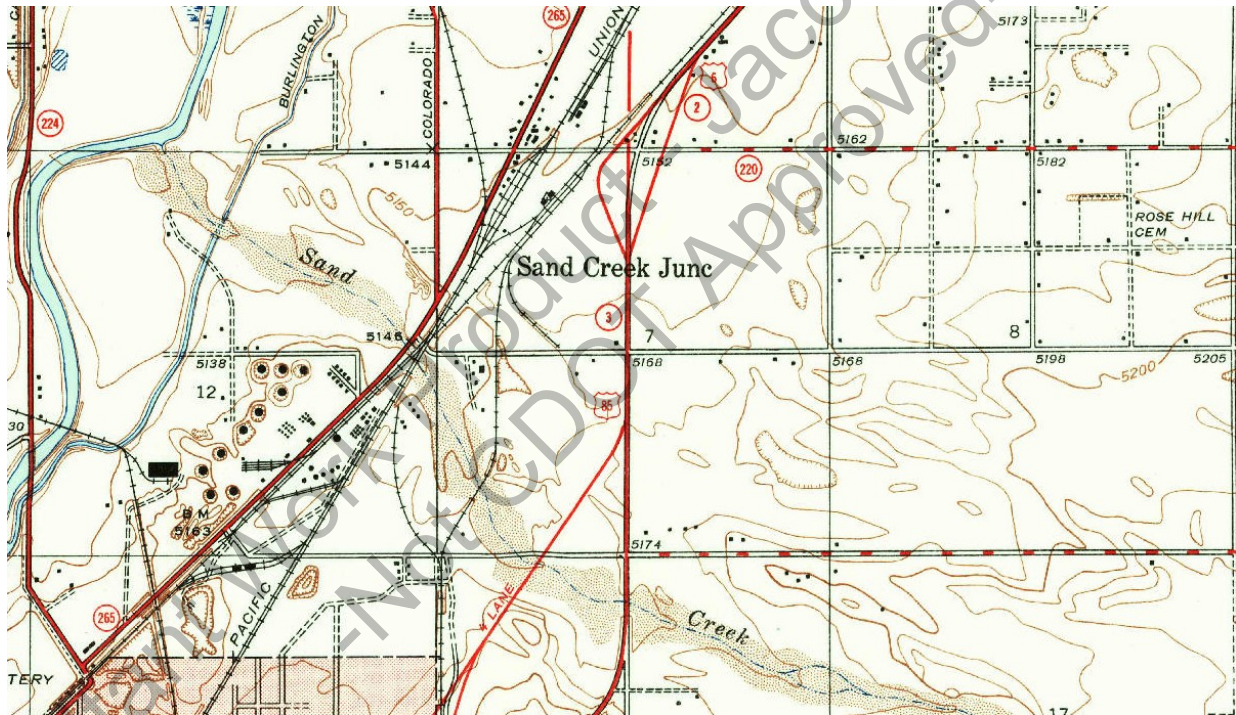


Figure 19. 1938 USGS map showing early-twentieth-century development in the Commerce City area. The early oil refineries are located southwest of Sand Creek Junction, but the area east of the railroads, including the central US 6/US 85 corridor, remains largely undeveloped.⁴³

Aerial photography from 1953 indicates the area was still largely agricultural, although a small industrial corridor began to emerge near the intersection of US 6/US 85 and E. 60th Avenue. A residential subdivision southwest of the intersection of E. 56th Avenue and Quebec Street, near the Rocky Mountain Arsenal, appears to have been laid out but not yet developed (see Figure 20). However, by 1957, following the incorporation of Commerce Town, maps indicate a fully developed commercial and industrial

⁴³ U.S. Geological Survey, "Derby, CO," 1938.

corridor along US 6/US 85. Similarly, the residential development near E. 56th Avenue and Quebec Street appears to be developed by 1957, including the Tiffany Subdivision and a small development on E. 52nd Place. This map also shows a large area of agricultural land remained in the center of Commerce Town (see Figure 21).

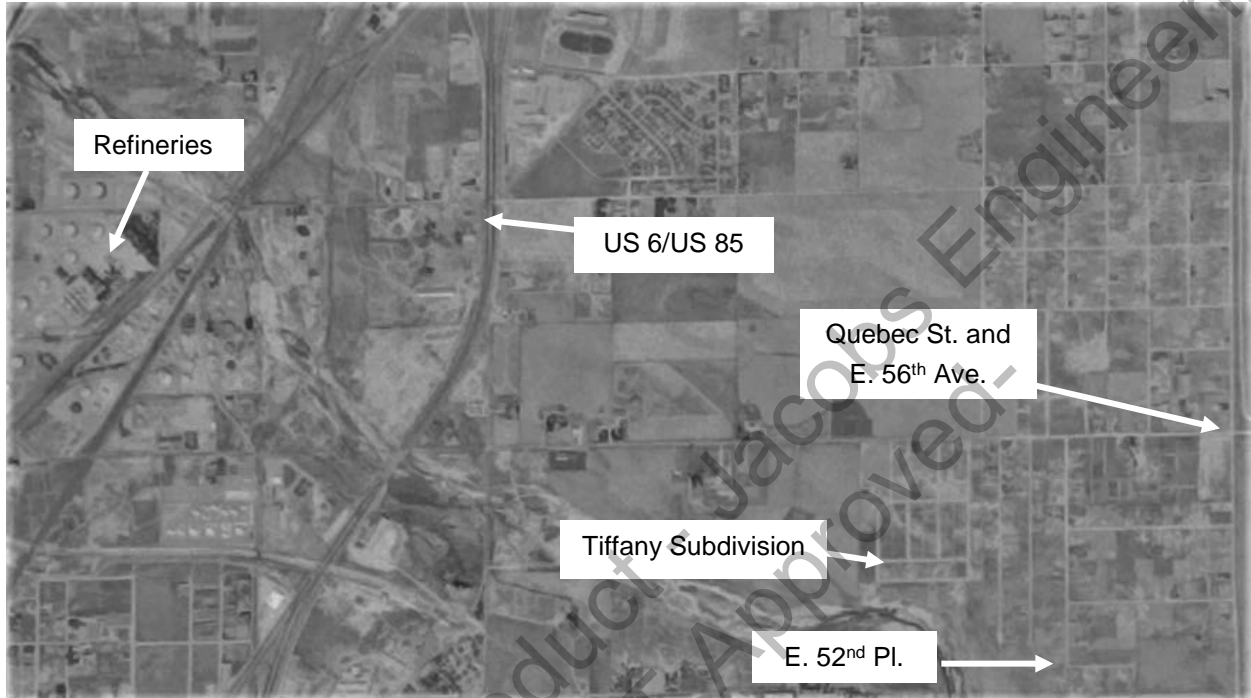


Figure 20. 1953 aerial photograph of Commerce Town. An industrial area can be seen between US 6/US 85 and the oil refineries along with early residential subdivisions to the east.⁴⁴

⁴⁴ U.S. Geological Survey, "AR1VAQC00030062" (U.S. Geological Survey, 1953), earthexplorer.usgs.gov, <https://earthexplorer.usgs.gov/metadata/4660/AR1VAQC00030062/>.

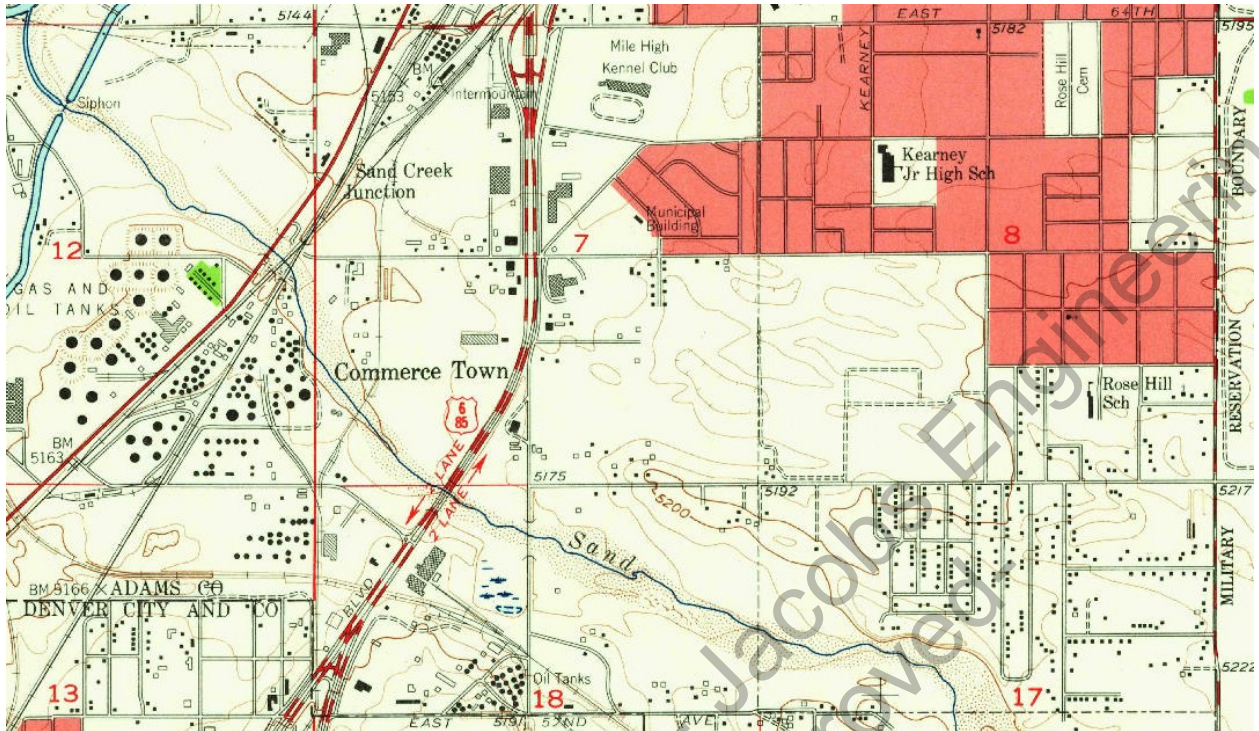


Figure 21. 1957 USGS map indicating the rapid growth of commercial and industrial properties along US 6/US 85, as well as residential subdivision to the southeast near the Rocky Mountain Arsenal.⁴⁵

By 1978 commercial enterprises and light industry had taken over residential properties north of E. 52nd Place. It also appears that by this time, all agricultural land use north of I-270 had ended. This trend continued into the 1990s and 2000s, with the land in the I-270 corridor predominantly used for commerce and industry surrounded by residential developments to the north (see Figure 22).

⁴⁵ U.S. Geological Survey, "Derby, CO," 1957.

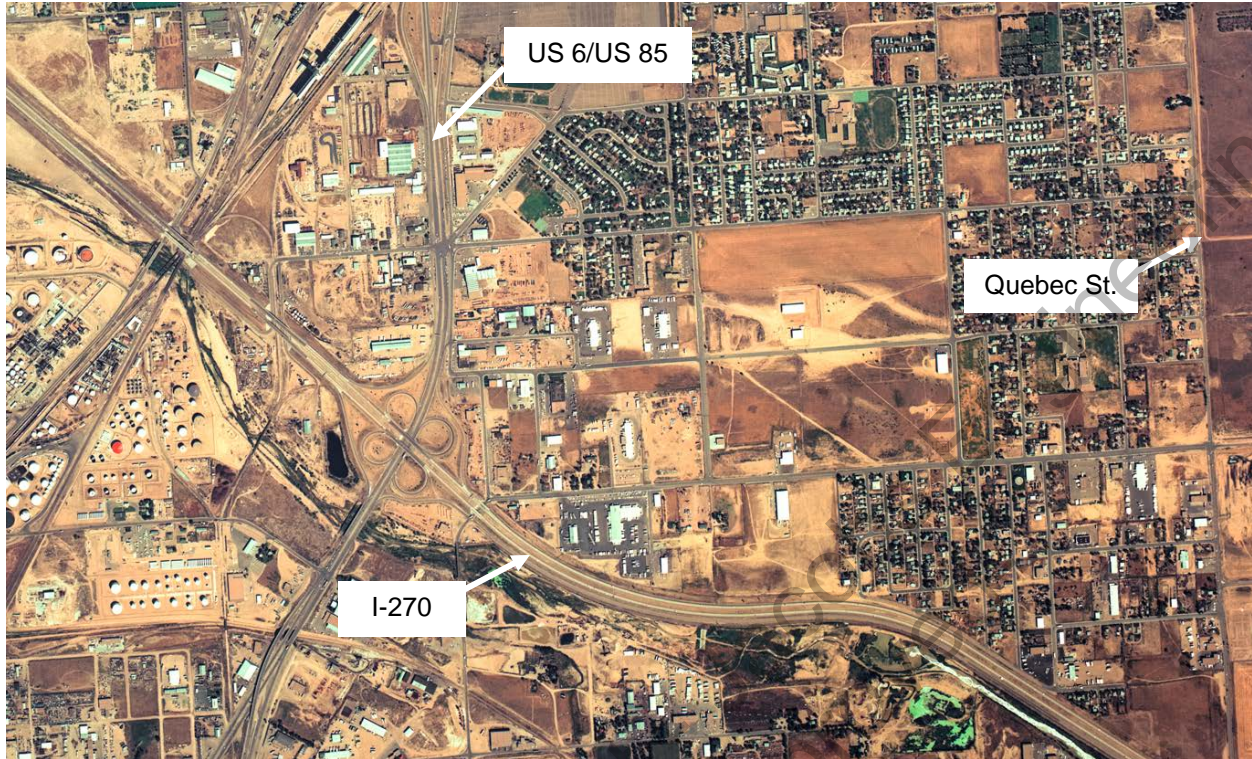


Figure 22. 1978 aerial photograph of Commerce City following the construction of I-270, which cuts diagonally through the area. The commercial and industrial center along the highways has grown and there are no longer any agricultural fields. To the southeast, another industrial area emerged adjacent to previous residential developments.⁴⁶

⁴⁶ U.S. Geological Survey, "ARA001260574860."

3. Research Design and Methodology

The objective of the historic resource investigations for the I-270 Environmental Assessment, was to identify and assess historic properties within the identified study area that possess significance and integrity to be eligible for the National Register. These documentation and significance assessments are intended to facilitate determination of the proposed project's effects on historic properties (properties listed in or eligible for inclusion in the National Register, consistent with the Section 106 process). The historic resource investigation included a search of previously recorded properties in the OAHP Compass database, intensive-level survey, archival research and preparation of OAHP forms. The study was carried out in accordance with the guidelines of the Colorado Historical Society and OAHP, as published in the *Colorado Cultural Resource Survey Manual* (2007) and consistent with the 2014 "Programmatic Agreement Among the FHWA, Colorado SHPO and CDOT Regarding Compliance with Section 106 of the National Historic Preservation Act."

As outlined in the 2014 Programmatic Agreement, architectural properties officially determined Not Eligible by SHPO remain Not Eligible and do not require further recordation, unless a property was less than 50 years of age at the time of recordation. If a property was less than 50 years old at the time of recordation an OAHP Revisitation Form (1405) is typically required. In the case of linear resources, the overall resource is typically assumed to be eligible for the purposes of a Section 106 evaluation. Discrete segments are then identified, evaluated, and determined to be either Supporting or Not Supporting of the eligibility of the overall resource. If a linear segment was determined Eligible and Supporting more than five years previously, it is typically reassessed.

The methods employed in this investigation are detailed below.

A. File search results

Table 1 lists properties that were recorded in Compass within and adjacent to the APE prior to the commencement of historical investigations for the I-270 Environmental Assessment project. Table 2 lists the previous surveys that have been conducted within and adjacent to the study area. These lists include those properties and surveys located within a 500-foot buffer of the I-270 right-of-way. Figure 23 shows the location of the previously recorded properties.

Table 1. Previously recorded properties in Compass within and adjacent to the APE

Note: GIS information was not provided within the Compass search for properties listed with an asterisk (*) and are not included on Figure 23.

OAHP Resource Number	Resource name	Address	Construction Date	Located in APE	National/State Register Evaluation (and Date of Evaluation) ⁴⁷
5AM.126	Long Expedition	N/A	1820 - Nonextant	No	Field not eligible (1982)
5AM.1286	N/A	6990 York Street, Adams County	1932	No	Officially not eligible (1999)
5AM.1287*	Siegrist Construction Company	6998 York Street, Adams County	1932	No	Officially not eligible (1999)
5AM.1292	Gardner Ditch - Gardener Ditch - Heller Ditch	N/A	1861	Yes	Officially eligible (1999)
5AM.1298.1	Market Street Railroad - Segment	N/A	1911 - 1913	Yes	Officially eligible (2001)
5AM.130	Platte River Trail	N/A	N/A - Nonextant	No	Field not eligible (1982)
5AM.132	Overland Trail And Stagecoach Route	N/A	N/A - Nonextant	No	Field not eligible (1982)
5AM.1321.1	Sand Creek Railroad Junction, Union Pacific Railroad	N/A	1882	Yes	Supports eligibility of entire linear resource (2017)
5AM.1363	Sand Creek Bridge - E-17-AT	Vazquez Boulevard/US 6 over Sand Creek	1940	Yes	Officially not eligible (2002)
5AM.1367	Sand Creek Bridge - E-17-C	Brighton Boulevard/SH 265 over Sand Creek	1949 Nonextant	N/A	Officially not eligible (2002)
5AM.1416	Union Pacific Railroad Bridge - COMC-4A-02-RR	UP Suncor Siding over E. 60 th Avenue and Sand Creek	1938	Yes	Officially not eligible (2002)
5AM.1417	Union Pacific Railroad Bridge - COMC-4A-02-RR	DP/UP over E. 60 th Avenue and Sand Creek	1960	Yes	Officially not eligible (2002)

⁴⁷ Properties determined eligible for or listed in the National Register are also considered eligible for the Colorado State Register of Historic Properties. Officially Eligible/Officially Not Eligible properties have been officially determined by SHPO. Field Eligible/Field Not Eligible properties have not received official concurrence from SHPO. Needs Data properties require further research and have not yet received an official or field determination. Supporting/Not Supporting refers to linear segments in relation to the overall resource and are equivalent to an Eligible/Not Eligible determination for the purposes of Section 106.

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OAH Resource Number	Resource name	Address	Construction Date	Located in APE	National/State Register Evaluation (and Date of Evaluation)⁴⁷
5AM.1418	Burlington Northern Santa Fe Railroad Bridge - COMC-5A-03-RR	CB&Q/BNSF over E. 60 th Avenue and Sand Creek	1970	Yes	Officially not eligible (2002)
5AM.1842	Kammerzell Residence	5360 Magnolia Street, Commerce City	1954	Yes	Officially not eligible (2007)
5AM.1843	The Nationwide Fabrication, Inc.	5311 Niagara Street, Commerce City	1954	Yes	Officially not eligible (2007)
5AM.1844	N/A	5300 Magnolia Street, Commerce City	N/A - Nonextant	N/A	Officially not eligible (2007)
5AM.1845	N/A	5410 Krameria Street, Commerce City	1947	Yes	Officially not eligible (2007)
5AM.1846	N/A	6470 E. 53 rd Way, Commerce City	1954	Yes	Officially not eligible (2007)
5AM.1847	The Barraza Residence	6320 E. 54 th Avenue, Commerce City	1939	Yes	Officially not eligible (2007)
5AM.1848	The Vilchis Residence	6330 E. 54 th Avenue, Commerce City	1944	Yes	Officially not eligible (2007)
5AM.1849	Overnite Transportation Company Business	5200 E. 56 th Avenue, Commerce City	1962	Yes	Officially not eligible (2007)
5AM.1850	The Beco Equipment Co.	5555 Dahlia Street, Commerce City	1960	Yes	Officially not eligible (2007)
5AM.1851	The 3939 Williams Building Corporation Business	5500 Colorado Boulevard, Commerce City	1953	No	Officially not eligible (2007)
5AM.1891	Doyle House - Beals Residence	6820 E 52 nd Place, Commerce City	1954	Yes	Officially not eligible (2007)
5AM.1892	Quintana Residence	6830 E. 52 nd Place, Commerce City	1952	Yes	Officially not eligible (2007)
5AM.1893	N/A	6411 E. 53 rd Way, Commerce City	1953	Yes	Officially not eligible (2007)

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Research Design and Methodology

OAHP Resource Number	Resource name	Address	Construction Date	Located in APE	National/State Register Evaluation (and Date of Evaluation)⁴⁷
5AM.1894	Callahan House - Gonzales Residence	6415 E. 53 rd Way, Commerce City	1954	Yes	Officially not eligible (2007)
5AM.1895	Titsworth House - Boren Residence	6481 E. 53 rd Way, Commerce City	1954	Yes	Officially not eligible (2007)
5AM.1899	Funk House - Klingbell Residence - Klingbell Trucking Co.	5431 Krameria Street, Commerce City	1951	Yes	Officially not eligible (2007)
5AM.1900	Saenz Residence - Martinez Residence	5410 Leyden Street, Commerce City	1935	No	Officially not eligible (2007)
5AM.1901	Gorden Residence	5411 Leyden Street, Commerce City	1942	No	Officially not eligible (2007)
5AM.1902	Van Den Berg Residence - Munoz Residence	5420 Leyden Street, Commerce City	1910	No	Officially not eligible (2007)
5AM.1903	Andrews Residence - Ruiz-Howell Residence	5435 Leyden Street, Commerce City	1939	No	Officially not eligible (2007)
5AM.1904	Johnson Residence	5401 Locust Street, Commerce City	1955	Yes	Officially not eligible (2007)
5AM.1905	Berk Residence	5350 Magnolia Street, Commerce City	1952	Yes	Officially not eligible (2007)
5AM.1906	Morse Residence - Cruz Residence	5366 Magnolia Street, Commerce City	1952	Yes	Officially not eligible (2007)
5AM.1907	Ray Residence - Oleson Residence	5360 Monaco Street, Commerce City	1954	Yes	Officially not eligible (2007)
5AM.1908	Gorden Residence	5331 Niagara Street, Commerce City	1954	No	Officially not eligible (2007)
5AM.1909	Solano Residence	5351 Niagara Street, Commerce City	1956	No	Officially not eligible (2007)
5AM.2410.1	Brighton Boulevard/SH 265 (Segment)	N/A	1891-1898	Yes	Supports eligibility of entire linear resource (2016)

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OAHP Resource Number	Resource name	Address	Construction Date	Located in APE	National/State Register Evaluation (and Date of Evaluation)⁴⁷
5AM.266	N/A	7150 Lafayette Street, Adams County	1880 - Nonextant	N/A	Field not eligible (1983)
5AM.267	N/A	940 E. 73rd Avenue, Adams County	Not Provided - Nonextant	N/A	Officially not eligible (1983)
5AM.268	N/A	901 E. 73rd Avenue, Adams County	Not Provided - Nonextant	N/A	Officially not eligible (1983)
5AM.269*	N/A	7151 Lafayette Street, Adams County	Not Provided - Nonextant	N/A	Officially not eligible (1983)
5AM.270	N/A	1020 E. 71st Avenue, Adams County	Not Provided - Nonextant	N/A	Officially not eligible (1983)
5AM.271*	N/A	7191 Lafayette Street, Adams County	Not Provided - Nonextant	N/A	Officially not eligible (1983)
5AM.272*	N/A	7220 Lafayette Street, Adams County	Not Provided - Nonextant	N/A	Officially not eligible (1983)
5AM.274*	N/A	7171 Lafayette Street, Adams County	Not Provided - Nonextant	N/A	Officially not eligible (1983)
5AM.3244	Yantorno Farm (Centennial Farm) - Center Greenhouse	7220 Lafayette Avenue, Adams County	1889	No	Officially needs data (2014)
5AM.464.16	Burlington Northern and Santa Fe Railroad - Segment	N/A	1882	Yes	Supports eligibility of entire linear resource (2017)
5AM.464.17	Chicago, Burlington & Quincy Railroad - Burlington Northern & Santa Fe Railroad (Segment)	N/A	1882	Yes	Supports eligibility of entire linear resource (2013)
5AM.465.9	Burlington Ditch - O'Brien Canal - Segment	N/A	1886	Yes	Supports eligibility of entire linear resource (2017)
5AM.472.17	Union Pacific Railroad - Dent Branch	N/A	1908-1909	Yes	Supports eligibility of entire linear resource (2017)
5AM.472.21	Kansas Pacific Railroad - Union Pacific Railroad (Segment)	N/A	1867-1870	Yes	Supports eligibility of entire linear resource (2013)

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OAHP Resource Number	Resource name	Address	Construction Date	Located in APE	National/State Register Evaluation (and Date of Evaluation)⁴⁷
5AM.72	N/A	7150 N. Washington Street, Adams County	1900-1914 - Nonextant	N/A	Field not eligible (1980)
5AM.73*	N/A	7151 N. Washington Street, Adams County	1910-1919 - Nonextant	N/A	N/A (1980)
5AM.74	N/A	7190 N. Washington Street, Adams County	1922 - Nonextant	N/A	Field not eligible (1980)
5AM.75*	N/A	7250 N. Washington Street, Adams County	1920-1929 - Nonextant	N/A	Field not eligible (1980)
5AM.76*	N/A	7295 N. Washington Street, Adams County	1920-1929 - Nonextant	N/A	Field not eligible (1980)
5AM.77	Farrel - Larson Farm	7381 N. Washington Street, Adams County	1886	No	Field eligible (1982)
5AM.80.1	Lower Clear Creek Canal (Segment) - Clear Creek Ditch	N/A	1860-1869	No	Supports eligibility of entire linear resource (2009)
5AM.81.1	Colorado Agricultural Canal	N/A	1860-1869	No	Does not support eligibility of entire linear resource (2009)
5DV.711	Denver Municipal Airport - Stapleton International Airport Site	E. 32nd Avenue & Quebec, Denver	1920-1929 - Nonextant	No	Officially not eligible (2017)
5DV.8192	Central Park Blvd./I-70 Interchange	N/A	Unknown	No	Officially not eligible (2009)

Table 2. Previous surveys

Survey No.	Project name	Date
AM.AE.R9	Cultural Resource Survey of The Cherokee Diversion Project, Commerce City, Adams County, Colorado	07/30/2010
AM.CH.R40	Inventory and Evaluation of Historic Resources State Highway 270 Extension. (IM 2706-030)	05/28/2002
AM.CH.R43	Project I 76-1 (68) Us 36-I 270 Connection	03/06/2002
AM.CH.R44	Project M 7731(1), Washington Street, SH 224, North To 83rd Avenue, Cultural Resources Report for Historical Resources	02/10/2009
DV.CH.NR13	Quebec Street Improvements Environmental Review: Results of Cultural Resource Investigations	08/26/2003
DV.CH.NR24	Paleontological Technical Report: 47th Street Bridge Replacement Project, City and County of Denver, Colorado	06/03/2009
DV.CH.R4	North I-25 Corridor	12/10/1986
MC.CH.NR61	Archaeological Survey of Highway Project I-70-4(72) Denver & Adams County Quebec St to I-270	09/27/2006
MC.CH.NR63	Archaeological Survey of IR 025-2(198), Denver And Adams Counties	01/26/2007
MC.CH.R187	I-70 East Cultural Resources Survey Report	03/15/2010
MC.CH.R96	A Cultural Resource Survey of Interstates 25, 70, 225, And 270, U.S. Highways 34 And 160, And State Highways 13 And 470, for the Proposed Adesta Communications Fiber Optic System, Colorado (C Sw00-102)	03/17/2008
MC.FC.R1	Cultural Resources Inventory for The MFS Network Technologies' Denver Metro Backbone Routes 1 And 1a, Denver And Adams Counties, Colorado (SWCA No. 99-310)	03/18/2008

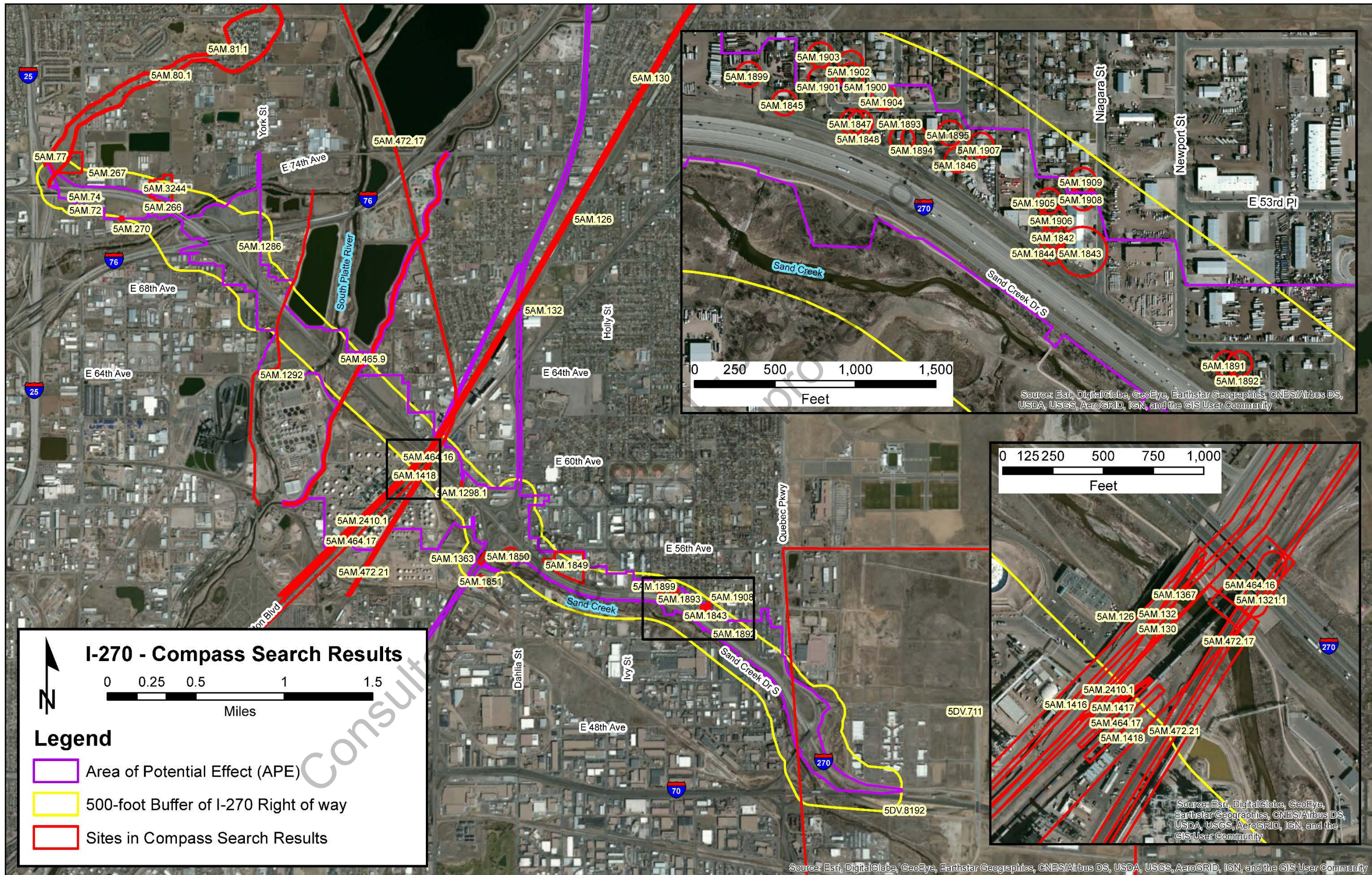


Figure 23. Previously recorded resources within a 500-foot buffer of the I-270 right-of-way.

B. Field inventory methods

For the initial *Selective Historic Resources Inventory Report* Dianna Litvak, senior historian, and Ethan Raath, historian, conducted field survey on August 8-9, 2019. Ethan Raath and Katherine Oldberg, historian, conducted the second phase of field survey on July 1-2 and July 17, 2020. All historians meet the Secretary of the Interior Standards for history and architectural history. Architectural properties were photographed from the right-of-way. For linear resources, photographs were taken between the start and end points of the linear resource segments and any significant features were noted. Right-of-entry was granted by the Farmer's Reservoir and Irrigation Company (FRICO) and Metro Wastewater to survey segments of the Burlington Ditch/O'Brien Canal and the Gardener's Ditch. Photographs were taken using a digital SLR camera and an iPad tablet. Field notes were recorded by hand and electronically on an iPad tablet. Aerial drone photos were taken of selected properties by CDOT on October 3, 2019, and July 17, 2020.

The boundaries of all 40 individual architectural properties were determined to be consistent with the legal lot for each property. New segments were determined for 10 of the 11 linear resources. New linear segments were identified in previously unevaluated locations based on proximity to the study area and condition relative to the overall resource. Where previously evaluated segments intersected the APE, these segments were evaluated based on their length in relation to the study area and changes in condition relative to the overall resource. Segments that are too short do not cover the extent of the APE. If a segment is too long, the potential effects from this project would only impact a small portion of that segment. In some cases, the condition of the resource within the APE no longer reflects the overall condition of the previously recorded segment. In most cases new segments were identified to accurately convey the potential for effects from the project on the overall linear resource. The boundaries of all linear segments were established by Mead & Hunt with CDOT concurrence.

C. Archival research methods

During the initial phase of field survey and historic context development in 2019, previously recorded historical information about the study area and about specific historical resources was collected from the OAHP, including existing site forms and survey reports. Further research was conducted at the Denver Public Library (DPL). Resources consulted at the DPL and the History Colorado Stephen H. Hart Research Center included local community histories, historic maps, city directories, historic newspapers, and other published resources. The Adams County Historical Society and Commerce City Historical Society were contacted regarding pre-World War II industrial history in the Commerce City area. Web-based resources were also consulted, including technical reports, articles, historic mapping, and historic photographs. In addition, web searches revealed additional sources specific to individual surveyed resources, such as company histories, and these are listed on the source field in the OAHP inventory forms.

During the second phase of field survey in 2020, research efforts were largely limited to online research due to the stay-at-home orders put in place in response to the COVID-19 pandemic. The Denver Public Library, Adams County Historical Society, and Commerce City Historical society were closed to in-person research. Historical information about the study area and about specific historical properties within the study area was collected utilizing a variety of web-based resources. Property information, including construction dates and ownership records, was collected from the Adams County Assessor and Adams

County Clerk and Recorder's online database. Information regarding the historic owners and residents of surveyed resources was gathered from public records available from Ancestry, digitized historic newspapers available from the Colorado Historic Newspapers Collection, Genealogy Bank, and Newspapers.com. Historic aerial photos were accessed from the USGS Earth Explorer online database and historic maps were accessed from the USGS Topo View database and the Denver Public Library Digital Collections.

Additional records not available online, including building permits and assessor cards, were collected from the Adams County Assessor, Adams County Office of Community and Economic Development, and the City of Commerce City. These records were not available for every property and specific limitations are discussed within the individual OAHP inventory forms (see Appendix D). Additional research was conducted at the History Colorado Stephen H. Hart Research Center specifically focused on the history of prominent members of the Welby Italian community in the records of the Assumption of the Blessed Virgin Mary Catholic Church. Other sources consulted throughout both phases of field survey and research included secondary literature on the local history of Denver, Commerce City, Adams County, and Colorado. Specific areas of research among secondary sources included the history of the Italian community in Welby; railroad, state highway, and interstate development in Colorado; the history of oil production in Colorado; early community development and industries in the Commerce City area; and post-World War II residential, commercial, and industrial development in Adams County. All sources are listed in the attached bibliography and the source field in the OAHP inventory forms (see Appendix D).

D. Evaluation of significance

All historic resources identified with the APE were evaluated for eligibility for inclusion in the National Register.⁴⁸ The National Register was established in 1966 as part of the National Historic Preservation Act and is administered by the National Park Service. The criteria properties must meet to be eligible for listing on the National Register are specified in the Code of Federal Regulations, Title 36, Part 60, which reads, in part:

The quality of significance in American history, architecture, archeology, engineering, and culture is present in districts, sites, buildings, structures, and objects that possess integrity of location, design, setting, materials, workmanship, feeling, and association, and:

- A. That are associated with events that have made a significant contribution to the broad patterns of our history; or
- B. That are associated with the lives of persons significant in our past; or
- C. That embody the distinctive characteristics of a type, period, or method of construction, or that represent the work of a master, or that possess high artistic values, or that represent a significant and distinguishable entity whose components may lack individual distinction; or
- D. That has yielded, or may be likely to yield, information important in prehistory or history.

To be eligible for inclusion in the National Register a resource must exhibit sufficient physical integrity to convey its significance, in addition to being associated with one or more of the National Register criteria

⁴⁸ Exceptions to this are properties previously determined Officially Not Eligible and were more than 50 years of age at the initial time of determination, and properties associated with I-270 which are subject to the 2005 Advisory Council on Historic Preservation (ACHP) Interstate Highway exemption.

listed above. All cultural resources identified within the study area were evaluated for the seven aspects of integrity as outlined specifically in the federal regulations, which are:

- **Location:** The place where the historic property was constructed or the historic even occurred.
- **Design:** The combination of elements that create the form, plan, space, structure, and style of a property.
- **Setting:** The physical environment of a historic property.
- **Materials:** The physical elements that were combined or deposited during a particular period of time and in a particular pattern or configuration to form a historic property.
- **Workmanship:** The physical evidence of the crafts of a particular culture or people during any given period in history or prehistory.
- **Feeling:** A property's expression of the aesthetic or historic sense of a particular period of time.
- **Association:** The direct link between an important historic event or a person and a historic property.

E. Inventory form preparation

The study area consists of pre-World War II residential properties; isolated postwar residential properties; a postwar residential subdivision; pre- and postwar commercial properties; multiple linear resources including irrigation ditches, railroads, roads and State Highways; a railroad junction; and an isolated bridge.

Forty-one newly recorded individual architectural properties, representing the pre- and postwar eras, were recorded individually on Architectural Inventory (1403) forms, as was one isolated bridge. Two individual architectural properties that were less than 50 years old at the time of their previous evaluation were recorded on Cultural Resource Re-Visitation (1405) forms (5AM.1849 and 5AM.1850). One postwar residential subdivision, Larusso Subdivision, was surveyed and evaluated according to the boundaries of the original plat, including any pre-World War II residential properties, and was recorded on a Post-World War II Residential Suburban Subdivision (1403b) form.

In addition, 11 newly recorded linear properties were evaluated and recorded on Management Data (1400), and Linear Component (1418) forms. One linear property was recorded on a Cultural Resource Re-Visitation (1405) form. The boundaries and segment lengths of linear properties were determined during the APE development process. One railroad junction was recorded on Management Data (1400) and an Archaeology Component (1402) forms

Boundaries, narrative descriptions, historic archival material, and significance assessments for all properties within the APE are included in the OAHP forms and in Section 4 of this report. Forms also include U.S. Geological Survey (USGS) topographic maps, site sketch maps (prepared in ArcView), and digital photographs. The forms are included as Appendix D (submitted separately with this report).

4. Results of Research, Survey, and Evaluation

This section provides an overview of the results of the historical survey for the I-270 EA project. It includes the 41 new and revisited non-linear properties and the 11 new and revisited linear segments recorded by Mead & Hunt and Kristi Miniello in 2019 and 2020. Table 3 includes the site number and name, locational information, date of construction, determinations for individual eligibility and district status, and which OAHP inventory form was prepared. Brief summaries including property descriptions, National Register eligibility assessments, and assessments of integrity (if applicable) are provided for each property following Table 3.

Consultant Work Product - Jacobs Engineering
-Not CDOT Approved-

Table 3. Historic resources inventory results and National Register determinations⁴⁹

Site No.	Site Name	Address	Construction Date	National Register Determination (2020)	OAHP Form
Architectural Properties					
5AM.1849	Overnite Transportation Company Business	5200 E. 56 th Avenue, Denver	1962	Not eligible	OAHP1405
5AM.1850	The Beco Equipment Co.	5555 Dahlia Street, Commerce City	1962	Not eligible	OAHP1405
5AM.4031	Larusso Subdivision	E. 68 th Place to the north, I-270 to the east, E. 68 th Avenue to the south, and the back edge of the properties fronting Race Street to the west. Adams County	1942-1970	Not eligible	OAHP1403b
5AM.4032	Sherwood House	2101 E. 68 th Avenue, Adams County	1944	Not eligible	OAHP1403
5AM.4034	Teegee & Company, LLC	2180 E. 68 th Avenue, Adams County	1922	Not eligible	OAHP1403
5AM.4035	Di Giacomo-Slagel House	6820 York Street, Adams County	c.1955	Not eligible	OAHP1403
5AM.4036	King House	6776 York Street, Adams County	c.1905	Not eligible	OAHP1403
5AM.4037	King House	6780 York Street, Adams County	c.1900	Not eligible	OAHP1403
5AM.4038	Weland House	6750 York Street, Adams County	c.1925	Not eligible	OAHP1403
5AM.4039	Chapman-Depinto House	6702 York Street, Adams County	1963	Not eligible	OAHP1403
5AM.4040	Royston House	6700 York Street, Adams County	1955	Not eligible	OAHP1403
5AM.4041	NA	6690 York Street, Adams County	1953	Not eligible	OAHP1403
5AM.4042	Chaplinskiy House	6610 York Street, Adams County	c.1910	Not eligible	OAHP1403
5AM.4043	Kuhns House	6620 York Street, Adams County	1905	Not eligible	OAHP1403
5AM.4044	Suncor Energy	3875 E. 56 th Avenue 5800 Brighton Road 5801 Brighton Boulevard, Commerce City	1930, c.1937, 1949, 1953, c.1978	Needs data/Treat as eligible	OAHP1403
5AM.4045	Denver Oil	4100 E. 60 th Avenue, Commerce City	1922	Not eligible	OAHP1403

⁴⁹ All National Register recommendations refer to individual eligibility. No National Register-eligible historic districts were identified as part of this survey.

Section 4
Results of Research, Survey, and Evaluation

Site No.	Site Name	Address	Construction Date	National Register Determination (2020)	OAHP Form
5AM.4046	Domenico Transportation Services Building	5699 Dexter Street, Commerce City	1964	Not eligible	OAHP1403
5AM.4047	Plastics, Inc.	5701 Dexter Street, Commerce City	c.1947	Eligible	OAHP1403
5AM.4048	Marquez House	6860 E. 52nd Place, Commerce City	1966	Not eligible	OAHP1403
5AM.4049	Duran House	5251 Oneida Street, Commerce City	1955	Not eligible	OAHP1403
5AM.4050	Applegarth House	5250 Oneida Street, Commerce City	1955	Not eligible	OAHP1403
5AM.4070	Corner Stone Truck Repair	5790 Dahlia Street, Commerce City	c.1966	Not eligible	OAHP1403
5AM.4107	NA	7141 Lafayette Street, Adams County	1976	Not eligible	OAHP1403
5AM.4108	NA	6875 Columbine Street, Adams County	1950-1959	Not eligible	OAHP1403
5AM.4109	Martinez House	6824 Columbine Street, Adams County	1937	Not eligible	OAHP1403
5AM.4110	Semmaterials Energy Partners, LLC.	4310 E 60th Avenue, Commerce City	1953	Not eligible	OAHP1403
5AM.4111	United Asphalts, Inc.	4306 E 60th Avenue, Commerce City	1965	Not eligible	OAHP1403
5AM.4112	Stewart & Stevenson Power Products	5820 Dahlia Street, Commerce City	1946	Not eligible	OAHP1403
5AM.4113	Western Material Handling	5678 Eudora Street, Commerce City	c.1960	Not eligible	OAHP1403
5AM.4114	Wagner Rents: The CAT Rental Store	5455 Holly Street, Commerce City	1973	Not eligible	OAHP1403
5AM.4115	E&K Trucking, Inc.	5421 Krameria Street, Commerce City	1975	Not eligible	OAHP1403
5AM.4116	Masek Golf Cars	5345 Newport Street, Commerce City	1973	Not eligible	OAHP1403

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Site No.	Site Name	Address	Construction Date	National Register Determination (2020)	OAHP Form
5AM.4117	Denver Truck & Trailer	5280 Newport Street, Commerce City	1974	Not eligible	OAHP1403
5AM.4118	E. 64 th Avenue Bridge over Burlington Ditch/O'Brien Canal	N/A	c.1910-1930	Not eligible	OAHP1403
5AM.4122	Keller	5665 Eudora Street 5775 Eudora Street 5675 Eudora Street Commerce City	1967	Not eligible	OAHP1403
5AM.4144	NA	5450 Krameria Street, Commerce City	1960	Not eligible	OAHP1403
5AM.4145	Hernandez House	5400 Locust Street, Commerce City	1954	Not eligible	OAHP1403
5AM.4146	Colombo House	5401 Magnolia Street, Commerce City	c.1935	Not eligible	OAHP1403
5AM.4147	NA	5358 Magnolia Street, Commerce City		Not eligible	OAHP1403
5AM.4148	Duran House	6870 E. 52 nd Place, Commerce City	1955	Not eligible	OAHP1403
Linear Properties					
5AM.464.21	Chicago Burlington & Quincy/Burlington Northern Santa Fe Railroad Segment—Brush Line	Between E. 56 th Avenue and I-270 in Commerce City	1881-1882	Eligible – Supporting segment; Assume overall resource eligible	OAHP1400 and OAHP1418
5AM.465.9	Burlington Ditch/O'Brien Canal	Between South Platte River near Franklin Street and E. 74 th Avenue in Commerce City	1886	Eligible – Supporting segment; Overall resource officially eligible	OAHP1405
5AM.472.41	Denver Pacific/Union Pacific Railroad Segment—Greeley Line	Between E. 56 th Avenue and I-270 in Commerce City	1870	Eligible – Supporting segment; Assume overall resource eligible	OAHP1400 and OAHP1418
5AM.1292.2	Gardener's Ditch Segment	Between 6610 York Street and 6750 York Street in Adams County	1861	Eligible – Non-supporting segment; Overall resource officially eligible	OAHP1400 and OAHP1418

Section 4
Results of Research, Survey, and Evaluation

Site No.	Site Name	Address	Construction Date	National Register Determination (2020)	OAHP Form
5AM.1298.3	Chicago, Burlington & Quincy Market Street Line Segment	Between E. 56 th Avenue and E. 60 th Avenue in Commerce City	1911-1913	Eligible – Supporting segment; Assume overall resource eligible	OAHP1400 and OAHP1418
5AM.2410.2	Brighton Boulevard/SH 265	Approx. 441 feet SW from the I-270 overpass ending at the junction with US 6/US 85 in Commerce City	1880s	Eligible – Supporting segment; Assume overall resource eligible	OAHP1400 and OAHP1418
5AM.3924.2	Vasquez Boulevard/U.S. Highway 6/U.S. Highway 85/State Highway 2	Between E. 56 th Avenue and I-270 interchange in Commerce City	1932-1938	Eligible – Non-supporting segment; Assume overall resource eligible	OAHP1400 and OAHP1418
5AM.3924.3	U.S. Highway 6 (Vasquez Boulevard, CDOT Route 006H)	Between I-270 interchange and I-76 in Commerce City	1932-1938	Eligible – Non-supporting segment; Assume overall resource eligible	OAHP 1400 and OAHP 1418
5AM.4101.2	York Street	York Street between E. 66 th Avenue and E. 74 th Avenue in Adams County	1880s	Eligible – Non-supporting segment; Assume overall resource eligible	OAHP1400 and OAHP1418
5AM.4119	Sand Creek Junction	Crossing point of CB&Q/BNSF Brush Line and DP/UP Greeley Line in Commerce City	1882	Eligible	OAHP1400 and OAHP1402
5AM.4121.1	Union Pacific Railroad Siding at Suncor Energy	Between E. 56 th Avenue and I-270 in Commerce City	c.1930s-1940s	Not eligible – Non-supporting segment	OAHP1400 and OAHP1418

A. Individual architectural properties

- (1) **5AM.1849 – The Overnite Transportation Company/UPS Freight (5200 E. 56th Avenue, map label 43)**

Description

This one-story, L-shaped office, garage, and loading dock has a gable roof. Windows are original metal fixed windows and walls are brick within the office portion of the building and corrugated metal for the garage and loading dock portions. The building contains multiple garage bays and loading docks on the east and west elevations. To the west of the building sits a detached rectangular garage with corrugated metal walls and a gable roof. The buildings are located on a paved asphalt lot. Commercial properties surround the parcel. I-270 runs along the southern perimeter of the parcel and E. 56th Avenue runs along the northern perimeter of the parcel.



Figure 24. 5200 E. 56th Avenue facade (north) and west elevation, view facing southeast.

Historical summary

The current office, garage, and loading dock building and the detached garage were built in 1962. The loading dock extending to the east from the main building was built between 1971 and 1991. The original owner of this property is unknown. The Overnite Transportation Company has owned this property since at least 2004.

Significance assessment

This property was determined officially not eligible in 2007. Current assessment and revisitation of the property concurs with this finding.

(2) 5AM.1850 – Beco Equipment Co. (5555 Dahlia Street, map label 38)

Description

This two-story, irregular plan office and garage has a flat roof. Windows are original metal multi-light windows and walls are concrete block and brick. The building contains multiple garage bays on the rear elevation (southeast). An addition was built on the facade (northeast) sometime after 1960. To the west of the building sits a concrete block flat roof shed. A large, irregular plan garage and warehouse was built on the property in 1985. The cloverleaf interchange at Interstate-270 (I-270) and Vasquez Boulevard sits along the northern perimeter of the parcel, Vasquez Boulevard to the west, Sand Creek and the Sand Creek Regional Greenway to the south, and I-270 to the east.



Figure 25. 5555 Dahlia Street, view facing southwest.

Historical summary

The current office and garage building on this property was built in 1960. The original owner of this property is unknown. The Beco Equipment and Construction business has owned this property since 1980. The large, irregular-plan garage and warehouse building was built in 1985.

Significance assessment

This property was determined officially not eligible in 2007. Current assessment and revisitation of the property concurs with this finding.

(3) 5AM.4032 – Sherwood House (2101 E. 68th Avenue, map label 3)

Description

This one-story, rectangular-plan house reflects limited Tudor Revival elements such as the high-pitched, side-gable roof with projecting gable entrance, arched picture window, and prominent chimney placed asymmetrically on the facade. The walls are clad in brick-veneer with contrasting checker-board decorative brickwork and the cross-gable roof is covered with asphalt shingle. A large raised concrete patio with a decorative iron railing provides the main entry. This property also includes a side-gable barn (c.1950), a three-bay garage (c.1980), a two-bay garage (2010), and a c.2015 barrel-roof shed.



Figure 26. Sherwood House, front (south) facade and side (east) elevation. View facing north/northwest.

Historical summary

This house was built in 1944. In 1946 Sam and Anna Marie Larusso purchased the house and the surrounding 10 acres of rural land from Rosa, Rose, Tony, and Violet Perisichitte in 1946. In 1954 the Larussos subdivided the property to form the Larusso Subdivision, carving out a larger lot around their residence.

Significance assessment

This property does not have an association with significant agricultural development in the greater Welby area or with any significant events, patterns, or trends associated with residential development in Adams County and does not possess significance under Criterion A. The property owners are not considered persons of significance under Criterion B. The house has minimal architectural details related to the Tudor Revival style but overall, it is a modest representation that does not embody all of the distinctive characteristics of that style. In addition, the historic multi-light windows and front brick knee-wall are no longer intact. Therefore, the house does not possess architectural or design significance and is unable to convey significance as a farmhouse under Criterion C. This property is not likely to contain information

important to history or prehistory beyond what is already documented and does not possess significance under Criterion D. Due to the lack of significance, this building is not eligible for inclusion in the National Register.

(4) 5AM.4034 – Teegee & Company, LLC (2180 E. 68th Avenue, map label 4)

Description

This one-story, square-plan Bungalow has an asphalt-shingle-covered pyramidal hip roof with wide eaves, exposed rafter tails, and an interior brick chimney. The symmetric front (east) facade has a central recessed porch that is enclosed with plate glass in metal frames. Walls are clad in brick veneer and feature contrasting columns of red and brown bricks placed around the windows and a decorative belt course at sill level. The building has been repurposed for commercial use and has an associated parking lot and modern garage.



Figure 27. Office and garage of Teegee & Company, view facing east.

Historical summary

This house was constructed in 1922 as a farmhouse. Nicolo and Lucia Domenico purchased a larger property in 1918 and it is likely the Domenico family owned this property at the time of its construction. It remained in the ownership of the Domenico family until 2000.

Significance assessment

This property does not have a direct association with significant activities or events associated with agricultural development in the Welby area under Criterion A. This property appears to have remained in the Domenico family since it was constructed in 1922 until 2000, but the Domenico family was not found to be a historically significant family within the Italian community of Welby under Criterion B. Although the building has minimal architectural details related to the Bungalow type, including exposed rafters and brick construction it can no longer convey its historic agricultural associations. The building and property have been significantly altered for commercial use, including alterations to the porch and, most importantly, the construction of a parking lot and large garage on the property. Therefore, the building does not possess architectural or design significance under Criterion C. The building is not likely

to contain information important to history or prehistory beyond what is already documented and does not possess significance under Criterion D. Due to lack of significance, this building is not eligible for inclusion in the National Register.

(5) 5AM.4035 – Di Giacomo-Slagel House (6820 York Street, map label 8)

Description

This one-story, rectangular-plan Ranch house has a hip roof with wide eaves, a hip projection on the front (west) facade, and a shed-roof entry porch with decorative wrought-iron scroll posts; the entire roof is covered with asphalt shingles. Fenestration includes the main entrance, a single wood slab entry door with a metal security door, and original, aluminum, fixed and one-by-one, sliding sash windows with brick rowlock sills. The property also includes two small sheds (c.1990).



Figure 28. Di Giacomo-Slagel House, front (west) facade and side (south) elevations. View facing east/northeast.

Historical summary

Although the Adams County GIS Website Building Summary shows this house as constructed in 1926, the form, massing, materials, and design all suggest it was constructed c.1955. Adams County property records are not available prior to 2003 and the original owner is unknown. The present owners, Sylvester and Roxanne Di Giacomo, have owned the property since 2006, and Donna Slagel became a part owner in 2009.

Significance assessment

This property does not have significant association with events associated with residential development in the southwest Adams County area or prominent Italian families in the Welby area under *Criteria A* or *Criteria B*. This house is a modest representation of the Ranch style with its single-story, rectangular-plan, brick construction, and projecting eaves and is not associated with any subdivision or development plan. As such, the house does not possess architectural or design significance under *Criterion C*. The house is not likely to contain information important to history or prehistory beyond what is already documented and does not possess significance under Criterion D. Due to lack of significance, this building is found not eligible for inclusion in the National Register.

(6) 5AM.4036 – King House (6776 York Street, map label 9)

Description

This one-story, square-plan building has a pyramidal roof with overhanging eaves, asphalt shingles, and a central brick chimney. Walls are clad in asphalt paper patterned to simulate brick, and windows are boarded. The main entrance is the original wood panel door with a c.1980 aluminum storm. It is located on an irregular lot with a secondary pyramidal-roof building (c.1910) and a modern communications tower. This property is historically associated with the 6780 York Street property to the south.



Figure 29. King House at 6776 York Street, front (west) facade and side (south) elevations. View facing east/northeast.

Historical summary

This building was constructed c.1905. Adams County property records are not available prior to 1955, but in that year Amelia and William Condon purchased the property from Antonio, Carmen, and Anthony Delusio. In 1980 Charles and Carol King purchased this property from John Condon, then subdivided the land, to be known as King Subdivision (later amended to King Subdivision No. 2), and this building is on lot 1 of the subdivision.

Significance assessment

This property does not have a direct association with significant activities, events, or significant Italian families associated with agricultural development in the Welby area. As such, this building does not possess significance under *Criteria A* or *B*. The house, with a square form and pyramidal hip roof, reflects a modest vernacular form and does not embody the defining characteristics of a particular style, period, or method of construction under *Criterion C*. The property is not likely to contain information important to

history or prehistory beyond what is already documented and does not possess significance under *Criterion D*. Due to lack of significance, this property is not eligible for inclusion in the National Register.

(7) 5AM.4037 – King House (6780 York Street, map label 10)

Description

This one-and-one-half-story, rectangular-plan, frame house has a hip roof with wide eaves and hip dormers on the south, east, and west elevations, all covered with asphalt shingles. Walls are clad in original horizontal wood and replacement vinyl siding with wood fish scale shingles on the dormers. Fenestration consists of replacement, metal, one-over-one, sash windows and a replacement metal with multi-light main entrance door. A detached garage (c.1920) is located to the east of the house.

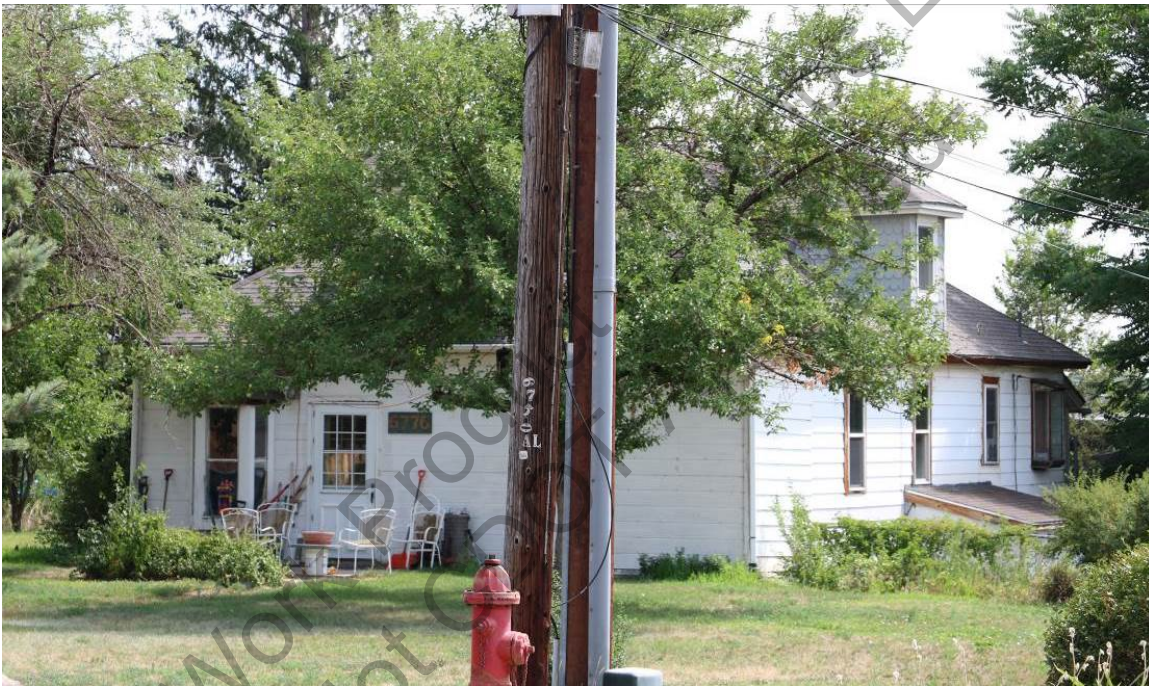


Figure 30. King House at 6780 York Street, front (west) facade and side (south) elevations. View facing east/northeast.

Historical summary

This house was constructed c.1900 and the detached garage c.1920. This property is historically associated with the parcel to the north, 6776 York Street, which is also owned by Charles and Carol King. Adams County property records for past owners are not available for this property.

Significance assessment

This property does not have a direct association with significant activities, events, or persons associated with agricultural development or prominent Italian families in the Welby area under *Criteria A* or *Criteria B*. The house represents a modest example of the cottage style but does not fully embody the distinctive characteristics of that style due to a lack of exterior features including a front porch, columns, or decorative eaves and does not possess architectural or design significance under *Criterion C*. The house is not likely to contain information important to history or prehistory beyond what is already documented

and does not possess significance under *Criterion D*. Due to lack of significance, this building is not eligible for inclusion in the National Register.

(8) 5AM.4038 – Weland House (6750 York Street, map label 11)

Description

This side passage commercial building, formerly a house, has a hip roof with wide eaves. The original hip roof entry porch has been enclosed, and the remodeled main entry has a metal with glazing replacement door. The walls are clad in stucco and windows are generally one-by-one sliding sash. It is located on a large lot with a concrete parking lot, a front-gable metal shed, a small concrete block building, a large front-gable metal garage, and mobile home (c.1960).



Figure 31. Weland House, front (west) facade and side (north) elevations. View facing east/southeast.

Historical summary

This building was constructed c.1925. Adams County property records are not available prior to the 1990s. Aerial imagery from the 1950s confirms that there was agricultural production on this property at that time, but the associated farm fields are now covered by Cat Lake to the east. In 2015 the current owner, Todd Weland, was granted the property from the Erich Weland trust.

Significance assessment

This modified house does not have a direct association with significant activities or events associated with agricultural development or significant Italian families in the Welby. As such, this building does not possess significance under *Criteria A* or *Criteria B*. The building has been significantly altered to function as a commercial office and any historic architectural details are no longer present and it no longer embodies the distinctive characteristics of a specific type, period, or method of construction. Therefore, the building does not possess architectural or design significance under *Criterion C*. The building is not likely to contain information important to history or prehistory beyond what is already documented and does not possess significance under *Criterion D*. Due to lack of significance, this building is not eligible for inclusion in the National Register.

(9) 5AM.4039 – Chapman-Depinto House (6702 York Street, map label 12)

Description

This one-story, rectangular-plan Ranch house has a side-gable asphalt-shingled roof with wide eaves and asphalt shingles and a decorative gable over a picture window and main entry. It also has a two-bay attached garage on the north elevation. The exterior walls are brick and fenestration includes a glazed wood panel main entry door, a wood panel with secondary entrance by the garage, and replacement vinyl windows. The lot includes an additional detached garage and two front-gable sheds (c.1990).



Figure 32. Chapman-Depinto House, front (west) facade and side (north) elevation. View facing east/southeast.

Historical summary

This house was constructed in 1963 and the original owners were Joseph V. and Catherine M. De Pinto. There are no available records on ownership prior to 2004. In 2004 Joseph Depinto transferred the property into joint ownership, with himself and Renee Chapman as grantees. In 2015 ownership was granted to Renee Chapman and Victor and Rick Depinto.

Significance assessment

This Ranch house is not associated with a particular subdivision and does not have association with significant activities or events associated with residential development in the southwest Adams County area and is not significant under *Criterion A*. Because the Depinto family was not found to be a significant Italian family in the Welby area the property does not have significance under *Criterion B*. Although the building has minimal architectural details related to the Ranch style, including single-story construction with overhanging eaves and an attached garage, it is a modest representation of this style that does not embody the distinctive characteristics of that type. Therefore, the house does not possess architectural or design significance under *Criterion C*. The house is not likely to contain information important to history or prehistory beyond what is already documented and does not possess significance under *Criterion D*. Due to lack of significance, this building is not eligible for inclusion in the National Register.

(10) 5AM.4040 – Royston House (6700 York Street, map label 13)

Description

This Ranch house has a hip roof with boxed eaves and a front porch with iron posts and brick planters. The walls are clad in yellow brick with contrasting red brick at the raised basement level. Fenestration includes a replacement metal door and original aluminum fixed and casement windows placed directly beneath the eaves. The lot includes and detached concrete-block garage.



Figure 33. Royston House, front (west) facade and side (south) elevation. View facing east/northeast.

Historical summary

This house was constructed in 1955 and was purchased by the original owners, Leonard and Violet Serravo, that same year. In 2005 Violet Serravo granted the property to Eric Royston.

Significance assessment

This property does not have significant association with agricultural development in the Welby area or significant activities and events associated with residential development in southwest Adams County and is not significant under *Criterion A*. The Serravo family was not found to be a significant Italian family in the Welby community under *Criterion B*. Although the building has minimal architectural details related to the Ranch style, including a single story with a hip roof and boxed eaves, it is overall a modest representation of this style and does not embody the more distinctive characteristics of that style or its period of construction. In addition, the front porch was modified at an unknown date. Therefore, the house does not possess architectural or design significance under *Criterion C*. The house is not likely to contain information important to history or prehistory beyond what is already documented and does not possess significance under *Criterion D*. Due to lack of significance, this building is not eligible for inclusion in the National Register.

(11) 5AM.4041 – 6690 York Street (map label 14)

Description

This Ranch house has a hip roof with wide boxed eaves and a shed-roof porch with angled pole supports and incorporated planters. The walls are clad with brick and fenestration includes a wood slab door and aluminum picture and one-by-one sliding sash windows. There is a large addition to the rear connected to a detached frame garage via a narrow breezeway. In addition to the house and garage the lot includes a side-gable shed.



Figure 34. House at 6690 York Street, front (west) facade and side (south) elevation. View facing east/northeast.

Historical summary

This residence was constructed in 1953. The original owners are unknown, but Adams County Assessor records indicate the property was owned by Phil Manie in 1975. The Metro Wastewater Reclamation District purchased the property in 2018.

Significance assessment

This property does not have significant association with agricultural development in the Welby area or significant activities and events associated with residential development in southwest Adams County and is not significant under *Criterion A*. It also does not have direct association with significant persons under *Criterion B*. Although the house has minimal architectural features including a single-story plan with a hip roof, brick construction, and windows located near the roofline, this house does not embody the defining characteristics of a postwar Ranch style house. In addition, the front porch was modified at an unknown date. Therefore, the house does not possess architectural or design significance under *Criterion C*. The house is not likely to contain information important to history or prehistory beyond what is already documented and does not possess significance under *Criterion D*. Due to lack of significance, this building is not eligible for inclusion in the National Register.

(12) 5AM.4042 – Chaplinskiy House (6610 York Street, map label 16)

Description

This central passage vernacular house faces away from the street and the facade (north) is not visible from the right-of-way. It has a front-gable roof with narrow, flared, boxed eaves. The walls have replacement cladding including stucco and vinyl siding in the gable ends, and the windows are vinyl, one-by-one, sliding sash replacements. The lot also includes a workshop with a clipped-gable roof and a detached garage.



Figure 35. Chaplinskiy House, rear (west) and side (north) elevations. View facing east/southeast.

Historical summary

This house was constructed c.1910 as a farmhouse and the original lot totaled five acres between York Street and the South Platte River. The original owners are unknown, but Adams County property records show that the James family owned and lived on this property in the 1950s. In 1999 Valentina Chaplinskaya granted the property to current owners Anatoly and Marina Chaplinskiy.

Significance assessment

Although this property is an early and relatively intact example of truck farming in this area, research and field review did not reveal a direct association with significant activities, events, or prominent families associated with agricultural development in the Welby area under *Criteria A* or *Criteria B*. The building represents a vernacular form with minimal architectural details and does not embody the essential characteristics of a specific type, period, or method of construction. The house has been heavily modified including relocation of the main entrance and removal of historic wood windows and siding. As such, it does not possess architectural or design significance under *Criterion C*. The house is not likely to contain information important to history or prehistory beyond what is already documented and does not possess

significance under *Criterion D*. Due to lack of significance, this building is not eligible for inclusion in the National Register.

(13) 5AM.4043 – Kuhns House (6620 York Street, map label 15)

Description

This central passage vernacular house has a front-gable roof with flared boxed eaves and a ridgeline brick chimney, and a hip-roof entry porch with square wood posts. The walls are clad in asbestos shingle and windows are one-over-one sash. There is a large addition on the rear elevation. The lot also includes and detached concrete-block garage.



Figure 36. Kuhns House, front (west) facade. View facing east.

Historical summary

This house was constructed in 1905 as a farmhouse and the original lot totaled five acres between York Street and the South Platte River. The original owners are unknown, but Adams County property records indicate that the Josephine and Pasqual Serravo purchased the property from James and Catherine in 1946. In 1971 Anthony and Leonard Serravo, Marie Pheil, and Mildred Cassai granted this property to the current owners, Larry and Marianne Kuhns.

Significance assessment

Although this property is an early and relatively intact example of truck farming in this area, research and field review did not reveal a direct association with significant activities, events, or prominent families associated with agricultural development in the Welby area under *Criteria A* or *Criteria B*. This house has minimal architectural details and does not embody the characteristics of a particular type, period, or method of construction. Further, its historic materials have been removed or covered by modern siding. Therefore, it does not possess architectural or design significance under *Criterion C*. The house is not likely to contain information important to history or prehistory beyond what is already documented and

does not possess significance under *Criterion D*. Due to lack of significance, this building is not eligible for inclusion in the National Register.

(14) 5AM.4044 – Suncor Energy (3875 E. 56th Avenue, 5800 Brighton Road, 5801 Brighton Boulevard, map label 20)

Description

Access to Suncor Energy was not permitted for field survey and therefore the architectural description is largely based on current and historic aerial imagery. Suncor Energy is a petroleum refining facility spread out over three parcels just south of Sand Creek, bisected by Brighton Boulevard, the BNSF Railroad, and the UP Railroad. All three parcels are dominated by catalytic cracking facilities, which are characterized by metal towers and chimneys several stories tall, connected by multiple pipelines, and supported by external metal frames. Within the cracking facilities there are several metal-frame utilitarian buildings.

In addition, there are groupings of offices, garages, and other buildings located throughout the property.

The remaining structures are cylindrical oil storage tanks, primarily located on the eastern portion of the property.



Figure 37. Overview of Suncor Energy property from I-270. Photo includes the facilities at 5800 Brighton Road (left) and 5801 Brighton Boulevard (right). View facing south.

Historical summary

In 1929 the Colorado-based Continental Oil merged with the Oklahoma-based Marland Oil Company to form Conoco Oil. That same year Continental Oil purchased the rights to new oil discoveries in the Fort Collins-Wellington area. In 1930 Conoco built a new refinery to process the oil from northern Colorado on the west side of Brighton Boulevard at Sand Creek Junction. The location was chosen due to the proximity to the UP and CB&Q rail lines and an available water source from Sand Creek. By 1940 Bay Petroleum, later Colorado Refining Co. (CRC), had also opened a refinery on the east side of Brighton Boulevard. In 1949 Conoco modernized and expanded the refinery to the south and west. By the early

1950s the Conoco and CRC refineries had largely grown to fill the current land parcels. In 2002 Conoco merged with Phillips Petroleum Company to form ConocoPhillips. In 2003 ConocoPhillips sold its refinery on the west side of Brighton Boulevard to Calgary-based Suncor Energy (USA) Inc. Suncor also purchased the refinery on the east side of Brighton Boulevard from CRC in 2005. Currently the combined facilities, under Suncor Energy Inc., represent the only petroleum refinery in Colorado.

Significance assessment

Suncor Energy represents the conglomeration of two historic oil refineries and is the only operating oil refinery remaining in Colorado. Together, these facilities spearheaded the industrial development of the Commerce City area in the mid-twentieth century. After incorporation as Commerce Town in 1952, the civic leaders actively promoted industrial development over residential development, resulting in the commercial and industrial corridor along I-270 and US 6/US 85. As such, Suncor Energy is potentially eligible for listing in the National Register under *Criterion A: Industry*.

Research did not uncover any significant associations between this property and persons associated with industrial oil and gas development in Colorado under *Criterion B*.

The individual buildings at Suncor Energy were not surveyed and it is unknown if they possess any unique architectural features, although they generally appear to be utilitarian in nature and do not readily reflect a particular style, era, method of construction, or the work of a master. The cracking facilities appear to be composed of standard components and technology that are common to oil refineries. However, further research is needed to determine the presence or development of unique and innovative technologies, as well as the presence of older technologies that may convey significance related to petroleum refineries. Because a complete survey of the Suncor property was not possible at this time, it cannot be determined if the property possesses significance under *Criterion C*.

Suncor Energy is not likely to contain information important to history or prehistory beyond what is already documented and is therefore not eligible under *Criterion D*.

A complete assessment of physical integrity was not possible without access to Suncor Energy. However, aerial maps and photographs indicate that the property has changed significantly since its initial construction in 1930 and it is difficult to determine how many of the original buildings remain on the property. Although a more detailed assessment is required, the property has operated as a refinery since its initial construction and appears to retain relatively good integrity of location, feeling, and association. Alterations to the entire property over time raise concern regarding integrity of setting, design, workmanship, and materials.

Because further research and survey is required to effectively determine National Register eligibility, Suncor Energy is determined as *Needs Data*.

(15) 5AM.4045 – Denver Oil (4100 E. 60th Avenue, map label 27)

Description

This building consists of a core 1922 structure with several additions dating from c.1925 to c.1955. The original structure is a two-story gable-roof brick warehouse with low parapets at the gable ends, metal multi-light windows, and a large bay opening side (east) elevation. The original entrance is not visible. The additions are generally flat-roof, brick-clad building with metal windows. Two c.1940 additions at the northeast and northwest corners reflect modest modern influences such as glass-block panels flanking the main entrance, an original wood-with-glazing door on the facade, and a curved brick wall and wood slab entry door at the northwest corner.



Figure 38. Main building at Denver Oil, side (east) and rear (south) elevations. View facing west/northwest.

Historical summary

The first portion of this building was completed in 1922. According to the former owner, Gregory Wolf, this building originally housed the Garden Farm Dairy. Garden Farm Dairy was associated with Robinson Dairy, one of the largest dairy operations in the Denver area. After World War II the building was converted into a machine shop that produced hydraulic valves, serpentine belts, oil pipe parts, and ice auger parts. Mr. Wolfe also stated that the shop produced some rocket components that were used on the Apollo 11 spacecraft. The present owner, Denver Oil LLC, purchased the property in 2018.

Significance assessment

Although this building represents an early industrial property in the area, it no longer conveys its original use as a dairy facility and no written records were found linking this address to a specific business. This lack of evidence fails to reveal a direct association with significant activities, events, or persons associated with industrial development in the Commerce City area under *Criteria A* or *Criteria B*. The central 1922 portion of the building appears to represent a common form for small industrial buildings of the time and the multiple additions have occluded a significant portion of this original structure from view. Aside from the modest c.1940 modernist features, the additions do not embody the characteristics of any cohesive stylistic design, period, or method of construction. Therefore, the building does not possess architectural or design significance under *Criterion C*. The building is not likely to contain information

important to history or prehistory beyond what is already documented and does not possess significance under *Criterion D*. Due to the lack of significance, this building is not eligible for inclusion in the National Register.

(16) 5AM.4046 – Domenico Transportation Services Building (5699 Dexter Street, map label 33)

Description

This one-story, rectangular-plan, commercial building (Building A) has a flat roof and a flat metal awning with metal tie rods spans the front (east) facade. Walls are clad in metal paneling with brick veneer beneath the awning on the facade. Other fenestration includes metal fixed and sliding sash windows. A large addition to the rear has a flat roof, metal walls, and a metal parapet with an additional “Domenico Transportation” sign. A secondary entrance on the side (south) elevation of the addition is a glass and metal door with a sidelight.

Two associated buildings are located on this property. Building B is a one-story, rectangular-plan, front-gable roof garage with metal panel walls, and an oversized bay door located west of Building A. Building C is an L-plan open storage shed located west of Buildings A and B has a shed-roof, metal pole supports, and metal panel walls on the north portion only.



Figure 39. Domenico Transportation Services Building, front (east) facade and side (south) elevation. View facing northwest.

Historical summary

This property was acquired by the Pepper Tank Company from Carl and Margaret Sackett in 1954. Buildings A and B were constructed in 1964 and Building C was constructed in 1986. The Pepper Tank Company is the current owner but the building has a sign for Domenico Transportation Services.

Significance assessment

Research and field review did not reveal a direct association with significant activities, events, or persons associated with industrial development in the Commerce City area. As such, this building does not possess significance under *Criteria A* or *B*. The building has minimal architectural details, does not embody the characteristics of a specific architectural style or building type, lacks artistic value, and is not the work of a master. Therefore, the building does not possess architectural or design significance under *Criterion C*. The building is not likely to contain information important to history or prehistory beyond what

is already documented and does not possess significance under *Criterion D*. Due to lack of significance, this building is not eligible for inclusion in the National Register.

(17) 5AM.4047 – Plastics, Inc. (5701 Dexter Street, map label 34)

Description

This long, narrow, rectangular-plan production shed is approximately 33,000 square feet and consists of a taller front-gable portion with a shed-roof portion to the north. Concrete-block construction makes up the lower portion of both the gable and shed-roof sections. Fenestration includes glass and metal entry doors and fixed metal windows, and two oversized metal bay doors on the south elevation. In addition, original, oversized, fixed, metal, multi-light windows are at clerestory level of the eastern half of the shed-roof section, and all but the rear (west) elevation on the gable portion; metal paneling has covered most of the windows on the south elevation. A small, one-story, shed-roof addition is located at the southwest corner.



Figure 40. Plastics, Inc. building, front (east) facade and side (north) elevation. View facing southwest.

Historical summary

The building located at 5701 Dexter Street was constructed around 1947 as an industrial production shed by the Pepper Tank Company. The Pepper Tank Company is the earliest documented owner of the property, and while the company also had operations in Wyoming, the building at 5701 Dexter Street was the company's home base and primary location at the time of the building's construction and for at least a decade or more after. Beall Equipment and Timpte-Beall, Inc. operated in this facility between 1960 and 1976. Beall sold pipes and tanks, while Timpte-Beall, Inc. produced snowplows for all manner of trucks. CR Folsom Real Estate, the current owners, acquired the property in 2017 and it is occupied by Plastics, Inc., which supplies plastic supplies to the construction industry. The building has housed a series of manufacturing concerns since its construction around 1947, representing industrial and commercial growth trends in Commerce City in the decades following World War II. The property is surrounded by commercial properties to the north, west, and south, and is buffered from I-270 by these properties.

Significance assessment

The property was constructed around 1947 as an industrial production shed by the Pepper Tank Company. Research uncovered from public records or historic newspapers did not reveal any of the companies associated with this parcel to have important associations with the industrial development or significant trends of Commerce City. Therefore, this property does not possess significance under *Criterion A*. Research did not uncover any associations with any significant persons. While Joseph E.

Pepper, founder and owner of the Pepper Tank Company, was an industrious and successful businessman, there is no information to support his individual significance as an important person in the community. As such, the property is not eligible under *Criterion B*. The building is a representative, intact, and early example of a postwar concrete block and metal manufacturing production shed in Commerce City. The manufacturing production shed at 5701 Dexter Street retains all of the character-defining features of this property type and is one of a small pool of surviving postwar production sheds in Commerce City. The building at 5701 Dexter Street is less altered than most of the other surviving production sheds in Commerce City; it is the only one with a full exposed facade of original clerestory windows and one of only a couple without large modern additions. It is also one of only two examples of this type in Commerce City that used concrete block. The building embodies the distinctive characteristics of a postwar manufacturing production shed and, as such, is determined eligible under *Criterion C*. The building is not likely to contain information important to history or prehistory beyond what is already documented and does not possess significance under *Criterion D*.

Changes over time include replacement windows and doors, as well as covering original steel sash clerestory windows, infilling a large garage bay with concrete block, and a shed-roof addition. These changes have somewhat affected the building's integrity of design and materials. However, the building retains sufficient integrity of location, setting, feeling, association, and workmanship to convey its significance as a postwar manufacturing production shed. These integrity concerns do not affect its ability to qualify for listing in the National Register of Historic Places under *Criterion C*.

(18) 5AM.4048 – Marquez House (6860 E. 52nd Place, map label 68)

Description

This Ranch house has a hip roof with narrow boxed eaves and a hip-roof porch with square brick columns has been partially enclosed with lattice. The walls are clad in asbestos shingles and replacement plywood paneling and the fenestration includes original aluminum fixed and one-by-one sliding sash windows. The lot also includes and hip roof garage (c.1970) and side-gable frame shed (c.2000).



Figure 41. Marquez House, front (north) facade and side (east) elevation. View facing southwest.

Historical summary

This house was constructed in 1966 and the original owner was Ernest Marquez, who purchased the property in 1957. Maria Marquez granted the property to the current owner, Iris Marquez, in 2011.

Significance assessment

This property does not have a direct association with any specific land developments or other significant activities, events, or persons associated with residential development in the Commerce City area under *Criterion A* or *Criterion B*. This house has minimal architectural details including a hip roof with boxed eaves and brick porch, which do not fully embody the defining characteristics of a Ranch house, or a particular period or method of construction. Therefore, the house does not possess architectural or design significance under *Criterion C*. The house is not likely to contain information important to history or prehistory beyond what is already documented and does not possess significance under *Criterion D*. Due to lack of significance, this building is not eligible for inclusion in the National Register.

(19) 5AM.4049 – Duran House (5251 Oneida Street, map label 70)

Description

This Ranch house has a front-gable roof with narrow eaves and a side-gable, enclosed two-bay garage wing on the side (north) elevation. The raised entry is within an inset porch that has a wood post, railing, and steps. The walls are clad in vinyl siding with stone veneer at the raised basement level and fenestration includes replacement doors and vinyl replacement windows. The lot also includes a detached front-gable garage (c.1955) and a prefabricated shed (c.2000).

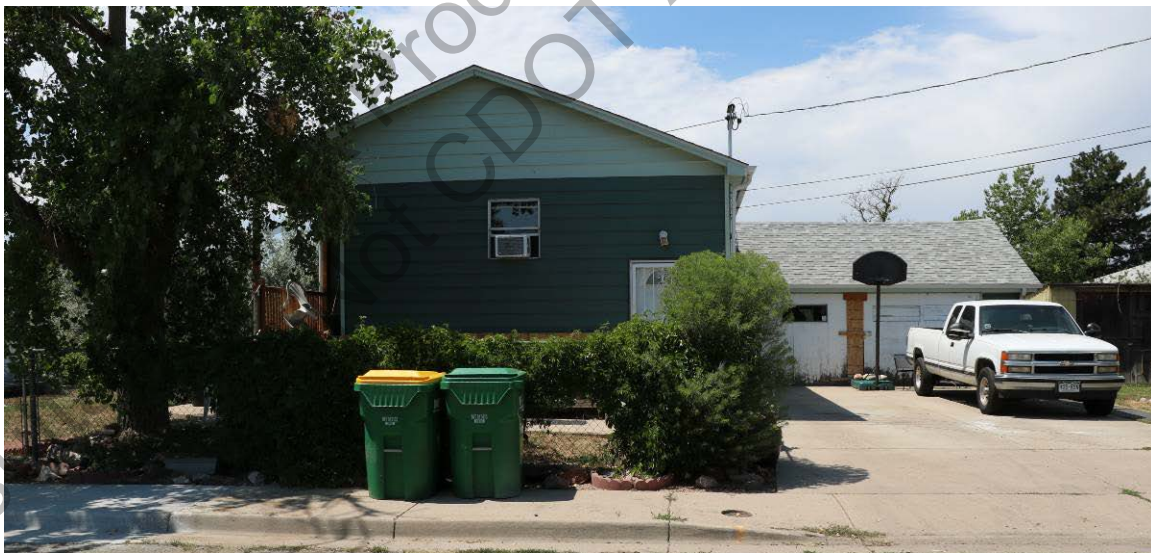


Figure 42. Duran House, front (east) facade. View facing west.

Historical summary

This property was constructed in 1955 and the first owner was J Floyd Snodgrass. Adams County property records show that purchased the property from in 1959. The current owners, Manuel and Estella Duran, purchased the property from I.V. and Edna Littlefield in 1960.

Significance assessment

This property does not have a direct or significant association with the larger trend of postwar residential development or significant persons in the Commerce City area under *Criteria A* or *B*. The house has limited architectural features such as a single-story rectangular plan and projecting eaves, which do not fully embody the defining characteristics of a Ranch house, or a particular period or method of construction. Therefore, this house does not possess architectural or design significance under *Criterion C*. The house is not likely to contain information important to history or prehistory beyond what is already documented and does not possess significance under *Criterion D*. Due to lack of significance, this building is not eligible for inclusion in the National Register.

(20) 5AM.4050 – Applegarth House (5250 Oneida Street, map label 71)

Description

This Ranch house has a side-gable roof and an attached, shed-roof, single-bay enclosed carport. The walls are clad in asbestos shingles and fenestration includes horizontally divided, two-over-two and replacement sliding sash windows. The main entry is obscured, but the two secondary entrances consist of sliding glass doors with metal surrounds. There are no other associated buildings on the lot.



Figure 43. Applegarth House, front (west) facade. View facing east.

Historical summary

This house was constructed in 1955 and the first owner was C.W. Thuringer. The current owner, Martha Applegarth purchased the property in 1976.

Significance assessment

This property does not have a direct or significant association with the larger trend of postwar residential development or significant persons in the Commerce City area under *Criteria A* or *Criteria B*. The house does not embody the characteristics of a specific architectural style, period, or method of construction and does not possess architectural or design significance under *Criterion C*. It is not likely to contain

information important to history or prehistory beyond what is already documented and does not possess significance under *Criterion D*. Due to lack of significance, this building is not eligible for inclusion in the National Register.

(21) 5AM.4070 – Corner Stone Truck Repair (5790 Dahlia Street, map label 40)

Description

This property is a two-story, rectangular-plan industrial building with a metal exterior and a barrel roof. A 1,000-square-foot, one-story addition with a rectangular plan and a flat roof is located on the west side and a 3,000-square-foot, two-story addition with a rectangular plan and a gabled roof is located on the east side. The building has one multi-light window near the west end of its north side. The building is surrounded by commercial properties to the north and east and Vasquez Boulevard and I-270 to the south and west.



Figure 44. The Corner Stone Truck Repair building. View looking southeast from Dahlia Street.

Historical summary

The original metal warehouse building with a barrel roof and one-story office was constructed around 1966. The 1969 directory of Denver subdivisions lists H.M. Rattle Co. Street Sweeping Equipment as the occupant of the property. In 1974 Saunders Leasing Co., a truck rental company, owned the property. Ken Johnson purchased the property sometime in the mid-1980s, and in 1997 the parcel was named “Ken Johnson Subdivision.” Johnson’s son, George, sold it to Fate Enterprises, LLC in 2015. Today Cornerstone Truck Repair occupies the property and services diesel semi-trucks and tractors.

Significance assessment

This property is in an area of Commerce City that was historically commercial and industrial. The property was built near the end of the postwar commercial development in the area and has no important association with that pattern of development in Commerce City. As a result, the property is not eligible under *Criterion A*. Research of the businesses located at this property in public records, historic, newspapers, and local community documents did not reveal a direct association with significant persons of significance under *Criterion B*. This is a modest industrial building from the 1960s. The property does

not convey historic significance in architectural design, materials or workmanship and does not possess high artistic values nor demonstrate a particular method of construction and is, therefore, not eligible under *Criterion C*. The building is not likely to contain information important to history or prehistory beyond what is already documented and does not possess significance under *Criterion D*.

(22) 5AM.4107 – 7141 Lafayette Street (map label 1)

Description

This property consists of an isolated residential garage with stucco walls and a hipped roof with slight overhanging eaves and exposed rafters. The facade includes a made up of a single-car garage bay next to a double-car garage bay.



Figure 45. Garage at 7141 Lafayette Street, facade (east) and south elevations. View facing northwest.

Historical summary

Adams County Assessor records indicate this building was constructed in 1976. However, aerial photos indicate the presence of a structure in its approximate location as early as 1963. This property was purchased by the DeCarlo family c.1910 and remains within the family's ownership. A main house and at least one other outbuilding were demolished for the construction of I-270 c.1990.

Significance assessment

Although this property is historically associated with agriculture and Italian settlement in the Welby area, as an isolated outbuilding this garage cannot convey historic significance in these areas under *Criterion A* or *Criterion B*. This garage cannot convey significance associated with a type, period, or method of construction, or any previous agricultural associations and does not possess architectural or design significance under *Criterion C*. This property is not likely to contain information important to history or

prehistory beyond what is already documented and does not possess significance under Criterion D. Due to the lack of significance, this building is not eligible for inclusion in the National Register.

(23) 5AM.4108 – 6875 Columbine Street (map label 6)

Description

The property at 6875 Columbine Street consists of five residential buildings:

- Building A is a Minimal Traditional house with a hip roof, stucco cladding, an attached one-bay garage, and a recessed porch entry.
- Building B is a Ranch house with a side-gable roof, horizontal siding, and a shed-roof porch.
- Building C is a Minimal Traditional house with a hip roof, stucco cladding, and a shed-roof porch with red brick half-columns and metal supports.
- Building D is a one-and-one-half-story front gable vernacular house with horizontal wood siding, an off-center recessed porch, and a detached one-bay garage.
- Building E is a Minimal Traditional house with a hip roof, stucco cladding, an attached one-bay garage, and a recessed porch entry.



Figure 46. 6875 Columbine Street, Building A, facade (south) and west elevations. View facing northwest.

Historical summary

The five buildings currently located at 6875 Columbine Street were constructed between 1950 and 1959. In 1952 Sam and Anna Marie Larusso purchased the property from Ralph Fabrizio. The Larussos developed the parcel along a central corridor that was planned as a segment of E. 69th Avenue, with separate addresses for each building and rented the individual houses. The Knights of Columbus Home Association of Welby purchased the property in 1977 and continued to rent the five houses. The current owners, Jedidiah Properties, purchased the property in 2020.

Significance assessment

This property does not have significant association with agricultural development in the Welby area or residential development in southwest Adams County and does not possess significance under *Criterion A*. Sam and Anna Marie Larusso were not found to be significant individuals, or part of a prominent or significant family within the Italian community in Welby under *Criterion B*. The five houses modest Minimal Traditional, Ranch, and vernacular forms associated with residential architecture during the 1950s and do not fully embody the character-defining features of each style. Therefore, this property does not possess architectural or design significance under *Criterion C*. This property is not likely to contain information important to history or prehistory beyond what is already documented and does not possess significance under *Criterion D*. Due to the lack of significance, this property is not eligible for inclusion in the National Register.

(24) 5AM.4109 – Martinez House (6824 Columbine Street, map label 7)

Description

This one-story double-pen house has a front-gable roof with hip-roof additions flanking either side of the original house (on the east and west elevations). The original 1937 house is clad with vertical board and batten siding and the remaining walls are clad with horizontal wood siding. The facade (north) has a front porch awning with decorative iron posts and a fiberglass roof. The lot also includes a side-gable frame shed.



Figure 47. 6824 Columbine Street facade (north) and west elevation, view facing southeast. The original house had vertical painted wood siding, visible under the front awning.

Historical summary

Adams County Assessor records indicate that this house was constructed in 1937. The earliest known owner was Joseph Porreco, who purchased the property in 1955. Joseph Porreco was a truck farmer by trade but there is no evidence that he cultivated the small, narrow lot. The current owner, Maria Martinez, purchased the property in 2014.

Significance assessment

This property represents only a small remnant of a historic farm property and does not have significant association with agricultural development in the Welby area or significant association with residential community development under *Criteria A*. Joseph Porreco was not found to be a significant individual within the Italian community of Welby and this property is not significant under *Criterion B*. This house represents a vernacular-style, double-pen form with multiple additions that obscure the historic form. The house no longer embodies the character-defining features of any particular architectural style and does not possess architectural or design significance under *Criterion C*. The building is not likely to contain information important to history or prehistory beyond what is already documented and does not possess significance under *Criterion D*. Due to lack of significance, this property is not eligible for inclusion in the National Register.

(25) 5AM.4110 – Semmaterials Energy Partners, LLC (4310 E. 60th Avenue, map label 32)

Description

This one-story, rectangular-plan, Ranch-style commercial office building was built in 1953 and has a side-gable roof with overhanging eaves and asphalt shingles. Windows are original casement and replacement vinyl horizontal and vertical sliding and walls are clad in weatherboard and brick. The property contains large, rectangular outbuildings and many large cylindrical storage tanks. The property has an irregular shaped parcel boundary with I-270 following the southern perimeter of the property, railroad tracks along its west, and commercial properties to the north and east.



Figure 48. Overview of the buildings at 4310 E. 60th Avenue, view facing southeast. Photo from the Colorado Department of Transportation.

Historical summary

This commercial property was constructed in 1953. The earliest ownership record associated with the current legal description dates to the 1962 *Denver Combined Suburban Directory*, which lists the Pepper Tank Company at this address. It was likely built as secondary storage and office space for the Pepper Tank Company, main office located at 5701 Dexter Street (5AM.4047). The company operated out of Denver, Colorado and Casper, Wyoming in the 1940s and 1950s. The Pepper Tank Company and additional companies associated with the parcel appear to have been associated with the oil and gas industry. The chain of ownership from its construction indicates that businesses associated with the oil and gas industry have occupied this parcel since at least 1962.

Significance assessment

Research uncovered from public records or historic newspapers did not reveal any of the companies associated with this parcel to have important associations with the industrial development or significant trends of Commerce City. Therefore, this property does not possess significance under *Criterion A*. Research in public records, historic, newspapers, and local community documents did not reveal this property to have a direct association with significant persons of significance under *Criterion B*. The 1953 commercial building resembles the Ranch style with its one-story design and low-pitched, side-gable roof, but generally has minimal architectural details and does not fully embody the characteristics of that style. Overall, the buildings on this property lack artistic value and are not the work of a master. Therefore, the building does not possess architectural or design significance under *Criterion C*. The building is not likely to contain information important to history or prehistory beyond what is already documented and does not possess significance under *Criterion D*. Due to lack of significance, this building is not eligible for inclusion in the National Register.

(26) 5AM.4111 – United Asphalts, Inc. (4306 E. 60th Avenue, map label 31)

Description

Located at 4306 E. 60th Avenue in Commerce City, this resource consists of a 1965 one-story, rectangular-plan, commercial warehouse with a metal side-gable roof, fixed windows, and corrugated metal walls. The building contains multiple garage bays and three entrances with metal doors. Multiple outbuildings and cylindrical storage tanks surround the building. The property is located on a large dirt lot to the south of I-270 and is surrounded by commercial properties to the north and east, Sand Creek to the south, and railroad tracks to the west.



Figure 49. 4303 E. 60th Avenue, view facing north. Photo from the Colorado Department of Transportation.

Historical summary

In 1947 the Pepper Tank Company acquired this property from William D. and Doris E. Gerretson. According to Adams County records, two buildings were constructed on this property in 1965: a commercial building and small utility building. In 1979 the Pepper Tank Company deeded the property to United Asphalts, Inc., which is the current owner of the property. This chain of ownership indicates that businesses associated with the oil and gas industry have occupied this parcel since at least 1965.

Significance assessment

This property does not have an association with significant industrial development or with significant events, patterns, or trends of Commerce City and does not possess significance under Criterion A. The property owners are not considered to persons of significance under Criterion B. The 1965 side-gable warehouse, modular office buildings, and small utility outbuildings at 4306 E. 60th Avenue have minimal architectural details, do not embody the characteristics of a specific architectural style or building type, lack artistic value, and are not the work of a master. Therefore, the property does not possess architectural or design significance under Criterion C. The property is not likely to contain information important to history or prehistory beyond what is already documented and does not possess significance under *Criterion D*. Due to lack of significance, this property is not eligible for inclusion in the National Register.

(27) 5AM.4112 – Stewart & Stevenson Power Products (5820 Dahlia Street, map label 39)

Description

Located at 5840 Dahlia Street, this one-story, irregular-plan service garage and warehouse consists of two 1946 buildings, one barrel-shaped and one flat roof, that have been joined together through many flat and gable roof additions to make one building. Walls are made of corrugated metal and concrete block and windows are original metal fixed or casement windows. The building faces Dahlia Street to the west, E. 58th Place to the north, an adjacent commercial property to the east, and E. 58th Avenue to the south. The property contains one small outbuilding to the east of the main building.



Figure 50. 5820 Dahlia Street overview and facade (west), view facing southeast.

Historical summary

According to Adams County records, two commercial service garages were built in 1946 on this property. The original owner of this property is unknown. The Faris-Mortiz Equipment Company was associated with the property between 1962 and 1979. Stewart and Stevenson Power Inc. moved into the structure in 1980.

Significance assessment

The two original buildings on this parcel were constructed in 1946 as service garages. Research did not indicate the property represents or is associated with significant commercial development or other significant trends related to industry and commerce in Adams County. As a commercial property, it is not associated with any significant businesses. Therefore, this property does not possess significance under *Criterion A*. Research of the businesses located at this property in public records, historic, newspapers, and local community documents did not reveal a direct association with significant persons of significance under *Criterion B*. The two original 1946 buildings have been combined into a single building over time through the construction of additions and do not represent a cohesive architectural design. Aside from the barrel-roofed section, the original buildings and the single building that stands today have minimal architectural details, do not embody the characteristics of a specific architectural style, lack artistic value, and are not the work of a master. Therefore, these buildings do not possess architectural or design significance under *Criterion C*. This property is not likely to contain information important to history or

prehistory beyond what is already documented and does not possess significance under *Criterion D*. Due to lack of significance, this property is not eligible for inclusion in the National Register.

(28) 5AM.4113 – Western Material Handling (5678 Eudora Street, map label 42)

Description

This one-story, rectangular-plan, commercial office building with an over-height warehouse extending to the east has a flat roof with metal flashing along the roofline. Walls are made of brick and concrete block and windows are metal fixed, casement, and multi-light. The facade has red brick with beige brick around horizontally oriented tripartite casement windows. Historically, beige brick around the windows created a defining stripe across the facade. This stripe has recently been repainted white. Eudora Street flanks the western perimeter of the parcel, E. 56th Avenue to the south, and adjacent commercial properties to the east and north.



Figure 51. 5678 Eudora Street, view facing southeast.

Historical summary

This commercial property was constructed around 1960. The first owner, P-T Engineering Inc., leased the property to Allen Bearings Supply Company, Inc. in 1961. The companies who operated out of this property between the 1960s and 1990s include Allen Bearings, Western Bearings, and Kaman Bearing & Supply, all of which produced bearings and supplies related to the bearings industry. Roy and Kathy Bowling purchased the property in 1998. As of September 2020, the northern third of the building appears vacant and the southern third has Western Handling Materials as a tenant.

Significance assessment

The property located at 5678 Eudora Street was built around 1960 as a commercial building. The companies who operated out of this property between the 1960s and 1990s all produced bearings and supplies related to the bearings industry. None of these companies were found to have an important association with the industrial development or significant trends of Commerce City. Therefore, this property does not possess significance under *Criterion A*. Research of the businesses located at this property in public records, historic, newspapers, and local community documents did not reveal a direct association with significant persons of significance under *Criterion B*. The 1967 commercial building has some architectural details such as decorative brick window surrounds and paired entryways with flat roof awnings with wide eaves and decorative brick wall supports. However, these do not fully embody the characteristics of the Modern Movement architectural style and lack significant artistic value. Therefore,

the building does not possess architectural or design significance under *Criterion C*. The building is not likely to contain information important to history or prehistory beyond what is already documented and does not possess significance under *Criterion D*. Due to lack of significance, this building is not eligible for inclusion in the National Register.

(29) 5AM.4114 – Wagner Rents: The CAT Rental Store (5455 Holly Street, map label 44)

Description

This one-story, L-shaped warehouse has a metal cross-gable roof and a metal cornice. Walls are clad with corrugated metal and contain narrow vertical slit windows on the south elevation. The building contains multiple garage bays and one brick addition built between 1957 and 1991. It is located on a dirt lot and is surrounded by commercial properties to the west, north, and east. Interstate-270 follows along the property's southern perimeter.



Figure 52. Aerial overview of 5455 Holly Street. View facing north. Photo from the Colorado Department of Transportation.

Historical summary

This property was built in 1973 and the earliest known owner was the Ed H. Honnen Company. According to Adams County records, the Dotty Corporation of Colorado acquired the property from the Ed H. Honnen Company in 1978. The Dotty Corporation of Colorado leased 5455 Holly Street to the Wagner Equipment Company starting on July 1, 1979. In 1985 the Dotty Corporation of Colorado granted the property to the United Bank of Denver. The current owner of the property is Wagner Equipment Company and the current business at this location is Wagner Rents: The CAT Rental Store, according to the sign on the buildings. Research in Adams County records and newspapers did not result in additional historic background of any of these companies.

Significance assessment

This property does not have significant association with industrial development or significant trends in the Commerce City area and does not possess significance under *Criterion A*. Research of the businesses located at this property in public records, historic, newspapers, and local community documents did not reveal a direct association with significant persons of significance under *Criterion B*. The 1973 commercial building has minimal architectural details such as the corrugated metal cornice on the facade but does not embody the characteristics of a specific architectural style or building type, lacks artistic value, and is not the work of a master. Therefore, the building does not possess architectural or design significance under *Criterion C*. The building is not likely to contain information important to history or prehistory beyond what is already documented and does not possess significance under *Criterion D*. Due to lack of significance, this building is not eligible for inclusion in the National Register.

(30) 5AM.4115 – E&K Trucking, Inc. (5421 Krameria Street, map label 46)

Description

This two-story, rectangular-plan warehouse has a front-gable metal roof. Two metal vents are present on the south elevation and walls are clad in corrugated metal. Windows are present only on the north elevation and are metal sliders or are boarded shut. The facade is made up of three oversize garage bays with metal doors. Ground level entry is accessible from the north elevation and wooden stairs lead to a second level exterior landing and entry with a four-panel steel door with a fan light. The property is located within a mixed residential and commercial area with commercial properties to the west, residential to the north and east, and Interstate-270 to the immediate south.



Figure 53. 5421 Krameria Street overview and facade (east), view facing west.

Historical summary

The commercial building was built in 1975 and the original owners were Evert and Karen Klingbell. Evert Klingbell was the manager of Klingbell Trucking Company located at this property. The current owners of 5421 Krameria Street are Evert K. Klingbell and Rebecca Roberts-Klingbell and the current sign on the building reads “E&K Trucking.”

Significance assessment

The original business located at 5421 Krameria Street, Klingbell Trucking Company, later known as E&K Trucking, has operated out of this building since its construction in 1975. Research in public records and historic newspapers did not reveal the trucking company to have important associations with the industrial or commercial development or other significant trends of Commerce City. Therefore, this property does not possess significance under *Criterion A*. Research into the Klingbell family did not reveal them as significant persons under *Criterion B*. The 1975 front-gable corrugated metal garage has minimal architectural details, does not embody the characteristics of a specific architectural style or building type, lacks artistic value, and is not the work of a master. Therefore, the building does not possess architectural or design significance under *Criterion C*. The building is not likely to contain information important to history or prehistory beyond what is already documented and does not possess significance under *Criterion D*. Due to lack of significance, this building is not eligible for inclusion in the National Register.

(31) 5AM.4116 – Masek Golf Cars (5345 Newport Street, map label 64)

Description

This two-story, rectangular-plan service garage has an over-height, two-story office section extending towards the east, both with flat roofs. Windows are original fixed windows present only on the south and east elevations, and walls are made of poured precast concrete with decorative vertical ribbing. The facade (south) contains four oversized garage bays with metal doors and a large flat roof metal canopy. Decorative blue stripes appear on the facade and east and west elevations. The property is located on a paved asphalt lot, is surrounded by commercial properties to the west, north, and east, and Interstate-270 is visible to the south.



Figure 54. 5345 Newport Street facade (south) and east elevation, view facing northwest.

Historical summary

The current warehouse and office building on this property was constructed in 1973 and Franzen & Sons Plumbing, Inc. owned the property in 1972, according to Adams County records. Franzen & Sons granted ownership to Saunders Leasing System, Inc. in 1976. Great Point Developers owned the property

between 1977 and 2007. Great Point Developers sold the property to the current owners, New Ports LLC, in 2007.

Significance assessment

Research in historic newspapers and public records did not reveal Franzen & Sons, Great Point Developers, or New Ports LLC to have important associations with industrial development or significant events in Commerce City. Therefore, the property does not possess significance under *Criterion A*. Research of the businesses located at this property in public records, historic, newspapers, and local community documents did not reveal a direct association with significant persons of significance under *Criterion B*. The 1973 commercial building has minimal architectural details including the poured precast concrete exterior with decorative vertical ribbing but does not embody the characteristics of a specific architectural style or building type, lacks artistic value, and is not the work of a master. Therefore, the building does not possess architectural or design significance under *Criterion C*. The building is not likely to contain information important to history or prehistory beyond what is already documented and does not possess significance under *Criterion D*. Due to lack of significance, this building is not eligible for inclusion in the National Register.

(32) 5AM.4117 – Denver Truck & Trailer (5280 Newport Street, map label 65)

Description

This two-story, L-shaped warehouse and office has a side-gable roof. Windows are original metal sliding windows and walls are made of corrugated metal. The building contains multiple garage bays on the facade (west), south, and east elevations. A large shed addition built between 1978 and 1991 extends from the east elevation. The property is located on a paved asphalt and dirt lot, is surrounded by commercial properties to the west, north, and east, and residential properties to the south.



Figure 55. 5280 Newport Street facade (west) and south elevation, view facing northeast.

Historical summary

The current warehouse and office building on this property was constructed in 1974. In 1972, Franzen & Sons Plumbing, Inc. purchased the property from Carroll H. and E. Louise Breckenridge. Saunders Leasing System purchased the property in 1976. Businesses associated with the trucking industry, Saunders Leasing System and Denver Truck & Trailer, have occupied this property since 1976.

Significance assessment

The chain of ownership of 5280 Newport Street indicates that businesses associated with the truck and trailer industry have occupied this parcel since 1976 and research into these companies did not reveal any of the companies to have important associations with industrial development or significant events in Commerce City. Therefore, this property does not possess significance under *Criterion A*. Research of the businesses located at this property in public records, historic, newspapers, and local community documents did not reveal a direct association with significant persons of significance under *Criterion B*. The 1974 L-plan corrugated metal commercial building has minimal architectural details, does not embody the characteristics of a specific architectural style or building type, lacks artistic value, and is not the work of a master. Therefore, the building does not possess architectural or design significance under *Criterion C*. The building is not likely to contain information important to history or prehistory beyond what is already documented and does not possess significance under *Criterion D*. Due to lack of significance, this building is not eligible for inclusion in the National Register.

(33) 5AM.4118 – E. 64th Avenue Bridge over Burlington Ditch/O’Brien Canal (map label 19)

Description

This bridge is a single-span, riveted and welded metal girder bridge with concrete abutments and a dirt-surfaced deck. The superstructure was not visible beneath the deck due to access and safety concerns. The railing consists of narrow metal poles that were presumably connected with metal wire at one time.



Figure 56. E 64th Avenue bridge (5AM.4118) over the Burlington Ditch/O’Brien Canal. View facing northeast. Regional Transportation District North Metro (N line) elevated commuter rail line is visible to the east (right).

Historical summary

This bridge was installed c.1960 and may be a modified remnant of a former viaduct or roadway overpass from an unknown location. Although the original purpose has not been confirmed, it may be provided access over the Burlington Ditch/O'Brien Canal during the construction of I-270.

Significance assessment

Research does not conclusively indicate that this bridge was associated with any significant historic events, patterns, or trends during the mid-twentieth century and it is not significant under *Criterion A*. It does not have association with historically significant persons related to the history of Commerce City, Adams County, or Colorado under *Criterion B*. It is a heavily modified structure representing technologies from different eras of bridge construction, does not represent a recognizable bridge type, and does not have engineering or design significance under *Criterion C*. This bridge is not likely to contain information important to history or prehistory and does not possess significance under *Criterion D*. Due to lack of significance, the E. 64th Avenue bridge over the Burlington Ditch/O'Brien Canal is not eligible for inclusion in the National Register.

(34) 5AM.4122 – Keller (5665, 5675, and 5775 Eudora Street, map label 41)

Description

This commercial property features four buildings at three addresses:

- Building A (5775 Eudora Street) is a two-story, 119- by 120-foot, rectangular-plan, commercial garage and office building with a rolled asphalt flat roof. Fenestration are metal fixed windows and walls are clad in corrugated metal, corrugated concrete block, and concrete block.
- Building B is a one-story, 30- by 60-foot, rectangular-plan, oversized five-bay garage with a flat roof and an asphalt shingle cornice overhang. Walls are made of concrete block and the garage doors are metal.
- Building C (5675 Eudora Street) is a two-story, 60- by 100-foot, rectangular-plan, commercial garage with a low-pitched, side-gable metal roof. Walls are clad in corrugated metal and the only window (north elevation) is sliding.
- Building D (5665 Eudora Street) is a one-story, 30- by 60-foot, rectangular-plan, commercial garage and shop with a flat roof and an over-height back section extending to the west. Walls are made of concrete block and windows are fixed multi-light.



Figure 57. 5775 Eudora Street (Building A), view facing southwest (facade).

Historical summary

The first building on this parcel, a five-bay garage, was constructed in 1967 (Building B). The service garage located on this parcel (Building D, 5665 Eudora Street) was constructed in 1974, the c.1960 commercial garage and office building (Building A, 5775 Eudora Street) was moved to this location from a different parcel along Dahlia Street and I-270, and Building C, 5675 Eudora Street, was constructed in 2001. The earliest known business at this property was the Department of Highways Storage. Adams County records show property ownership changing from the F&D Corporation to E. Richard Jorgensen and Fred H. Carlson in 1971. Jorgensen and Carlson worked at the International Tractor & Equipment Company located on Newport Street, which moved onto to this property in the 1970s. Carlson was the president of the company. Jorgensen and Carlson were associated with the property until 1997. Elk Ridge Investments LLLP is the current owner of the parcel and Keller occupies the buildings.

Significance assessment

Research into the Department of State Highways' use of the property, as well as the F&D Corporation and International Tractor & Equipment Company in public records and historic newspapers did not reveal any significance with important events or industrial development of Commerce City under *Criterion A*. E. Richard Jorgensen and Fred H. Carlson purchased the property in 1971 and were associated with the property until 1997, but research into public records, historic newspapers, and local community documents did not reveal a direct association between these individuals and any significant activities or events related to the industrial development in the Commerce City area. As such, this building does not possess significance under *Criterion B*. These buildings and garages represent a range of construction dates and building materials. They are utilitarian commercial buildings with minimal architectural details such as the corrugated concrete facade on Building A and shingle cornice overhang on Building B. Overall, they do not embody the characteristics of a specific architectural style or building type, lack artistic value, and are not the work of a master. Therefore, the property does not possess architectural or design significance under *Criterion C*. This property is not likely to contain information important to history or prehistory beyond what is already documented and does not possess significance under *Criterion D*. Due to lack of significance, this property is not eligible for inclusion in the National Register.

(35) 5AM.4144 – 5450 Krameria Street (map label 47)

Description

This Modern Movement house has a low-pitch side-gable roof with wide eaves. The exterior walls are constructed from concrete block with projecting concrete brick accents. The windows are replacement sliding units with original concrete sills. The lot includes a detached concrete-block garage.



Figure 58. The house at 5450 Krameria Street, front (west) elevation. View facing east.

Historical summary

Adams County Assessor records indicate that this house was constructed in 1960. This property is part of the Tiffany Subdivision, which was platted by the Wilburg Company in 1952. Henry Nusse purchased this property and the adjacent parcel at 5460 Krameria in 1956. Henry Nusse lived in Denver and worked at the Gates Rubber Company in the 1950s. In 2009 Joyce Spieker sold the property to the current owners, Zephyr Investments and Jordan Services.

Significance assessment

This property was not part of designed subdivision and does not have significant association with the larger trend of postwar residential development in Commerce City under *Criterion A*. Henry Nusse was not found to be a historically significant person in Commerce City under *Criterion B*. This house has limited architectural features often associated with the Modern Movement style, such as modern concrete block construction, a low-pitch roofline with wide eaves, and projecting concrete brick details but does not fully embody the defining characteristics of this style under *Criterion C*. This property is not likely to contain information important to history or prehistory beyond what is already documented and does not possess significance under *Criterion D*. Due to lack of significance, this property is not eligible for inclusion in the National Register.

(36) 5AM.4145 – Hernandez House (5400 Locust Street, map label 53)

Description

This Ranch house has a cross-gable roof covered with asphalt shingles. The facade (west) has shed-roof front porch addition with wood supports and railing and a replacement front door. The exterior walls are clad with horizontal wood siding and vertical siding in the gable ends. There is a shed-roof addition on the side (south) elevation and a front-gable covered patio on the rear (east) elevation. The windows are generally fixed and sliding wood units. The lot includes a one-bay detached garage.



Figure 59. The Hernandez House, front (west) elevation. View facing northeast.

Historical summary

Adams County Assessor records indicate that this house was constructed in 1954. This property is part of the Tiffany Subdivision, which was platted by the Wilburg Company in 1952. Allen and Norma Jean Klein purchased the property in 1954. No information was found in local newspapers or city directories regarding the Kleins. This property was sold six times between 1999 and 2016, when it was purchased by the current owner, Bentura Chavez Hernandez.

Significance assessment

This house was not part of designed subdivision and does not have a direct or significant association with the larger trend of postwar residential development in Commerce City under *Criterion A*. No information was found regarding Allen and Norma Jean Klein to establish significance under *Criterion B*. This house has limited architectural features often associated with the Ranch style, such as a one-story rectangular plans and side-gable roofline but does not fully embody the defining characteristics of this style under *Criterion C*. This property is not likely to contain information important to history or prehistory beyond what is already documented and does not possess significance under *Criterion D*. Due to lack of significance, this property is not eligible for inclusion in the National Register.

(37) 5AM.4146 – Colombo House (5401 Magnolia Street, map label 58)

Description

This Minimal Traditional house has a cross-gable roof covered in asphalt shingles. The exterior walls are clad with horizontal metal siding. The facade (east) has a recent semi-circular stoop and replacement sliding windows. There is a gable-roof addition to the rear (west) elevation. The lot includes a gable-roof frame shed.



Figure 60. The Colombo House, front (east) elevation. View facing northwest.

Historical summary

Adams County Assessor records estimate that this house was constructed in c.1935. It was moved to its current location c.1975 in the Value Subdivision No. 1 by Lavern Franzen, who served on the city council of Commerce City in the early 1970s. The property was sold five times between 1998 and 2011, when it was purchased by the current owner, Yesenia Colombo.

Significance assessment

This house was moved to its current location c.1975 and pre-dates the postwar residential development associated with the Value Subdivision No. 1. As a moved property it can no longer convey any potential significance related to agricultural or residential development. Under *Criterion A*. Although Lavern Franzen was a member of the city council of Commerce City, further research did not establish Franzen as a historically significant person in the Commerce City under *Criterion B*. This house has limited architectural features often associated with the Minimal Traditional style, such as a one-story rectangular plan, side-gable roofline, and symmetrical facade but does not fully embody the defining characteristics of this style under *Criterion C*. This property is not likely to contain information important to history or prehistory beyond what is already documented and does not possess significance under *Criterion D*. Due to lack of significance in the above criteria, this property does not meet *Criterion Consideration B* for moved properties and is not eligible for inclusion in the National Register.

(38) 5AM.4147 – 5358 Magnolia Street (map label 60)

Description

This property had limited visibility during field survey. The original house on this parcel was condemned and demolished in the 1970s and the property currently consists of two remaining outbuildings. There is cross-gable frame garage with vertical wood siding and an asphalt shingle roof and a front-gable frame shed with an asphalt shingle roof.



Figure 61. The property at 5358 Magnolia Street. View facing east.

Historical summary

The original house on this parcel was constructed in 1953, and later condemned and demolished in 1974. Adams County Assessor records estimate that the remaining buildings on this property were constructed c.1960. Samuel Marcus sold the property to Charles and Gloria Kolander, who sold it to Lew Rice in the same year. It is unknown when Lew Rice sold the property. Following the demolition of the house, the property went through several changes in ownership.

Significance assessment

As isolated outbuildings, the current garage and shed can no longer convey any potential significance related to postwar residential development under *Criterion A*. Little information was found regarding the original owner Lew Rice, and as an isolated outbuilding this property cannot convey any potential significance related to historically significant persons under *Criterion B*. The remaining garage and shed are simple utilitarian outbuildings and do not represent a specific type, period, or method of construction under *Criterion C*. This property is not likely to contain information important to history or prehistory beyond what is already documented and does not possess significance under *Criterion D*. Due to lack of significance this property is not eligible for inclusion in the National Register.

(39) 5AM.4148 – Duran House (6870 E. 52nd Place, map label 69)

Description

This Minimal Traditional house has a cross-gable roof cover with asphalt shingles. The exterior walls are clad in horizontal metal siding. The facade (north) has a front-gable porch addition and there is a front-gable addition on the side (west) elevation with a side entrance. The windows are replacement sliding and fixed units, including a bay window on the facade.



Figure 62. The Duran House, front (north) elevation. View facing southwest.

Historical summary

Adams County Assessor records indicate that this house was constructed in 1955. This property is part of the Thuringer Subdivision No. 2, which was platted by C.W. Thuringer in 1952. In 1958 Sherman Newberry sold the property to Ruben Banardine and Ida Marie Moya. Manuel and Estella Duran purchased the property in 1966. Manuel Duran was a butcher and the house is still owned by the Duran family.

Significance assessment

This house was not part of designed subdivision and does not have a direct or significant association with the larger trend of postwar residential development in Commerce City under *Criterion A*. Neither the Moya nor Duran families were found to be significant persons in Commerce City under *Criterion B*. This house has limited architectural features often associated with the Minimal Traditional style, such as a one-story rectangular plan, side-gable roofline, and symmetrical facade but does not fully embody the defining characteristics of this style and has significant additions and modifications. As such, this house does not possess architectural or design significance under *Criterion C*. This property is not likely to contain information important to history or prehistory beyond what is already documented and does not possess significance under *Criterion D*. Due to lack of significance this property is not eligible for inclusion in the National Register.

B. Postwar residential subdivision

One postwar residential subdivision was identified in the APE and is discussed below.

(1) 5AM.4031 – Larusso Subdivision (map label 2)

Description

The boundaries of the Larusso subdivision are E. 68th Place to the north, I-270 to the east, E. 68th Avenue to the south, and the back edge of the properties fronting Race Street to the west. Eleven of the 15 houses in the subdivision reflect a common Ranch style design with brick veneer cladding displaying decorative brick banding, cross-hipped roof, and attached garage. Variations on this model include a side-gable roof, recessed porch, and variations in exterior chimney placement or no chimney. Mature deciduous and evergreen trees and shrubs are present in the front and back yards but there is no apparent landscape or streetscape plan implemented in the subdivision.



Figure 63. House at 6850 Race Street, within the Larusso Subdivision.



Figure 64. House at 2141 E. 68th Avenue, within the Larusso Subdivision.

Historical summary

The Larusso Subdivision was platted in 1954 on land purchased by Sam and Marie Larusso in 1946. It was one of many small residential subdivisions developed in Adams County in the 1950s. The subdivision plat originally included 44 parcels, but by 1963 only 15 parcels were developed. In the mid-1960s I-270 was constructed through much of the undeveloped subdivision.

Significance assessment

This subdivision does not have any known associations with significant developments in ethnic heritage, government, industry, transportation, or social history in Adams County and does not possess significance under *Criterion A*. Research into Sam Larusso and the Larusso family, and the early residents of the subdivision, did not indicate significant contributions to Adams County or the nearby community of Welby. Therefore, the subdivision is not significant under *Criterion B*. The houses in the subdivision represent modest postwar, suburban, Ranch-style residences constructed between 1944 and 1970, which are not innovative or distinctive within Adams County. The subdivision was not fully realized and does not represent an innovative or distinctive landscape design. For these reasons, the subdivision does not embody the distinctive characteristics of the Domestic Subdivision subtype. Therefore, the subdivision is not considered significant as a historic district under *Criterion C*. This property is not likely to contain information important to history or prehistory beyond what is already documented and does not possess significance under *Criterion D*. Due to lack of significance, this property is not eligible for inclusion in the National Register.

C. Linear resources

(1) 5AM.464.21 – Chicago Burlington & Quincy/Burlington Northern Santa Fe Railroad Segment—Brush Line (map label 26)

Description

This segment of the Burlington Northern Santa Fe Railway (BNSF) railroad Brush Line, formerly the Chicago Burlington & Quincy (CB&Q), is located between E. 56th Avenue to approximately 250 feet north of the I-270 right-of-way. The resource is comprised of standard gauge railroad track with wood ties and crushed rock ballast. The total length is 4,488 feet and the width is 15 feet along the entire segment. The segment crosses Sand Creek over a concrete box beam bridge with steel H-pile and concrete wall piers (COMC-SA-03-RR/5AM.1418, determined officially not eligible in 2002) and intersects the Union Pacific (UP) line (5AM.472.41) at Sand Creek Junction (5AM.4119), located directly beneath I-270.



Figure 65. Location of CB&Q/BNSF Segment 5AM.464.21 at I-270, indicated with blue arrows. View facing northwest. Jacobs, 2020.

Historical summary

The CB&Q Brush line was completed in May 1882 and provided the first direct rail connection between Denver and Chicago. It also connected and influenced the growth of several towns in Colorado's Eastern Plains, including Fort Morgan, Brush, Akron, Yuma, and Wray. In 1970 the CB&Q merged with the Great Northern and Northern Pacific railroads as the Burlington Northern Railroad. In the 1990s, the Burlington Northern merged with the Atchison, Topeka and Santa Fe to form the BNSF.

Significance assessment

A longer, previously recorded segment of CB&Q railroad in this location (5AM.464.17) was previously determined Officially Eligible in 2007 under *Criterion A*. The CB&Q is associated with early railroad development in Colorado. It provided the first direct connection between Denver and Chicago, as well as the farm towns on the Eastern Plains. It was therefore essential to Denver's development as an industrial metropolis and regional rail hub, as well as the development of small farming communities on the Eastern Plains. The CB&Q Railroad does not have association with significant persons under *Criterion B*, engineering significance under *Criterion C*, or significance for information potential under *Criterion D*. Segment 5AM.464.21 retains integrity of location, design, feeling, and association and supports the overall National Register eligibility of the CB&Q railroad.

(2) 5AM.465.9 – Burlington Ditch/O'Brien Canal (map label 18)

Description

This segment of the Burlington Ditch/O'Brien Canal is located between the Metro Wastewater Reclamation District Robert W. Hite Treatment Facility to the south and E. 74th Avenue to the north. The approximately 2.35-mile segment is 123 feet wide at I-270, including the canal and historic ditch rider roads to the east and west. The primary area of investigation is the crossing under I-270, which includes a concrete drop structure, 175 feet south of I-270, and a metal bridge crossing the canal at E. 64th Avenue (5AM.4118), 100 feet north of I-270. An elevated commuter rail line is located 70 feet east of the ditch. The canal embankment has been reinforced with large concrete and stone rubble through this area.



Figure 66. View of Burlington Ditch/O'Brien Canal at I-270, including elevated commuter rail line to the east (left), view facing south.

Historical summary

The Burlington Ditch was constructed in 1886 by the Burlington Ditch Company. The Denver Reservoir and Irrigation Company (DRICO) was formed in 1903 and acquired the Burlington Ditch. In 1909 DRICO enlarged the Burlington Ditch and built a new extension, the O'Brien Canal. DRICO was reorganized as the Farmers Reservoir and Irrigation Company (FRICO) in 1909.

Significance assessment

Segment 5AM.465.9 was previously evaluated and officially determined to be supporting of the overall eligibility of the Burlington Ditch/O'Brien Canal in 2009. The Burlington Ditch/O'Brien Canal is significant under *Criterion A* for its association with early irrigation in northeast Colorado. The Burlington Ditch/O'Brien Canal does not have association with significant persons under *Criterion B*, engineering significance under *Criterion C*, or significance for information potential under *Criterion D*. Due to the lack of major alterations, it is determined that this segment retain its previous determination of supporting the overall eligibility of the linear resource.

(3) 5AM.472.41 – Denver Pacific/Union Pacific Railroad Segment—Greeley Line (map label 28)

Description

This segment of the Union Pacific (UP) Railroad Greeley Line, formerly the Denver Pacific (DP) Railroad, is located between E. 56th Avenue to approximately 650 feet north of the I-270 right-of-way. The resource is comprised of standard gauge railroad track with wood ties and crushed rock ballast. The total length is 4,120 feet and the segment crosses Sand Creek over a concrete box beam bridge with steel H-pile and concrete wall piers (COMC-4A-02-RR/5AM.1417 - Officially Not Eligible, 2002). The segment is 30 feet wide in the double-tracked section between E 56th Avenue and the north end of the Sand Creek bridge. The segment is 15 feet wide in the single-tracked section between the Sand Creek bridge and the northern terminus. The segment intersects the Burlington Northern Santa Fe (BNSF) Brush Line (5AM.464.21) at Sand Creek Junction (5AM.4119), located directly beneath I-270.



Figure 67. Location of DP/UP Segment 5AM.472.41 at I-270, indicated with blue arrows. View facing northwest. Jacobs, 2020.

Historical summary

The DP was constructed in 1870. It was the first railroad built in Denver and provided a connection to the UP Transcontinental Railroad in Cheyenne, Wyoming. In 1880 the DP was consolidated into the UP.

Significance assessment

A longer, previously recorded segment of DP/UP railroad in this location (5AM.472.21) was previously determined Officially Eligible in 2013 under *Criterion A*. It was the first rail line to directly serve the Colorado Territory and the city of Denver. This line connected Denver by rail with the rest of the country via the transcontinental railroad. It is directly responsible for the growth of Denver's early industries as a regional hub for transportation, industry, and commerce. The DP/UP Railroad does not have association with significant persons under *Criterion B*, engineering significance under *Criterion C*, or significance for information potential under *Criterion D*. Segment 5AM.472.41 retains integrity of location, design, feeling, and association and supports the overall National Register eligibility of the UP railroad.

(4) 5AM.1292.2 – Gardener's Ditch Segment (map label 17)

Description

This segment of the Gardener's Ditch is an 865-foot long, 6-foot-wide former agricultural ditch located between the south property line of 6610 York Street and the northern right-of-way of I-270. Small sections of the historic ditch alignment are apparent as depressions in the landscape; however, these do not convey a contiguous linear resource and are overgrown with grasses and weeds. There is no evidence of historic irrigation resources such as water controls, diversion structures, or laterals along the entirety of the segment.



Figure 68. Location of the former Gardener's Ditch alignment at the south I-270 right-of-way, adjacent to the eastbound entrance ramp from York Street. I-270 is visible to the north (left). View facing southeast.

Historical summary

The Gardener's Ditch was originally constructed between 1861 and 1863. In the early twentieth century it was operated by the Gardener's Mutual Ditch Company and watered small truck farms in the Mouat and Starkes Subdivision, located between York Street and the South Platte River. In the early 2000s the Public Service Company of Colorado (PSCO) purchased the Gardener's Mutual Ditch Company, including all water rights serving the Mouat and Starkes Subdivision, and abandoned the ditch along this segment.

Significance assessment

The Gardener's Ditch was determined Officially Eligible in 1999 for its association with early irrigation and agriculture in Denver under *Criterion A*. It does not have association with significant persons under *Criterion B*, engineering significance under *Criterion C*, or significance for information potential under *Criterion D*. Segment 5AM.1292.2 of Gardener's Ditch has been abandoned and does not retain sufficient integrity to support the overall eligibility of Gardener's Ditch. It does not support the overall National Register eligibility of the Gardener's Ditch.

(5) 5AM.1298.3 – Chicago, Burlington & Quincy Market Street Line Segment (map label 30)

Description

This segment of the Burlington Northern Santa Fe Railway (BNSF) Railroad Market Street Line, formerly the Chicago Burlington & Quincy (CB&Q), is located between E. 56th Avenue and E. 60th Avenue. The resource is comprised of standard gauge railroad track with wood ties and crushed rock ballast. The total length is 2,850 feet long. A 1,115-foot double-tracked section that crosses under I-270 is 30 feet wide.

The remaining sections are 15 feet wide single track. It crosses Sand Creek over a 1965 timber stringer bridge with steel piles (Bridge 0.85 LS 904) that has not been previously evaluated and is located outside the project's APE.



Figure 69. Segment 5AM.1298.3 at I-270. Note that the segment is double-tracked at I-270. View facing south.

Historical summary

The Market Street Line was constructed by the CB&Q between 1911 and 1913. It served as a connection between downtown Denver, the industrial area northeast of the city, and the CB&Q Brush Line, which led to eastern Colorado and Midwestern markets.

Significance assessment

The CB&Q/BNSF Market Street Line (5AM.1298.1) was determined Officially Eligible in 2001 under *Criterion A* for its association with early commerce and industry in Denver. The CB&Q Railroad does not have association with significant persons under *Criterion B*, engineering significance under *Criterion C*, or significance for information potential under *Criterion D*. Segment 5AM.1298.3 retains its original alignment and retains integrity of location, design, feeling, and association and supports the overall National Register eligibility of the CB&Q/BNSF Market Street Line.

(6) 5AM.2410.2 – Brighton Boulevard/State Highway 265 (map label 21)

Description

Segment 5AM.2410.2 is approximately 1.25 miles long, extending on a northeast/southwest trajectory from approximately 441 feet southwest from the I-270 overpass ending at the junction with US 6/US 85. The asphalt-paved road consists of two travel lanes. Southwest of I-270 the segment crosses Sand Creek Bridge E-17-WP. The segment also crosses a concrete pedestrian path (Sand Creek Trail) at this location. The Chicago Burlington & Quincy/Burlington Northern Santa Fe Railroad Segment—Brush Line

(5AM.464.21), Union Pacific Railroad Siding at Suncor Energy (5AM.4121.1), Denver Pacific/Union Pacific Railroad Segment—Greeley Line (5AM.472.41), and the Sand Creek Junction (5AM.4119) all sit directly to the east of the segment under I-270. The segment passes through a mixture of commercial and residential properties.



Figure 70. Brighton Boulevard/SH 265 at Colorado Boulevard looking at the terminus of the segment beyond the I-270 overpass. View facing southwest.

Historical summary

Brighton Boulevard/SH 265 was first built as a county road in the 1880s. Early farm maps show the northern portion of the route from Riverside Cemetery in Adams County, heading northeast, as Road No. 31 (Willits Farm Map, 1899).⁵⁰ In Denver County the road was present south to Claude Street (one block west of York Street) in 1889. A 1909 map of Riverside Cemetery shows the alignment of Brighton Boulevard around the cemetery. The rest of the highway south of York Street was established between 1914 and 1930. In 1939 the portion south of York Street was incorporated into SH 224 (5AM.2410.1). By 1946 SH 265 was designated from York Street near Riverside Cemetery to its end point at US6/85. Research indicates this route was an important, early, and direct link for to transport agricultural goods between northeastern Adams County and Denver.

Significance assessment

Brighton Boulevard/SH 265 is significant under *Criterion A* as a farm-to-market road. SH 265 is a main transportation route from Brighton in Adams County, to Denver, providing an important artery for farms and ranches in the Brighton area to market shipping centers such as the Union Stockyards and rail shipping centers of the Union Pacific and Burlington Northern Railroads. Brighton Boulevard/SH 265 does not have association with significant persons under *Criterion B*, engineering significance under *Criterion C*, or significance for information potential under *Criterion D*. Segment 5AM.2410.2 retains integrity of

⁵⁰ Willits, "Willits Farm Map, Denver Colorado."

location, design, feeling, and association and supports the overall National Register eligibility of the SH 265 under *Criterion A*.

(7) 5AM.3924.2 – Vasquez Boulevard/U.S. Highway 6/U.S. Highway 85/State Highway 2 (map label (36))

Description

Segment 5AM.3924.2 is 4,073 feet long, extending on a northwest/southeast trajectory from E. 52nd Avenue to I-270. The asphalt-paved road consists of a divided highway and has three through lanes in each direction with a concrete median between the directions of traffic throughout the study area. Northeast of E. 52nd Avenue, the segment crosses Sand Creek to the south of the I-270 interchange on Sand Creek Bridge (Bridge E-17-AT/5AM.1363) and a metal coffer dam parallels the bridge to the west. To the southwest of E. 56th Avenue, an on-ramp for south traffic leading to SH 2 and I-70 parts to the right from the segment and crosses over Vasquez Boulevard to the east. The segment crosses railroad tracks over Bridge E-17-CB/5AM.3924.2 to the south of the SH 2 on-ramp and continues under the SH 2 overpass. To the south of the bridge over the railroad tracks, north SH 2 traffic merges with Vasquez Boulevard. Segment 5AM.3924.2 intersects a highway-railroad grade crossing with signals between the SH 2 overpass and the end of the segment at E. 52nd Avenue. Along Segment 5AM.3924.2, US 6, US 85, and SH 2 are the same route. This segment passes through commercial properties.



Figure 71. The north terminus of Segment 5AM.3924.2 at Vasquez Boulevard/US 6/US 85/SH 2 and I-270, view facing southwest.

Historical summary

Segment 5AM.3924.2 originally functioned as a farm-to-market road providing local farmers access to the factories and railroads located in Denver and surrounding towns. A historic review of maps of Adams County and the Commerce City area show alignment changes have modified the route's original use as a

rural farm-to-market road. The 1957 USGS Derby Quadrangle map depicts the same alignment as the 1899 Willits Farm Map and previous quad maps, but the road has been widened and now has two lanes in each direction. Aerial photographs from the 1960s and 1970s show the road as a divided urban roadway with two lanes in each direction by the mid-1960s and three directions in each direction by the 1970s. The most significant change occurred between 1960 and 1965, when a cloverleaf interchange was built where the segment meets present-day I-270. This segment crosses two bridges—Sand Creek Bridge (Bridge E-17-AT/5AM.1363) and Bridge E-17-CB (5AM.3924.2)—both officially not eligible per the 2002 CDOT bridge inventory.

Significance assessment

Under *Criterion A*, the segment of the highway in northeastern Colorado possesses significance under Transportation as an early farm-to-market road that provided primary access for farmers, particularly in Phillips, Logan, Washington, and Morgan Counties, during the 1910s and 1920s at the local level. The western portion of US 6, from Clear Creek Canyon to Grand Junction and east of Denver near Wiggins, possesses significance under *Criterion A* in the area of Politics/Government at the state level. These segments have a direct association with work completed by the Works Progress/Projects Administration (WPA) from 1937 to 1941. The WPA is an important Depression-era, federal work-relief program and an important theme in state history. Vasquez Boulevard/US 6/US 85/SH 2 does not have association with significant persons under *Criterion B*, engineering significance under *Criterion C*, or significance for information potential under *Criterion D*.

Segment 5AM.3924.2 does not retain historic integrity and is a non-supporting segment. This segment overall retains its original location and alignment. However, in response to changes in the surrounding area, it no longer conveys integrity of setting, feeling, or association with a twentieth-century farm-to-market road. Due to alterations to the street including widening, modernization, and the construction of the cloverleaf interchange where the segment meets I-270, this segment no longer retains integrity of design, materials, or workmanship. It is determined non-supporting of the eligibility of the overall resource.

(8) 5AM.3924.3 – U.S. Highway 6 (Vasquez Boulevard, CDOT Route 006H, map label 35)

Description

This segment of US 6 is approximately 3.49 miles long. At its south end it intersects with I-270 and travels north, where it gently curves to the northeast and joins I-76. Throughout the segment length, the asphalt-paved road consists of a divided highway. Between I-270 and E. 62nd Avenue it has three through lanes in each direction. Between E. 62nd Avenue and I-76 it has two through lanes in each direction. Metal guardrail separates the highway from bordering properties, and access to local arterials is readily available at numerous intersections throughout its length. From the I-270 interchange to a point approximately 0.52 mile north, US 6 and SH 2 are the same route. This segment travels through commercial and residential areas.



Figure 72. Looking southwest at US 6 (5AM.3924.3) near project's southern terminus at I-270

Historical summary

This feature is comprised of a section of asphalt-paved road and is associated with the U.S. Highway route that originally crossed the state of Colorado from Nebraska to Utah. The entire resource is approximately 261.46 miles long. US 6 in Colorado begins at the Nebraska border and enters the state at the town of Holyoke, travelling west through the eastern plains and continues through Commerce City, Denver, and Golden, then west until it reaches Mack, just east of the Utah border. The segment of the highway in northeastern Colorado (Phillips, Logan, Washington, and Morgan Counties) served as an early local farm-to-market road by providing the primary connection between Colorado and neighboring Nebraska and rail connections to the larger markets to transport goods.

Significance assessment

Overall, US 6 in Colorado possesses significance under *Criterion A* due to its role as an early farm-to-market road during the 1910s and 1920s at the local level. US 6 does not possess significance under *Criterion B*. Previous research did not indicate the use of innovative or important engineering design or construction techniques that would distinguish this highway from other roads, and it does not possess significance under *Criterion C*. The technology of road construction is well understood and documented and is unlikely to yield important information that cannot be discerned from archived plans and other records. Therefore, this road does not possess significance under *Criterion D*.

Segment 5AM.3924.3 does not retain historic integrity and is a non-supporting segment. A review of historic maps shows that alignment changes in Adams County and the Commerce City area have altered the route's ability to convey its integrity as a farm-to-market road and the rural setting of which it was once a part. The most impactful changes occurred sometime between 1957 and 1965, when the alignment of the segment changed. Another significant change around that period of time was the introduction of a cloverleaf interchange where the component encounters present-day I-270.

(9) 5AM.4101.2 – York Street (map label 5)

Description

This segment of York Street is approximately 1 mile long between E. 66th Avenue and E. 74th Avenue in North Washington, Adams County. The asphalt-paved road consists of a two-lane road between E. 66th Avenue and E. 67th Avenue and a four-lane road between I-270 and E. 74th Avenue following a north/south trajectory that passes through urban and suburban settings. Each lane measures approximately 12 feet wide. The segment crosses over I-270 north of E. 68th Avenue.



Figure 73. View of Segment 5AM.4101.2 at I-270. View facing south.

Historical summary

The confluence of Sand Creek and the South Platte River was first settled as an agricultural area that supported the growing city of Denver in the nineteenth century. Prior to the postwar period, Welby, Commerce City, and North Washington were small agricultural communities connected by farm-to-market roads. This segment of York Street was first built as a county road in the 1880s. Early farm maps show the entire length of the segment. From E. 66th Avenue, heading north to Clear Creek, the farm map shows the segment as Road No. 83 and from Clear Creek north as Road No. 22. The segment appears on the 1938 USGS Derby Quadrangle map as part of SH 224. The USGS 1965 Commerce City Quadrangle map names the segment as York Street.

Statement of Significance

The entire length of York Street is assumed significant under *Criterion A* as a farm-to-market road. The road is not associated with persons of significance under *Criterion B*. The road's design and construction appear to fall within the established standard practices of road design and construction of the time and

the road does not possess significance under *Criterion C*. The technology of road construction is well understood and documented and is unlikely to yield important information that cannot be discerned from archived plans and other records. Therefore, this road does not possess significance under *Criterion D*.

This segment of York Street does not retain historic integrity. This segment overall retains its original location and alignment. However, in response to changes in the surrounding area, it no longer conveys integrity of setting, feeling, or association with a twentieth-century farm-to-market road. Due to alterations to the street including widening and modernization, this segment no longer retains integrity of design, materials, or workmanship. It is determined non-supporting of the eligibility of the overall resource.

(10) 5AM.4119 – Sand Creek Junction (map label 25)

Description

Sand Creek Junction is an X-shaped railroad junction at the crossing of the Chicago, Burlington & Quincy (CB&Q)/Burlington Northern Santa Fe (BNSF) Brush Line (5AM.464.21) and the Denver Pacific (DP)/Union Pacific (UP) Greeley Line (5AM.472.41). Both rail lines are single-tracked at the junction, with standard-gauge rails, wood ties, and crushed rock ballast. The recorded resource is approximately 60 feet long and 20 feet wide.



Figure 74. Location of Sand Creek Junction beneath the westbound I-270 overpass indicated with blue arrow. View facing north. Jacobs, 2020.

Historical summary

Sand Creek Junction was constructed in 1882 at the crossing of the UP and CB&Q Railroads. Two additional rail lines, the UP Dent Branch (5AM.472.17) and the CB&Q Market Street Line (5AM.1298.3),

were constructed from Sand Creek Junction in the early twentieth century. In 1930 Conoco Oil and the Colorado Refining Company constructed oil refineries just south of Sand Creek Junction. The multiple railroad lines at Sand Creek Junction spurred industrial development in Commerce City after World War II.

Significance assessment

Sand Creek Junction is significant under *Criterion A* for its association with early railroad transportation in Colorado and twentieth century industrial development in Commerce City. It does not have association with significant persons under *Criterion B*, engineering significance under *Criterion C*, or significance for information potential under *Criterion D*. Sand Creek Junction retains integrity of setting, design, location, feeling, and association and is determined eligible to the National Register under *Criterion A*.

(11) 5AM.4121.1 – Union Pacific Railroad Siding at Suncor Energy (map label 22)

Description

This siding of the Union Pacific (UP) railroad is located between a point 350 feet north of E. 56th Avenue to its junction with the UP Greeley Line, approximately 600 feet north of the I-270 right-of-way. The resource is comprised of standard gauge railroad track with wood ties and crushed rock ballast. The total length is 5,008 feet long and the width is 15 feet along the entire segment. The segment crosses Sand Creek over a timber pile bridge (COMC-4A-01-RR/5AM.1416). 5AM.4121.1 is located approximately 45 feet west of Sand Creek Junction (5AM.4119).



Figure 75. Location of UP Siding 5AM.4121.1 at I-270, indicated with blue arrows. View facing northwest. Jacobs, 2020.

Historical summary

The UP Greeley Line was constructed in 1870. In 1930 Conoco Oil constructed a refinery at Sand Creek Junction to process oil from the Fort Collins-Wellington Field. The location was chosen due to the proximity to the UP and CB&Q rail lines and an available water source from Sand Creek. This siding was constructed c.1940 to provide access for the UP to the Conoco refinery.

Significance assessment

The UP Siding at Suncor Energy was evaluated as an individual linear resource because it has stronger association with the oil refineries constructed in the 1930s than the UP Greeley Line (1870) or the UP Dent Branch (1909). Sidings are generally considered minor elements associated with primary rail lines and do not convey significance on their own as they are only intended to travel short distances between two points. As such, 5AM.4121.1 is not significant under *Criterion A*. 5AM.4121.1 does not have association with significant persons under *Criterion B*, engineering significance under *Criterion C*, or significance for information potential under *Criterion D*. Due to lack of significance, this property is not eligible for inclusion in the National Register.

Consultant Work Product - Jacobs Engineering
-Not CDOT Approved-

5. Section 106 Effects Determinations and Section 4(f)

A. Summary of Section 106 Effects Determinations

All historic properties identified within the APE were assessed for potential adverse effects under Section 106 based on review of the conceptual-level project designs for the Proposed Action (see Project Description in Section 2.B). A project will have an adverse effect on a historic property if the project may alter any features that contribute to its eligibility for the National Register in a way that would diminish the integrity of the property. A finding of *Adverse Effect* requires additional work to resolve the adverse effect. The criteria for a finding of *Adverse Effect* to historic properties are specified in the Code of Federal Regulations, Title 36, Part 800, which reads, in part:

An adverse effect is found when an undertaking may alter, directly or indirectly, any of the characteristics of a historic property that qualify the property for inclusion in the National Register in a manner that would diminish the integrity of the property's location, design, setting, materials, workmanship, feeling, or association. Consideration shall be given to all qualifying characteristics of a historic property, including those that may have been identified subsequent to the original evaluation of the property's eligibility for the National Register. Adverse effects may include reasonably foreseeable effects caused by the undertaking that may occur later in time, be farther removed in distance or be cumulative. (See [36 CFR 800.5\(a\)\(1\)](#)).

If the project or undertaking will have effects on historic properties that do not meet these criteria, there may be a finding of *No Adverse Effect*. A finding of *No Adverse Effect* states that the direct or indirect effects stemming from a project are not sufficient to diminish the integrity of a historic property and therefore do not have potential to alter its determination of eligibility for the National Register. A finding of *No Adverse Effect* may also be appropriate if avoidance measures are taken to limit impacts to historic properties, such as selecting a different design alternative or making modifications to the project.

In the event that there are no historic properties identified within the APE, or the historic properties are found not eligible for the National Register, there is no potential for an adverse effect because the integrity or eligibility of the resources cannot be diminished. In these cases, the agency may make a finding of *No Historic Properties Affected*.

A total of 71 historic linear and non-linear properties built in 1976 or earlier, adjacent to I-270 in Commerce City and unincorporated Adams County, are present within the project APE. Of these, 51 were surveyed and recorded by Mead & Hunt and Kristi Miniello, and 20 were previously surveyed and determined officially not eligible. One architectural property, located at 6680 York Street, built in 1950, is located in the APE and is over 50 years old but was not surveyed because it is buffered from all work by surrounding properties and no direct or indirect impacts are anticipated for this property (it is noted on the APE maps with an asterisk).

Eleven linear properties were surveyed and recorded between 2019 and 2020. Of these, five segments support the overall eligibility of their respective resources and four segments do not support the overall eligibility of their resources. One linear resource is determined not eligible in its entirety (Union Pacific Railroad Siding at Suncor Energy). Sand Creek Railroad Junction, included with the linear properties, is determined eligible for listing in the National Register.

Section 5
Effects Determinations and Section 4(f)

Forty architectural properties were surveyed and recorded for the I-270 EA between 2019 and 2020, including 38 individual properties, one historic subdivision, and one bridge. Twenty-two individual architectural properties were previously surveyed within the APE and determined officially not eligible. Two of these previously surveyed properties that reached 50 years of age since the original survey were re-evaluated as not eligible during this project. One property is determined eligible (5701 Dexter Street, 5AM.4047), and one is determined as “Needs Data” (Suncor Energy, 5AM.4044) and is assumed eligible for the purposes of Section 106.

This section includes a summary of the Proposed Action and Section 106 effects determinations for the 12 eligible and assumed eligible properties listed below. The Proposed Action would have *No Adverse Effect* on these properties.

- 5AM.464.21: Chicago Burlington & Quincy/Burlington Northern Santa Fe Railroad Segment—Brush Line (map label 26)
- 5AM.465.9: Burlington Ditch/O’Brien Canal (map label 18)
- 5AM.472.41: Denver Pacific/Union Pacific Railroad Segment—Greeley Line (map label 28)
- 5AM.1298.3: Chicago, Burlington & Quincy Market Street Line Segment (map label 30)
- 5AM.2410.2: Brighton Boulevard/State Highway 265 (map label 21)
- 5AM.4119: Sand Creek Junction (map label 27)
- 5AM.1292.2: Gardener’s Ditch Segment (map label 17)
- 5AM.3924.2: Vasquez Boulevard/U.S. Highway 6/U.S. Highway 85/State Highway 2 (map label 36)
- 5AM.3924.3: Vasquez Boulevard/U.S. Highway 6 (map label 35)
- 5AM.4101.2: York Street (map label 5)
- 5AM.4044: Suncor Energy (map label 20)
- 5AM.4047: Plastics, Inc. (map label 34)

A total of 59 properties were determined not eligible for National Register, including one linear property in its entirety and 48 previously and newly surveyed properties. For these properties the determination of effect is *No Historic Properties Affected*. For more information on these effects determinations, refer to Table 4, which includes a summary of right-of-way acquisitions, temporary easements, and permanent easements and map labels that correspond to the APE maps. Please also refer to the Right-of-way Acquisition or Easement Map in Appendix C, for additional details. The Proposed Action would not result in the Section 106 determinations of *Adverse Effect* for any of the properties.

Table 4. Eligibility and effects determinations and proposed right-of-way acquisitions and easements

Site No.	Site Name	Address/Location	Construction Date	National Register Determination (2020) / Prior Determination	Right-of-way Acquisitions, Temporary / Permanent Easements Within Historic Boundary (Total acreage of property) ⁵¹	Effects Determination	Map Label
Architectural Properties							
5AM.1363	Sand Creek Bridge - E-17-AT	Vasquez Boulevard/US 6	1940	Officially not eligible (2002)	None	No Historic Properties Affected	37
5AM.1416	Union Pacific Railroad Bridge - COMC-4A-02-RR	UP Suncor Siding over E. 60 th Avenue and Sand Creek	1938	Officially not eligible (2002)	None	No Historic Properties Affected	23
5AM.1417	Union Pacific Railroad Bridge - COMC-4A-02-RR DP/UP over E. 60 th Avenue and Sand Creek	DP/UP over E. 60 th Avenue and Sand Creek	1960	Officially not eligible (2002)	None	No Historic Properties Affected	24
5AM.1418	Burlington Northern Santa Fe Railroad Bridge - COMC-5A-03-RR	CB&Q/BNSF over E. 60 th Avenue and Sand Creek	1970	Officially not eligible (2002)	None	No Historic Properties Affected	25
5AM.1842	Kammerzell Residence	5360 Magnolia Street, Commerce City	1954	Officially not eligible (2007)	None	No Historic Properties Affected	62
5AM.1843	The Nationwide Fabrication, Inc.	5311 Niagara Street, Commerce City	1954	Officially not eligible (2007)	None	No Historic Properties Affected	63

⁵¹ Acreage for right-of-way acquisitions and temporary/permanent easements are approximate and are based on early, conceptual-level, plans.

Table 4. Eligibility and effects determinations and proposed right-of-way acquisitions and easements

Site No.	Site Name	Address/Location	Construction Date	National Register Determination (2020) / Prior Determination	Right-of-way Acquisitions, Temporary / Permanent Easements Within Historic Boundary (Total acreage of property) ⁵¹	Effects Determination	Map Label
5AM.1845	N/A	5410 Krameria Street, Commerce City	1947	Officially not eligible (2007)	None	No Historic Properties Affected	48
5AM.1846	N/A	6470 E 53 rd Way, Commerce City	1954	Officially not eligible (2007)	None	No Historic Properties Affected	56
5AM.1847	The Barraza Residence	6320 E 54 th Avenue, Commerce City	1939	Officially not eligible (2007)	None	No Historic Properties Affected	49
5AM.1848	The Vilchis Residence	6330 E 54 th Avenue Commerce City	1944	Officially not eligible (2007)	None	No Historic Properties Affected	50
5AM.1849	Overnite Transportation Company Business	5200 E 56 th Avenue, Commerce City	1962	Not eligible (2020)/Officially not eligible (2007)	0.52 acre temporary easement (20.24 acres)	No Historic Properties Affected	43
5AM.1850	The Beco Equipment Co.	5555 Dahlia Street, Commerce City	1960	Not eligible 2020)/Officially not eligible (2007)	0.238 acre permanent easement; 0.126 acre temporary easement (3.56 acres)	No Historic Properties Affected	38
5AM.1891	Doyle House - Beals Residence	6820 E 52 nd Place, Commerce City	1954	Officially not eligible (2007)	0.007 acre temporary easement (0.13 acre)	No Historic Properties Affected	66
5AM.1892	Quintana Residence	6830 E. 52 nd Place, Commerce City	1952	Officially not eligible (2007)	0.006 acre temporary easement (0.161 acres)	No Historic Properties Affected	67
5AM.1893	N/A	6411 E. 53 rd Way, Commerce City	1953	Officially not eligible (2007)	None	No Historic Properties Affected	52

Table 4. Eligibility and effects determinations and proposed right-of-way acquisitions and easements

Site No.	Site Name	Address/Location	Construction Date	National Register Determination (2020) / Prior Determination	Right-of-way Acquisitions, Temporary / Permanent Easements Within Historic Boundary (Total acreage of property) ⁵¹	Effects Determination	Map Label
5AM.1894	Callahan House - Gonzales Residence	6415 E. 53 rd Way, Commerce City	1954	Officially not eligible (2007)	None	No Historic Properties Affected	54
5AM.1895	Titsworth House - Boren Residence	6481 E. 53 rd Way, Commerce City	1954	Officially not eligible (2007)	None	No Historic Properties Affected	55
5AM.1899	Funk House - Klingbell Residence - Klingbell Trucking Co.	5431 Krameria Street, Commerce City	1951	Officially not eligible (2007)	None	No Historic Properties Affected	45
5AM.1904	Johnson Residence	5401 Locust Street, Commerce City	1955	Officially not eligible (2007)	None	No Historic Properties Affected	51
5AM.1905	Berk Residence	5350 Magnolia Street, Commerce City	1952	Officially not eligible (2007)	None	No Historic Properties Affected	59
5AM.1906	Morse Residence - Cruz Residence	5366 Magnolia Street, Commerce City	1952	Officially not eligible (2007)	None	No Historic Properties Affected	61
5AM.1907	Ray Residence - Oleson Residence	5360 Monaco Street, Commerce City	1954	Officially not eligible (2007)	None	No Historic Properties Affected	57

Table 4. Eligibility and effects determinations and proposed right-of-way acquisitions and easements

Site No.	Site Name	Address/Location	Construction Date	National Register Determination (2020) / Prior Determination	Right-of-way Acquisitions, Temporary / Permanent Easements Within Historic Boundary (Total acreage of property) ⁵¹	Effects Determination	Map Label
5AM.4031	Larusso Subdivision	E. 68 th Place to the north, I-270 to the east, E. 68 th Avenue to the south, and the back edge of the properties fronting Race Street to the west. Adams County	1942-1970	Not eligible (2020)	None	No Historic Properties Affected	2
5AM.4032	Sherwood House	2101 E. 68 th Avenue, Adams County	1944	Not eligible (2020)	None	No Historic Properties Affected	3
5AM.4034	Teegee & Company, LLC	2180 E. 68 th Avenue, Adams County	1922	Not eligible (2020)	None	No Historic Properties Affected	4
5AM.4035	Di Giacomo-Slagel House	6820 York Street, Adams County	c.1955	Not eligible (2020)	None	No Historic Properties Affected	8
5AM.4036	King House	6776 York Street, Adams County	c.1905	Not eligible (2020)	None	No Historic Properties Affected	9
5AM.4037	King House	6780 York Street, Adams County	c.1900	Not eligible (2020)	None	No Historic Properties Affected	10
5AM.4038	Weland House	6750 York Street, Adams County	c.1925	Not eligible (2020)	0.053 acre acquisition; 0.106 temporary acre easement (0.46 acre)	No Historic Properties Affected	11

Table 4. Eligibility and effects determinations and proposed right-of-way acquisitions and easements

Site No.	Site Name	Address/Location	Construction Date	National Register Determination (2020) / Prior Determination	Right-of-way Acquisitions, Temporary / Permanent Easements Within Historic Boundary (Total acreage of property) ⁵¹	Effects Determination	Map Label
5AM.4039	Chapman-Depinto House	6702 York Street, Adams County	1963	Not eligible (2020)	0.012 acre acquisition; 0.109 acre permanent easement; 0.03 temporary easement (0.94 acre)	No Historic Properties Affected	12
5AM.4040	Royston House	6700 York Street, Adams County	1955	Not eligible (2020)	0.076 acre permanent easement (1.84 acres)	No Historic Properties Affected	13
5AM.4041	NA	6690 York Street, Adams County	1953	Not eligible (2020)	0.051 acre temporary easement (0.6 acre)	No Historic Properties Affected	14
5AM.4042	Chaplinskiy House	6610 York Street, Adams County	c.1910	Not eligible (2020)	0.011 acre temporary easement (1.87 acres)	No Historic Properties Affected	16
5AM.4043	Kuhns House	6620 York Street, Adams County	1905	Not eligible (2020)	0.012 acre temporary easement (1.76 acres)	No Historic Properties Affected	15
5AM.4044	Suncor Energy	3875 E. 56 th Avenue 5800 Brighton Road 5801 Brighton Boulevard Commerce City	1930, c.1937, 1949, 1953, c.1978	Needs data/Treat as eligible (2020)	0.252 acre temporary easement (45.67 acres)	No Adverse Effect	20
5AM.4045	Denver Oil	4100 E. 60 th Avenue, Commerce City	1922	Not eligible (2020)	0.071 acre temporary easement (1.37 acres)	No Historic Properties Affected	29

Table 4. Eligibility and effects determinations and proposed right-of-way acquisitions and easements

Site No.	Site Name	Address/Location	Construction Date	National Register Determination (2020) / Prior Determination	Right-of-way Acquisitions, Temporary / Permanent Easements Within Historic Boundary (Total acreage of property) ⁵¹	Effects Determination	Map Label
5AM.4046	Domenico Transportation Services Building	5699 Dexter Street, Commerce City	1964	Not eligible (2020)	0.307 acre acquisition; 0.154 acre temporary easement (3.87 acres)	No Historic Properties Affected	33
5AM.4047	Plastics, Inc.	5701 Dexter Street, Commerce City	c.1947	Eligible (2020)	None	No Adverse Effect	34
5AM.4048	Marquez House	6860 E. 52 nd Place, Commerce City	1966	Not eligible (2020)	0.007 acre temporary easement (0.41 acre)	No Historic Properties Affected	68
5AM.4049	Duran House	5251 Oneida Street, Commerce City	1955	Not eligible (2020)	0.002 acre temporary easement (0.14 acre)	No Historic Properties Affected	70
5AM.4050	Applegarth House	5250 Oneida Street, Commerce City	1955	Not eligible (2020)	None	No Historic Properties Affected	71
5AM.4070	Corner Stone Truck Repair	5790 Dahlia Street, Commerce City	c.1966	Not eligible (2020)	0.016 acre temporary easement (2.11 acres)	No Historic Properties Affected	40
5AM.4107	NA	7141 Lafayette Street, Adams County	1976	Not eligible (2020)	0.017 acre temporary easement (0.96 acre)	No Historic Properties Affected	1
5AM.4108	NA	6875 Columbine Street, Adams County	1950-1959	Not eligible (2020)	None	No Historic Properties Affected	6
5AM.4109	Martinez House	6824 Columbine Street, Adams County	1937	Not eligible (2020)	None	No Historic Properties Affected	7

Table 4. Eligibility and effects determinations and proposed right-of-way acquisitions and easements

Site No.	Site Name	Address/Location	Construction Date	National Register Determination (2020) / Prior Determination	Right-of-way Acquisitions, Temporary / Permanent Easements Within Historic Boundary (Total acreage of property) ⁵¹	Effects Determination	Map Label
5AM.4110	Semmaterials Energy Partners, LLC.	4310 E. 60 th Avenue, Commerce City	1953	Not eligible (2020)	0.087 acre temporary easement (5.08 acres)	No Historic Properties Affected	32
5AM.4111	United Asphalts, Inc.	4306 E. 60 th Avenue, Commerce City	1965	Not eligible (2020)	0.008 acre temporary easement (0.22 acre)	No Historic Properties Affected	31
5AM.4112	Stewart & Stevenson Power Products	5820 Dahlia Street, Commerce City	1946	Not eligible (2020)	None	No Historic Properties Affected	39
5AM.4113	Western Material Handling	5678 Eudora Street, Commerce City	c.1960	Not eligible (2020)	0.14 acre temporary easement (1.69 acres)	No Historic Properties Affected	42
5AM.4114	Wagner Rents: The CAT Rental Store	5455 Holly Street, Commerce City	1973	Not eligible (2020)	0.04 acre temporary easement (3.82 acres)	No Historic Properties Affected	44
5AM.4115	E&K Trucking, Inc.	5421 Krameria Street, Commerce City	1975	Not eligible (2020)	None	No Historic Properties Affected	46
5AM.4116	Masek Golf Cars	5345 Newport Street, Commerce City	1973	Not eligible (2020)	None	No Historic Properties Affected	64
5AM.4117	Denver Truck & Trailer	5280 Newport Street, Commerce City	1974	Not eligible (2020)	None	No Historic Properties Affected	65

Table 4. Eligibility and effects determinations and proposed right-of-way acquisitions and easements

Site No.	Site Name	Address/Location	Construction Date	National Register Determination (2020) / Prior Determination	Right-of-way Acquisitions, Temporary / Permanent Easements Within Historic Boundary (Total acreage of property) ⁵¹	Effects Determination	Map Label
5AM.4118	E. 64 th Avenue Bridge over Burlington Ditch/O'Brien Canal	NA	c.1910-1930	Not eligible (2020)	Bridge may be used for recreational trail detour; guardrails may be added	No Historic Properties Affected	19
5AM.4122	Keller	5665 Eudora Street 5775 Eudora Street 5675 Eudora Street Commerce City	1967	Not eligible (2020)	0.221 acre acquisition; 0.169 acre temporary easement (3.75 acres)	No Historic Properties Affected	41
5AM.4144	NA	5450 Krameria Street, Commerce City	1960	Not eligible (2020)	None	No Historic Properties Affected	47
5AM.4145	Hernandez House	5400 Locust Street, Commerce City	1954	Not eligible (2020)	None	No Historic Properties Affected	53
5AM.4146	Colombo House	5401 Magnolia Street, Commerce City	c.1935	Not eligible (2020)	None	No Historic Properties Affected	58
5AM.4147	NA	5358 Magnolia Street, Commerce City		Not eligible (2020)	None	No Historic Properties Affected	60
5AM.4148	Duran House	6870 E. 52 nd Place, Commerce City	1955	Not eligible (2020)	None	No Historic Properties Affected	69
N/A	JVO Upholstery Services	6680 York Street, Adams County	1950	N/A - Not surveyed	None	N/A	*

Table 4. Eligibility and effects determinations and proposed right-of-way acquisitions and easements

Site No.	Site Name	Address/Location	Construction Date	National Register Determination (2020) / Prior Determination	Right-of-way Acquisitions, Temporary / Permanent Easements Within Historic Boundary (Total acreage of property) ⁵¹	Effects Determination	Map Label
Linear Properties (Including Bridge)							
5AM.464.21	Chicago Burlington & Quincy/Burlington Northern Santa Fe Railroad Segment—Brush Line	Between E. 56 th Avenue and I-270 in Commerce City	1881-1882	Eligible – Supporting segment (2020); Overlaps with Supporting segments 5AM.464.16 and 5AM.464.17; Includes bridge 5AM.1418 (not eligible, 2002)	None; Possible Right-of-Entry or Access Agreement may also be required	No Adverse Effect	26
5AM.465.9	Burlington Ditch/O'Brien Canal	Between South Platte River near Franklin Street and E. 74 th Avenue in Commerce City	1886	Eligible – Supporting segment (2020)/ Overall resource officially eligible/supporting, 2017	0.18 acre temporary encroachment to the west; 0.17 acre temporary encroachment to the east (0.35 acre)	No Adverse Effect	18

Table 4. Eligibility and effects determinations and proposed right-of-way acquisitions and easements

Site No.	Site Name	Address/Location	Construction Date	National Register Determination (2020) / Prior Determination	Right-of-way Acquisitions, Temporary / Permanent Easements Within Historic Boundary (Total acreage of property) ⁵¹	Effects Determination	Map Label
5AM.472.41	Denver Pacific/Union Pacific Railroad Segment—Greeley Line	Between E. 56 th Avenue and I-270 in Commerce City	1870	Eligible – Supporting segment (2020); Overlaps with Supporting Segments 5AM.472.17 and 5AM.472.21; Includes bridge 5AM.1417 (not eligible, 2002)	None; Possible Right-of-Entry or Access Agreement may also be required	No Adverse Effect	28
5AM.1292.2	Gardener's Ditch Segment	Between 6610 York Street and 6750 York Street in Adams County	1861	Eligible – Non-supporting segment (2020)/ Overall resource officially eligible 1999	0.095-acre temporary easement	No Adverse Effect	17
5AM.1298.3	Chicago, Burlington & Quincy Market Street Line Segment	Between E. 56 th Avenue and E. 60 th Avenue	1911-1913	Eligible – Supporting segment (2020); Overlaps with Supporting segment 5AM.1298.1	None; Possible Right-of-Entry or Access Agreement may also be required	No Adverse Effect	30

Table 4. Eligibility and effects determinations and proposed right-of-way acquisitions and easements

Site No.	Site Name	Address/Location	Construction Date	National Register Determination (2020) / Prior Determination	Right-of-way Acquisitions, Temporary / Permanent Easements Within Historic Boundary (Total acreage of property) ⁵¹	Effects Determination	Map Label
5AM.2410.2	Brighton Boulevard/SH 265	Approx. 441 feet SW from the I-270 overpass ending at the junction with US 6/US 85 in Commerce City	1880s	Eligible – Supporting segment (2020); Overlaps with Supporting segment 5AM.2410.1	None; Possible Right-of-Entry or Access Agreement may also be required	No Adverse Effect	21
5AM.3924.2	Vasquez Boulevard/U.S. Highway 6/U.S. Highway 85/State Highway 2	Between E. 56 th Avenue and I-270 interchange in Commerce City	1932-1938	Eligible – Non-supporting segment (2020); Includes bridge 5AM.1363 (not eligible, 2020)	Total area of impact for both segments: 6.86 Acres	No Adverse Effect	36
5AM.3924.3	U.S. Highway 6 (Vasquez Boulevard, CDOT Route 006H)	Between I-270 interchange and I-76 in Commerce City	1932-1938	Eligible – Non-supporting segment (2020)	Total area of impact for both segments: 6.86 Acres	No Adverse Effect	35
5AM.4101.2	York Street	York Street between E. 66 th Avenue and 74 th Avenue in Adams County	1880s	Eligible – Non-supporting segment (2020)	Area of impact 1.71 acres	No Adverse Effect	5
5AM.4119	Sand Creek Junction	Crossing point of CB&Q/BNSF Brush Line and DP/UP Greeley Line in Commerce City	1882	Eligible; Re-recording of 5AM.1321.1 (2020)	None; Possible Right-of-Entry or Access Agreement may also be required	No Adverse Effect	27

Table 4. Eligibility and effects determinations and proposed right-of-way acquisitions and easements

Site No.	Site Name	Address/Location	Construction Date	National Register Determination (2020) / Prior Determination	Right-of-way Acquisitions, Temporary / Permanent Easements Within Historic Boundary (Total acreage of property) ⁵¹	Effects Determination	Map Label
5AM.4121.1	Union Pacific Railroad Siding at Suncor Energy	Between E. 56 th Avenue and I-270 in Commerce City	c.1930s-1940s	Not eligible – Non-supporting segment (2020); Includes bridge 5AM.1416 (not eligible, 2002)	None; Possible Right-of-Entry or Access Agreement may also be required	No Historic Properties Affected	22

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B. Description of effects

The following sections provide Section 106 effects determinations for historic properties resulting from the Proposed Action. For additional information on the Proposed Action, refer to Section 2, which includes the typical section for the roadway (Figures 1 and 2). The typical section for the bridges is provided here, which applies to the locations where bridges would be replaced with wider bridges over historic linear features. The typical section of these bridges would accommodate four 12-foot highway lanes in each direction (two general purpose lanes, one auxiliary lane, and one potential express or general purpose lane), a 4-foot to 8-foot buffer between the express lane and general purpose lane, a 4-foot to 12-foot inside shoulder, and a 10-22-foot outside shoulder for the Proposed Action. Please refer to Figure 76 for additional information.

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Section 5
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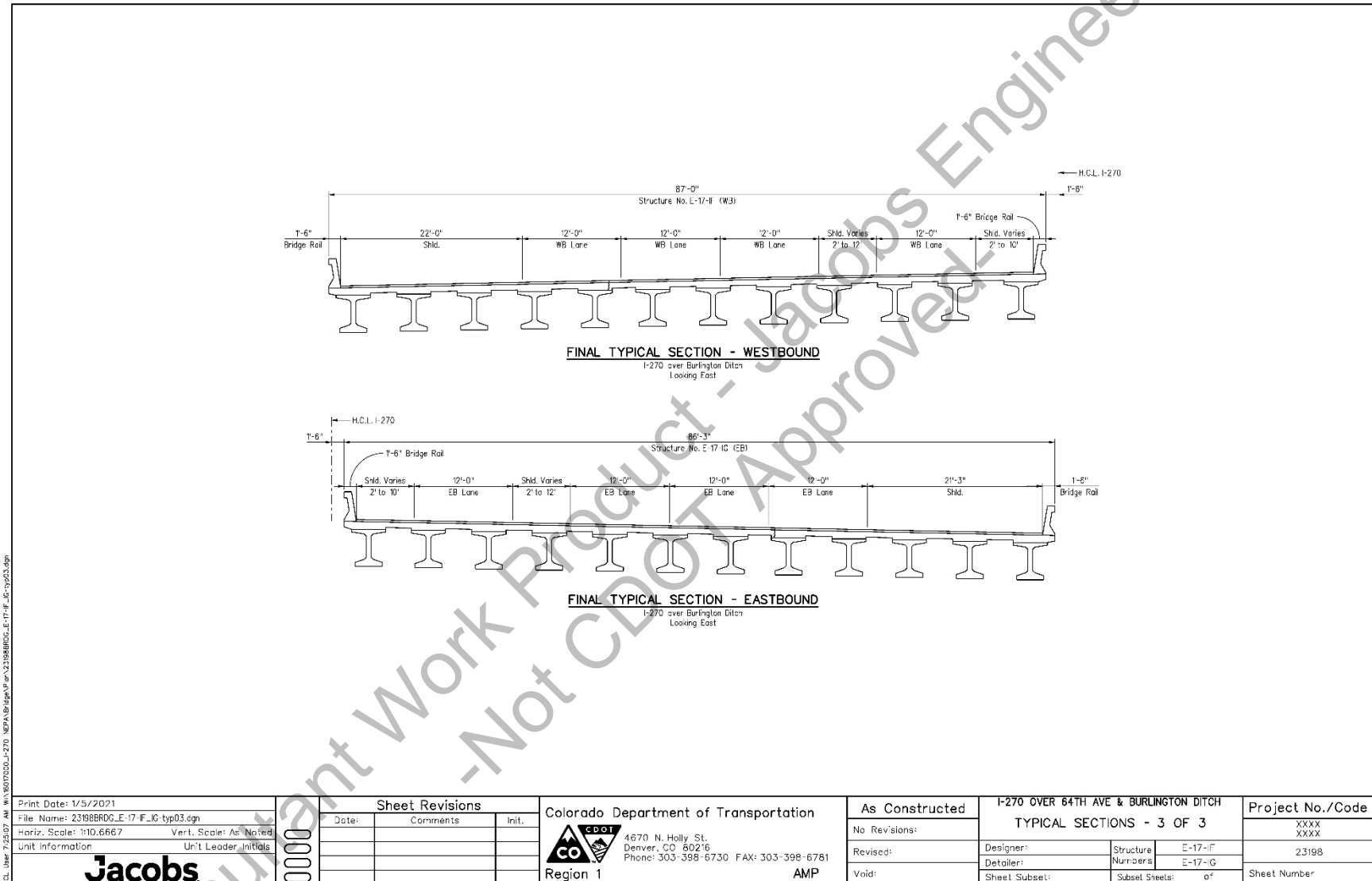


Figure 76. Final typical section of the proposed concrete girder spans over the Burlington Ditch/O'Brien Canal.

Print Date: 1/5/2021 File Name: 23198BRDC_E-17-F-IG-ty03.dgn Horiz. Scale: 1:10.6667 Vert. Scale: As Noted Unit Information: Unit Leader Initials Jacobs	Sheet Revisions Date: Comments Init.			Colorado Department of Transportation 4670 N. Holly St. Denver, CO 80216 Phone: 303-398-6730 FAX: 303-398-6781 Region 1 AMP		As Constructed No Revisions: Revised: Void:		I-270 OVER 64TH AVE & BURLINGTON DITCH TYPICAL SECTIONS - 3 OF 3 Designer: Structure Numbers E-17-F Detailer: E-17-G Sheet Subset: Subset Sheets: of		Project No./Code XXXX XXXX 23198 Sheet Number	

(1) 5AM.1292.2 - Gardener's Ditch Segment (map label 17)

The Gardener's Ditch was originally constructed between 1861 and 1863. Public Service Company of Colorado (PSCo) purchased and abandoned this segment of the ditch in the early 2000s. It is located at I-270 just east of York Street between the south property line of 6610 York Street and the northern right-of-way of I-270. The total length is 865 feet and the width is 6 feet, or 5,190 square feet. The ditch is located in an underground pipe under the I-270 highway and right-of-way.

The Gardener's Ditch is significant under *Criterion A* for its association with early irrigation and agriculture in Denver. This segment has been abandoned and no longer retains integrity of setting, materials, design, workmanship, feeling, or association, and is not recognizable as an irrigation ditch. It was determined to be a non-supporting segment of the Gardener's Ditch.



Figure 77. Location of the former Gardener's Ditch alignment at the south I-270 right-of-way, adjacent to the eastbound entrance ramp from York Street, view facing southeast. An underground drainage pipes would cross the location of the Gardener's Ditch north and south of the I-270 mainline and an above ground drainage would cross the ditch south of the I-270 mainline, within the right-of-way.

Proposed improvements at the location of the Gardener's Ditch include alterations to the interchange at I-270 and York Street. The westbound I-270 ramp would undergo a minor (15-foot) shift to the north, flattening the curve radius and allowing for expansion of the I-270 mainline. The eastbound I-270 ramp would be lengthened 1,050 feet then merged into the adjacent auxiliary lane. Widening of the I-270 mainline would require construction over this segment of the Gardener's Ditch and may require removal of the current culvert beneath I-270 (removal of approximately 1,600 square feet, or 345 feet by 4.52 feet of the segment, which totals 5,190 square feet). In addition, underground drainage pipes would be constructed within the I-270 right-of-way to the north and south of the mainline, crossing the trajectory of this segment of the Gardener's Ditch. An aboveground drainage ditch would also be constructed to the south of the I-270 mainline, cutting through the trajectory of this segment of the Gardener's Ditch. A small

temporary easement of 0.095 acre is required on the south side of I-270 to realign the York Street Entrance Ramp to eastbound I-270. See Figure 78 for details.

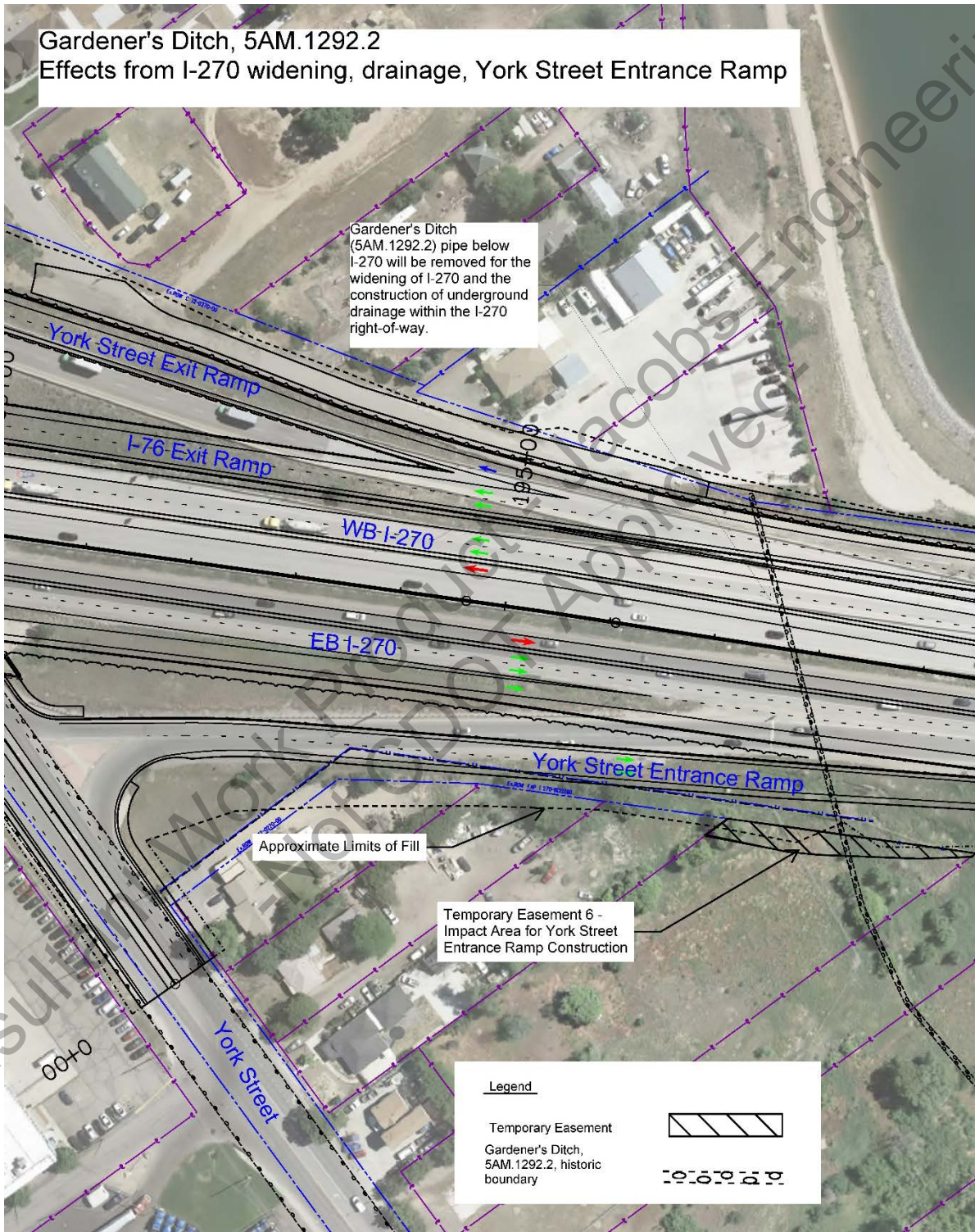


Figure 78. Effects of the project on the Gardener's Ditch segment 5AM.1292.2.

Due to the abandonment and subsequent loss of integrity this segment does not support the overall National Register eligibility of the Gardener's Ditch. As a result, the project would result in a *No Adverse Effect* determination to the Gardener's Ditch (5AM.1292, which includes the non-supporting segment 5AM.1292.2).

(2) 5AM.4101.2 – York Street (map label 5)

York Street was first built as a county road in the 1880s and was designated as SH 224 in the early twentieth century, although it no longer carries this designation. This segment was widened after construction of I-270 in the 1960s. This segment is located between E. 66th Avenue and E. 74th Avenue and crosses I-270 southeast of I-76. The length is approximately 1 mile and the width is 70 feet.

The entire length of York Street is assumed significant under *Criterion A* as a farm-to-market road. However, this segment of York Street does not retain historic integrity. In response to changes in the surrounding area and physical alterations, it no longer conveys integrity of setting, design, materials, workmanship, feeling, or association with a twentieth-century farm-to-market road. It is determined to be a non-supporting segment of York Street.



Figure 79. View of Segment 5AM.4101.2 at I-270, view facing south. York Street would be widened by 36 feet to accommodate new travel lanes and ramps connecting to I-270 would be altered.

Proposed improvements include alterations to the interchange at I-270 and York Street. For a distance of 1,000 feet where it crosses over I-270, York Street would be widened by approximately 36 feet to accommodate four (4), 12-foot travel lanes with 16-foot center turn lane median and an attached 10' wide multi-use path along the east side and 5' attached sidewalk along the west side. This wider York Street template would tie into improvements currently planned as a separate project as part of the Adams County Transportation Plan. The westbound I-270 ramp would undergo a minor (15-foot) shift to the

north, flattening the curve radius and allowing for expansion of the I-270 mainline. The eastbound I-270 ramp would be lengthened 1,050 feet then merged into the adjacent auxiliary lane. These proposed improvements would result in a total impact area of 1.71 acres. Due to the changes in setting and design, resulting in a loss of integrity, this segment does not support the overall National Register eligibility of York Street. As a result, the project would result in a *No Adverse Effect* determination to York Street (5AM.4101, which includes the non-supporting segment 5AM.4101.2).

(3) 5AM.465.9 – Burlington Ditch/O'Brien Canal (map label 18)

The Burlington Ditch was constructed in 1886 by the Burlington Ditch Company and is owned by the Farmer's Reservoir & Irrigation Company (FRICO). This ditch crosses I-270 southeast of the South Platte River. The 2.35-mile segment is approximately 123 feet wide at I-270, including the canal and historic ditch rider road to the east and a short gravel access road to the west. Within the segment is a concrete drop structure, 175 feet south of I-270, and a metal bridge crossing the canal at E. 64th Avenue (5AM.4118), 100 feet north of I-270. An elevated commuter rail line is located 70 feet east of the ditch. The canal embankment has been reinforced with large concrete and stone rubble through this area.

Segment 5AM.465.9 was previously evaluated and officially determined to be supporting of the overall eligibility of the Burlington Ditch/O'Brien Canal in 2009. The Burlington Ditch/O'Brien Canal is significant under *Criterion A* for its association with early irrigation in northeast Colorado. This segment supports the overall eligibility of the linear resource.



Figure 80. View of Burlington Ditch/O'Brien Canal at I-270, including elevated commuter rail line to the east (left), view facing south. The current I-270 bridges, which have bridge piers within the historic boundary, would be replaced with wider bridges that span the historic boundary with abutments outside of the boundary.

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Proposed improvements include replacing the two existing bridges (E-17-IF and E-17-IG) that span the Burlington Ditch/O'Brien Canal at this location with wider bridges. Please see Figure 81 and Figure 76 for details. The current two-span steel plate girder bridges (each approximately 40 feet wide) would be replaced with two single-span, concrete girder bridges that are each approximately 30 feet wider than the existing bridges. The longer span would require a greater structure depth for the proposed bridges. The structure depth of the new bridges would be 86 inches, as compared to the 55-inch structure depth of the current bridges. The current vertical clearance of 14.5 feet above the access roads would not change with the new bridges. The current piers within the historic boundary would be removed and not replaced as the new bridges span over the 123-foot, 2-inch historic boundary. Vertical abutment walls for the new bridges would be constructed outside of the historic boundary to avoid any direct impacts within the ditch boundary. The proposed design provides the required minimum 14-foot, 6-inch vertical clearance over the ditch and is below the higher vertical clearance of the commuter rail bridge on the east.

The concrete drop structure 175 feet south of I-270 would not be impacted. The ditch is a controlled flow structure so there is no need to make flood improvements, and no additional changes are proposed for the ditch. FRICO requires access to the service road on the west side of the ditch, which would be maintained as 20-foot wide gravel path. The shorter service road on the east would remain in its current condition with no additional improvements. The bridge over the ditch at 64th Avenue (5AM.4118) might be used as a detour for the recreational trail, which would include adding a new guardrail for pedestrian and bicyclist safety.

A temporary construction encroachment for right-of-entry to the ditch access roads would be required to remove the existing bridges and build the new bridges. These improvements would necessitate an encroachment of approximately 0.18-acre on the west side of the ditch, and 0.17-acre on the east side of the ditch, for a total of 0.35 acres in size of the 35-acre ditch property. Overall project construction is expected to take approximately two years. Work in the vicinity of the ditch would be temporary and will last between nine and twelve months, depending on project phasing. The property would be fully restored and returned to a condition that is at least as good as that which existed before construction. Refer to Figure 81 for more information.

The removal of the existing bridges would be conducted with minimal disturbance to the ditch embankments and no disturbance to the Burlington Ditch channel. The existing bridge pier columns would be removed to 1 foot below the natural ground surface then buried in accordance with CDOT Standard Specs Section 202.08 so that the existing bridge columns would no longer be visible. The bottom portions of the existing bridge columns as well as the existing bridge foundations (consisting of pile caps with driven piles) would be buried following structure removal. The removal of the existing bridges would be conducted from the ditch access roads with minimal disturbance to the ditch embankments and no disturbance to the channel, though a project special provision may be necessary to specify the specific requirements to protect the channel. The project specifications would include instructions for contractors that any additional changes or modifications to the ditch within the historic boundary would require SHPO consultation.

The removal of the existing bridge piers within the historic boundary would result in a direct effect to the ditch. Indirect effects would include placing the abutments for the two new bridges outside of the historic boundary. While there are changes to the setting of the historic ditch, the project would not change or

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alter the character-defining features of the ditch. The Burlington Ditch/O'Brien Canal has historically been crossed by several bridges and the current I-270 bridges have been in place since the 1960s. Replacing one set of non-historic bridges with another would not significantly impact the resource's integrity of setting, feeling, or association. Removal of the current piers may improve these aspects of integrity by removing a non-historic element from the historic boundary. There would be no change to the integrity of location, materials, design, or workmanship to the ditch. Because the historic integrity would not be significantly impacted, the ditch would still be able to convey its significance under Criterion A. As a result, the project would result in a *No Adverse Effect* determination to the Burlington Ditch/O'Brien Canal (5AM.465, which includes the supporting segment 5AM.465.9).

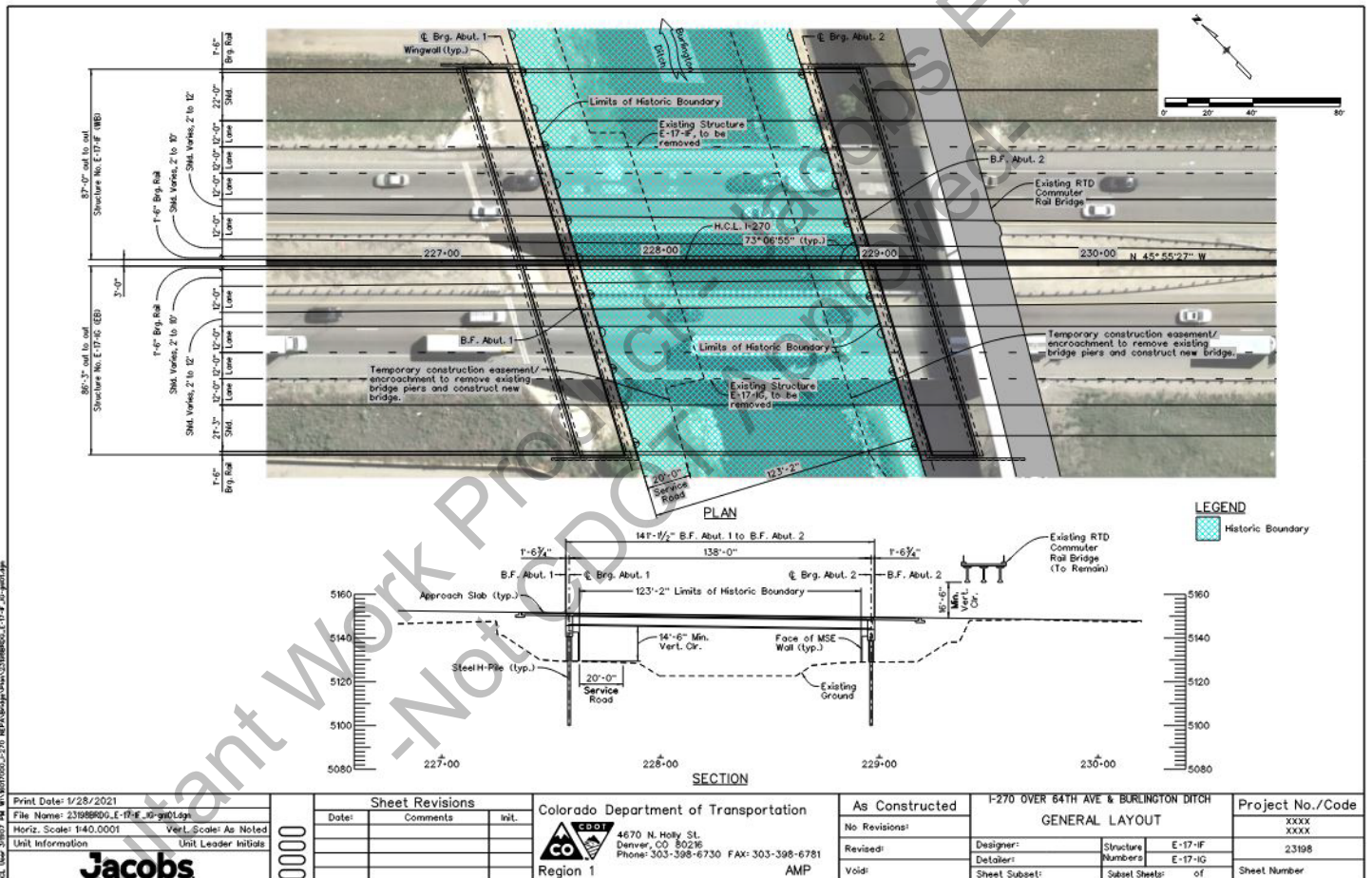


Figure 81. The plan view and cross section for the proposed improvements indicate the limits of the historic boundary of the Burlington Ditch (in blue) and the replacement bridges that would span the historic boundary, with piers placed outside of the historic boundary. The temporary easements are also depicted on this diagram.

(4) 5AM.2410.2 – Brighton Boulevard/State Highway 265 (map label 21)

Brighton Boulevard/SH 265 (Brighton Boulevard) was first built as a county road in the 1880s. This highway crosses I-270 west of the Union Pacific and Burlington Northern Santa Fe railroads at Sand Creek Junction. This segment is located between Sand Creek and the junction with US 6/US 85. The total length is approximately 1.25 miles and the width varies between approximately 51.5 feet and approximately 56 feet at I-270.

Brighton Boulevard/SH 265 is significant under *Criterion A* as a farm-to-market road. It was a primary transportation route that provided an important artery for farms and ranches in the Brighton area to markets and shipping centers in Denver. This segment supports the overall eligibility of the linear resource.



Figure 82. View of Brighton Boulevard/SH 265 at Colorado Boulevard looking at I-270, view facing southwest. The existing I-270 three-span bridge structures would be replaced with a wider, two-span, single structure that spans the highway right-of-way. No improvements would be made to the roadway within the historic boundary.

Proposed improvements include replacing the two bridges (E-17-IH and E-17-II) that span Brighton Boulevard/SH 265, and the CB&Q/BNSF and DP/UP railroads and Sand Creek Junction (see effect descriptions for these properties for additional information) at this location, with a single wider bridge. Please see Figure 83 for details. The current three-span steel plate girder bridges (each approximately 40 feet wide) would be replaced with a single two-span bridge approximately 50 feet wider than the existing bridges' combined width. The longer spans would require a greater structure depth, compared to the current 72-inch structure depth. There are two design options for the new bridge. Option 1 would replace the current bridges with a steel plate girder bridge with a 98-inch structure depth. Option 2 would replace the current bridges with a concrete girder bridge with a 111-inch structure depth. With both options, the two current bridge piers would be removed and replaced with a single pier located approximately 58 feet east of E. 60th Avenue. The current sloped bridge abutments would be replaced with vertical abutment walls at either end of the new bridge. The west abutment would be placed outside the historic boundary of

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Brighton Boulevard and may be set back from the roadway to provide space for future pedestrian access. The current vertical clearance of 16.5 feet from the roadway would remain unchanged.

The proposed action would not result in any improvements, including but not limited to widening or realignment, of this segment of Brighton Boulevard. Reconstruction of the west bridge abutment would require right-of-entry for construction access to remove the current slope paving and abutment. The construction of the new bridge would not have a direct effect to the highway segment because the new pier and bridge abutments would be outside of the historic boundary and no additional improvements to the roadway are included in the proposed action. There would be no change to the integrity of location, materials, design, or workmanship to the highway. Indirect effects would occur with the new bridge spanning the historic resource, but this would not change or alter the character-defining features of the highway. Brighton Boulevard/SH 265 has historically been crossed by other bridges and the current I-270 bridges have been in place since the 1960s. Replacing one set of non-historic bridges with another would not significantly impact the resource's integrity of setting, feeling, or association. Because the historic integrity would not be significantly impacted, the highway would still be able to convey its significance under *Criterion A*. As a result, the project would result in a *No Adverse Effect* determination to Brighton Boulevard/State Highway 265 (5AM.2410, which includes the supporting segment 5AM.2410.2).

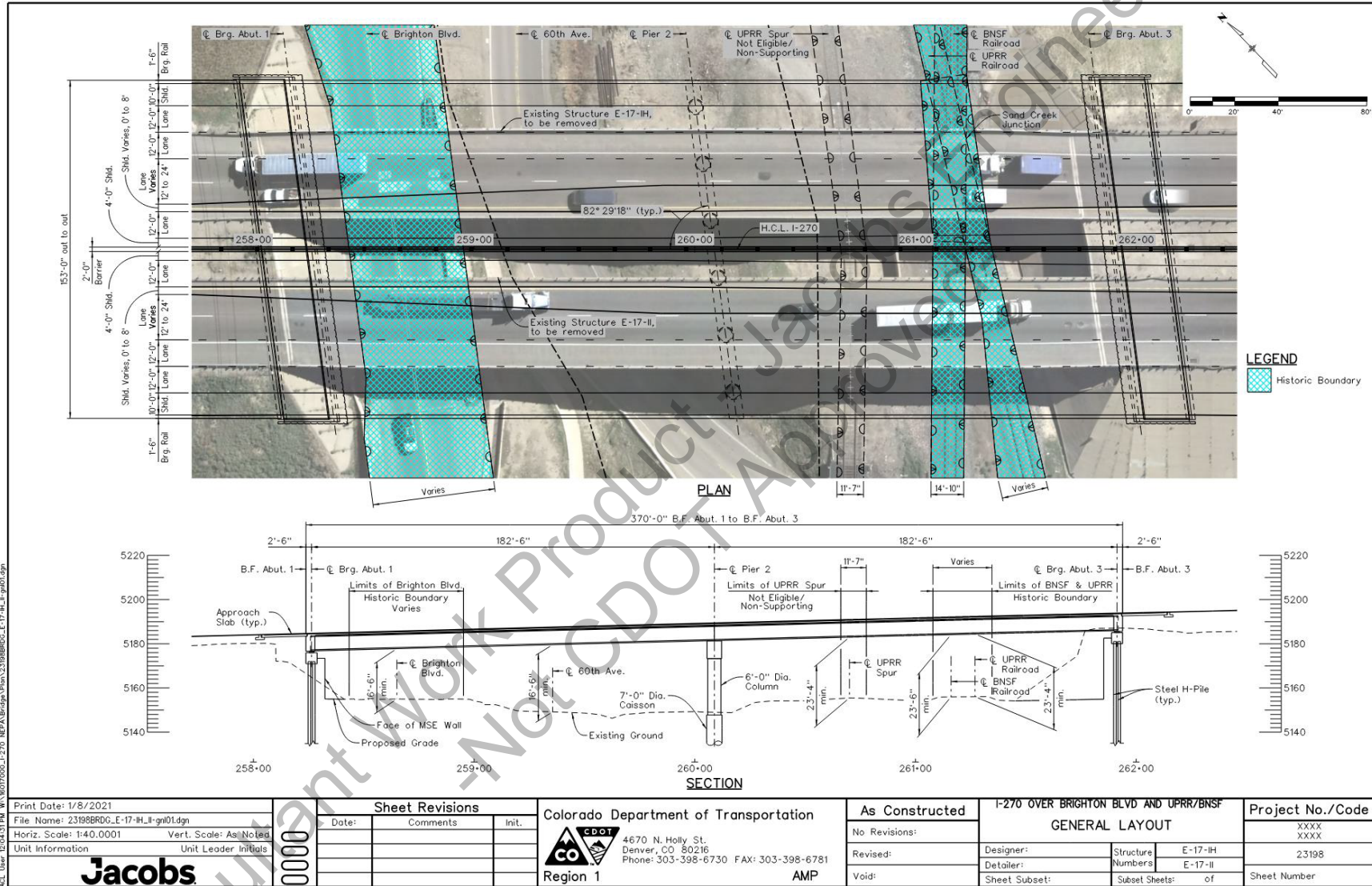


Figure 83. The plan view and cross section for the proposed improvements indicate the limits of the historic boundaries (from left to right) of Brighton Boulevard, the CBQ/BNSF, the DP/UP, and Sand Creek Junction (in blue) and the replacement bridges that would span the historic boundaries, with piers placed outside of the historic boundary.

(5) **5AM.464.21 – Chicago Burlington & Quincy/Burlington Northern Santa Fe Railroad
Segment—Brush Line (map label 26)**

The CB&Q Brush line was constructed in 1882 and is currently owned by the BNSF. This railroad crosses I-270 southeast of Brighton Boulevard/SH 265. This segment is located between E. 56th Avenue and a point approximately 250 feet north of the I-270 right-of-way. The total length is 4,488 feet and the width is approximately 15 feet along the entire segment. The segment also includes bridge COMC-SA-03-RR/5AM.1418 over Sand Creek, which was determined officially not eligible in 2002. It intersects the UP railroad (5AM.472.41) at Sand Creek Junction (5AM.4119), located directly beneath I-270.

The CB&Q/BNSF Brush line is significant under *Criterion A* for its association with early railroad development in Colorado. It provided the first direct rail connection between Denver and Chicago and influenced the growth of several towns in Colorado's Eastern Plains. This segment supports the overall eligibility of the linear resource.



Figure 84. View of CB&Q/BNSF Segment 5AM.464.21 at I-270, indicated with blue arrows, view facing northwest. The segment is approximately 15 feet wide, which includes the tracks and a narrow strip of ballast on either side. The existing I-270 three-span bridge structures would be replaced with a single wider, two-span structure that spans the railroad right-of-way, and the current pier (pictured) would be removed.

Proposed improvements include replacing the two bridges (E-17-IH and E-17-II) that span Brighton Boulevard/SH 265, and the CB&Q/BNSF and DP/UP railroads and Sand Creek Junction (see effect descriptions for these properties for additional information) at this location, with a single wider bridge. Please see Figure 76 for details. The current three-span steel plate girder bridges (each approximately 40 feet wide) would be replaced with a single two-span bridge approximately 50 feet wider than the existing bridges' combined width. The longer spans would require a greater structure depth, compared to the

current 72-inch structure depth. There are two design options for the new bridge. Option 1 would replace the current bridges with a steel plate girder bridge with a 98-inch structure depth. Option 2 would replace the current bridges with a concrete girder bridge with a 111-inch structure depth. With both options, the two current bridge piers would be removed and replaced with a single pier located approximately 58 feet east of E. 60th Avenue. The current sloped bridge abutments would be replaced with vertical abutment walls at either end of the new bridge. The west abutment would be placed outside the historic boundary of Brighton Boulevard and may be set back from the roadway to provide space for future pedestrian access. The current vertical clearance of 23.5 feet required by the railroads would remain unchanged.

The proposed action would not result in any changes or modifications, including but not limited to realignments, of this segment of the CB&Q/BNSF. Although it is not anticipated, a right-of-entry or other access agreement with BNSF may be necessary during the bridge construction to work above the railroad tracks within the historic boundary.

The construction of the new bridge would not have a direct effect to the railroad segment because the new pier and bridge abutments are outside of the historic boundary. There would be no change to the integrity of location, materials, design, or workmanship to the railroad. Indirect effects would occur with the new bridge spanning the historic resource, but this would not change or alter the character-defining features of the railroad. The current I-270 bridges have been in place since the 1960s. Replacing one set of non-historic bridges with another would not significantly impact the resource's integrity of setting, feeling, or association. Because the historic integrity would not be significantly impacted, the railroad would still be able to convey its significance under *Criterion A*. As a result, the project would result in a *No Adverse Effect* determination to the CB&Q/BNSF Brush line (5AM.464, which includes the supporting segment 5AM.464.21).

(6) 5AM.472.41 – Denver Pacific/Union Pacific Railroad Segment—Greeley Line (map label 28)

The DP railroad was constructed in 1870 and is currently owned by the UP railroad. This railroad crosses I-270 southeast of Brighton Boulevard/SH 265. This segment is located between E. 56th Avenue and a point approximately 650 feet north of the I-270 right-of-way. The total length is 4,120 feet and the width is approximately 15 feet at I-270. The segment also includes bridge COMC-4A-02-RR/5AM.1417 over Sand Creek, which was determined officially not eligible in 2002. The segment intersects the Burlington Northern Santa Fe (BNSF) Brush Line (5AM.464.21) at Sand Creek Junction (5AM.4119), located directly beneath I-270.

The DP/UP Greeley line is significant under *Criterion A* for its association with early railroad development in Colorado. It was the first railroad built in Denver and provided a connection to the UP Transcontinental Railroad in Cheyenne, Wyoming. This segment supports the overall eligibility of the linear resource.



Figure 85. View of DP/UP Segment 5AM.472.41 at I-270, indicated with blue arrows, view facing northwest. The segment is approximately 15 feet wide, which includes the tracks and a narrow strip of ballast on either side. The existing two I-270 three-span bridge structures would be replaced with a wider, single two-span structure that spans the railroad right-of-way, and the current pier (pictured) would be removed. Jacobs, 2020.

Proposed improvements include replacing the two bridges (E-17-IH and E-17-II) that span Brighton Boulevard/SH 265, and the CB&Q/BNSF and DP/UP railroads and Sand Creek Junction (see effect descriptions for these properties for additional information) at this location, with a single wider bridge. Please see Figure 76 for details. The current three-span steel plate girder bridges (each approximately 40 feet wide) would be replaced with a single two-span bridge approximately 50 feet wider than the existing bridges' combined width. The longer spans would require a greater structure depth, compared to the current 72-inch structure depth. There are two design options for the new bridge. Option 1 would replace the current bridges with a steel plate girder bridge with a 98-inch structure depth. Option 2 would replace the current bridges with a concrete girder bridge with a 111-inch structure depth. With both options, the two current bridge piers would be removed and replaced with a single pier located approximately 58 feet east of E. 60th Avenue. The current sloped bridge abutments would be replaced with vertical abutment walls at either end of the new bridge. The west abutment would be placed outside the historic boundary of Brighton Boulevard and may be set back from the roadway to provide space for future pedestrian access. The current vertical clearance of 23.5 feet required by the railroads would remain unchanged.

The proposed action would not result in any improvements, including but not limited to realignments, of this segment of the DP/UP. Although it is not anticipated, a right-of-entry or other access agreement with UP may be necessary during the bridge construction to work above the railroad tracks within the historic boundary.

The construction of the new bridge would not have a direct effect to the railroad segment because the new pier and bridge abutments are outside of the historic boundary. There would be no change to the integrity of location, materials, design, or workmanship to the railroad. Indirect effects would occur with the new bridge spanning the historic resource, but this would not change or alter the character-defining features of the railroad. The current I-270 bridges have been in place since the 1960s. Replacing one set of non-historic bridges with another would not significantly impact the resource's integrity of setting, feeling, or association. Because the historic integrity would not be significantly impacted, the railroad would still be able to convey its significance under *Criterion A*. As a result, the project would result in a *No Adverse Effect* determination to the DP/UP Greeley line (5AM.472, which includes the supporting segment 5AM.472.41).

(7) 5AM.4119 – Sand Creek Junction (map label 27)

Sand Creek Junction was constructed in 1882. It is an X-shaped railroad junction at the crossing of the CB&Q/BNSF Brush Line (5AM.464.21) and the DP/UP Greeley Line (5AM.472.41). The recorded resource is approximately 60 feet long and 20 feet wide.

Sand Creek Junction is significant under *Criterion A* for its association with early railroad transportation in Colorado and twentieth century industrial development in Commerce City. The junction connected the primary CB&Q and UP lines with local spurs and lead to industrial development in Commerce City after World War II.



Figure 86. View of Sand Creek Junction beneath the westbound I-270 overpass, indicated with blue arrow, view facing north. The resource is approximately 20 feet wide, which includes the track junction and narrow strips of ballast on either side. The two existing three-span bridge structures would be

replaced with a single wider, two-span structure that spans the railroad right-of-way, and the current pier (pictured) would be removed.

Proposed improvements include replacing the two bridges (E-17-IH and E-17-II) that span Brighton Boulevard/SH 265, and the CB&Q/BNSF and DP/UP railroads and Sand Creek Junction (see effect descriptions for these properties for additional information) at this location, with a single wider bridge. Please see Figure 76 for details. The current three-span steel plate girder bridges (each approximately 40 feet wide) would be replaced with a single two-span bridge approximately 50 feet wider than the existing bridges' combined width. The longer spans would require a greater structure depth, compared to the current 72-inch structure depth. There are two design options for the new bridge. Option 1 would replace the current bridges with a steel plate girder bridge with a 98-inch structure depth. Option 2 would replace the current bridges with a concrete girder bridge with a 111-inch structure depth. With both options, the two current bridge piers would be removed and replaced with a single pier located approximately 58 feet east of E. 60th Avenue. The current sloped bridge abutments would be replaced with vertical abutment walls at either end of the new bridge. The west abutment would be placed outside the historic boundary of Brighton Boulevard and may be set back from the roadway to provide space for future pedestrian access. The current vertical clearance of 23.5 feet required by the railroads would remain unchanged.

The proposed action would not result in any improvements, including but not limited to realignments, of the CB&Q/BNSF and DP/UP segments associated with Sand Creek Junction within the historic boundaries. Although it is not anticipated, a right-of-entry or other access agreement with UP and/or BNSF may be necessary during the bridge construction to work above the railroad tracks within the historic boundary.

The construction of the new bridge would not have a direct effect to the resource because the new pier and bridge abutments are outside of the historic boundary. There would be no change to the integrity of location, materials, design, or workmanship to the railroad. Indirect effects would occur with the new bridge spanning the historic resource, but this would not change or alter the character-defining features of the resource. The current I-270 bridges have been in place since the 1960s. Replacing one set of non-historic bridges with another would not significantly impact the resource's integrity of setting, feeling, or association. Because the historic integrity would not be significantly impacted, this railroad junction would still be able to convey its significance under *Criterion A*. As a result, the project would result in a *No Adverse Effect* determination to Sand Creek Junction (5AM.4119).

(8) 5AM.1298.3 – Chicago, Burlington & Quincy Market Street Line Segment (map label 30)

The CB&Q Market Street line was constructed between 1911 and 1913 and is currently owned by the Burlington Northern Santa Fe Railway. This railroad crosses I-270 northeast of Vasquez Boulevard/US 6/US 85/SH 2. This segment is located between E. 56th Avenue and E. 60th Avenue. The total length is 2,850 feet and the width is 30 feet at I-270. This segment also includes bridge 0.85 LS 904 over Sand Creek, that has not been previously evaluated for NRHP eligibility and is located outside the project's APE.

The CB&Q/BNSF Market Street line is significant under *Criterion A* for its association with early commerce and industry in Denver. It served as a connection between downtown Denver, the industrial

area northeast of the city, and the CB&Q Brush Line, which led to eastern Colorado and Midwestern markets. This segment supports the overall eligibility of the linear resource.



Figure 87. View of CB&Q/BNSF Segment 5AM.1298.3 at I-270, view facing south. The segment is 30 feet wide, which includes the double tracks and ballast on either side. The existing pair of three-span bridge structures would be replaced with a single wider, two-span structure that spans the railroad right-of-way.

Proposed improvements include replacing the two bridges (E-17-IJ and E-17-IK) that span I-270 at this location with a single wider bridge. Please see Figure 88 for details. The current pair of three-span steel plate girder bridges (each approximately 40 feet wide) would be replaced with a single two-span steel plate girder bridge approximately 40 feet wider than the existing bridges. The current bridge piers would be removed and replaced with a single pier located just east of the railroad rights-of-way and west of the E. 60th Avenue access road. The longer spans would require a greater structure depth of 105 inches, compared to the current structure depth of 57 inches. The current sloped bridge abutments would be replaced with either sloped or vertical abutment walls at either end of the new bridge. In order to meet BNSF requirements, all piers and abutments would be constructed outside of the railroads rights-of-way, which exceed the historic boundaries of this segment. The current vertical clearance of 23.5 feet required by BNSF would remain unchanged. There are no plans to alter E. 60th Avenue at this location, although it may be temporarily closed during construction. Bridge 0.85 LS 904 would not be affected.

The proposed action would not result in any improvements, including but not limited to realignments, of this segment of the CB&Q/BNSF. Although it is not anticipated, a right-of-entry or other access agreement with BNSF may be necessary during the bridge construction to work above the railroad tracks within the historic boundary.

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The construction of the new bridge would not have a direct effect to the railroad segment because the new pier and bridge abutments are outside of the historic boundary. There would be no change to the integrity of location, materials, design, or workmanship to the railroad. Indirect effects would occur with the new bridge spanning the historic resource, but this would not change or alter the character-defining features of the railroad. The current I-270 bridges have been in place since the 1960s. Replacing one set of non-historic bridges with another would not significantly impact the resource's integrity of setting, feeling, or association. Because the historic integrity would not be significantly impacted, this railroad junction would still be able to convey its significance under *Criterion A*. As a result, the project would result in a *No Adverse Effect* determination to the CB&Q/BNSF (5AM.1298, which includes the supporting segment 5AM.1298.3).

Consultant Work Product - Jacobs Engineering
-Not CDOT Approved-

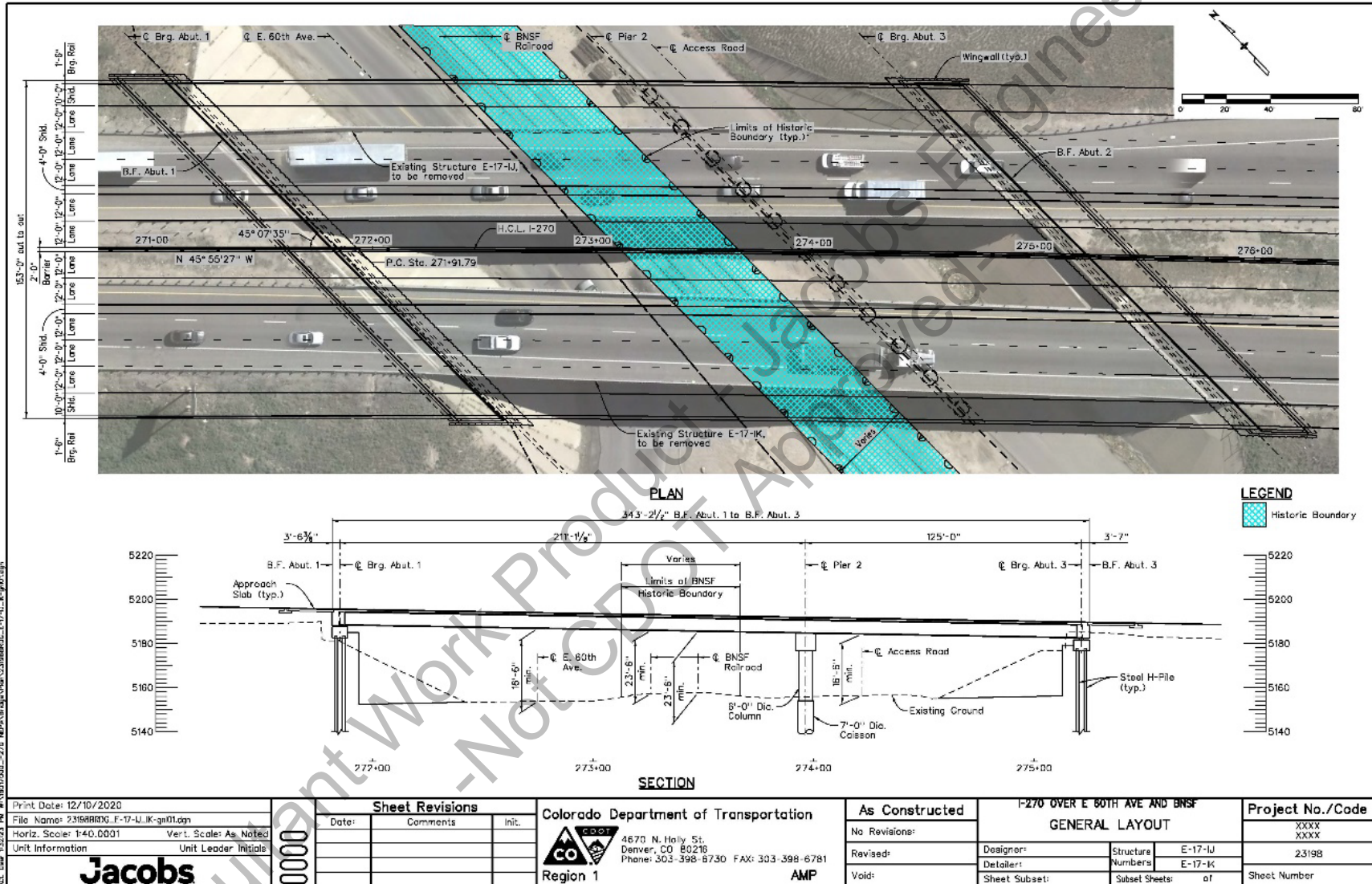


Figure 88. The plan view and cross section for the proposed improvements indicate the limits of the historic boundary of the CBQ Market Street Line segment (in blue) and the replacement bridge that would span the historic boundaries, with piers placed outside of the historic boundary.

(9) 5AM.4044 – Suncor Energy (map label 20)

Access to Suncor Energy was not permitted for field survey and therefore the description is largely based on current and historic aerial imagery. Suncor Energy is the conglomeration of two historic oil refineries that began operating on this site in 1930 and is the only operating oil refinery remaining in Colorado. The facility is spread out over three parcels (5800 Brighton Road, 5801 Brighton Boulevard, and 3875 East 56th Avenue) just south of Sand Creek, bisected by Brighton Boulevard, the BNSF Railroad, and the UP Railroad. All three parcels are dominated by catalytic cracking facilities, which are characterized by metal towers and chimneys several stories tall, connected by multiple pipelines, and supported by external metal frames. Within the cracking facilities there are several metal-frame utilitarian buildings. In addition, there are groupings of offices, garages, and other buildings located throughout the property. The remaining structures are cylindrical oil storage tanks, primarily located on the eastern portion of the property.

Suncor Energy is potentially eligible for listing in the National Register under *Criterion A: Industry* for its association with oil refining in Colorado and the commercial and industrial development of this part of Commerce City. The individual buildings at Suncor Energy were not surveyed and it is unknown if they possess any unique architectural features, although they generally appear to be utilitarian in nature and do not readily reflect a particular style, era, method of construction, or the work of a master. The cracking facilities appear to be composed of standard components and technology that are common to oil refineries. However, further research is needed to determine the presence or development of unique and innovative technologies, as well as the presence of older technologies that may convey significance related to petroleum refineries. Because a complete survey of the Suncor property was not possible at this time, it cannot be determined if the property possesses significance under *Criterion C*.

Because further research and survey is required to effectively determine National Register eligibility and integrity, Suncor Energy is determined as *Needs Data*. For the purposes of Section 106, the property is assumed eligible to the National Register.



Figure 89. Overview of Suncor Energy property from I-270. Photo includes the facilities at 5800 Brighton Road (left) and 5801 Brighton Boulevard (right). View facing south.

Proposed impacts to the Suncor property include a temporary easement (TE-93) of approximately 0.252 acre for the reconstruction of the Sand Creek Recreation Trail, located at the northern edge of the parcel at 3875 E. 56th Avenue. See Figure 90 and Figure 91 for details. Overall project construction is expected to take approximately two years. Work on the Suncor property to improve the trail would be temporary and is not expected to last more than six months. The proposed improvements would consist of minor grading, realignment of the trail, barrier removal and installation of new barriers to separate users of the trail from the road, culvert construction, and pavement restoration on E. 60th Avenue. The property would be fully restored and returned to a condition which is at least as good as that which existed before construction.



Figure 90. Location of Sand Creek Recreation Trail within Suncor Energy that would be improved and subject to a temporary easement (TE-93). View to the southeast.

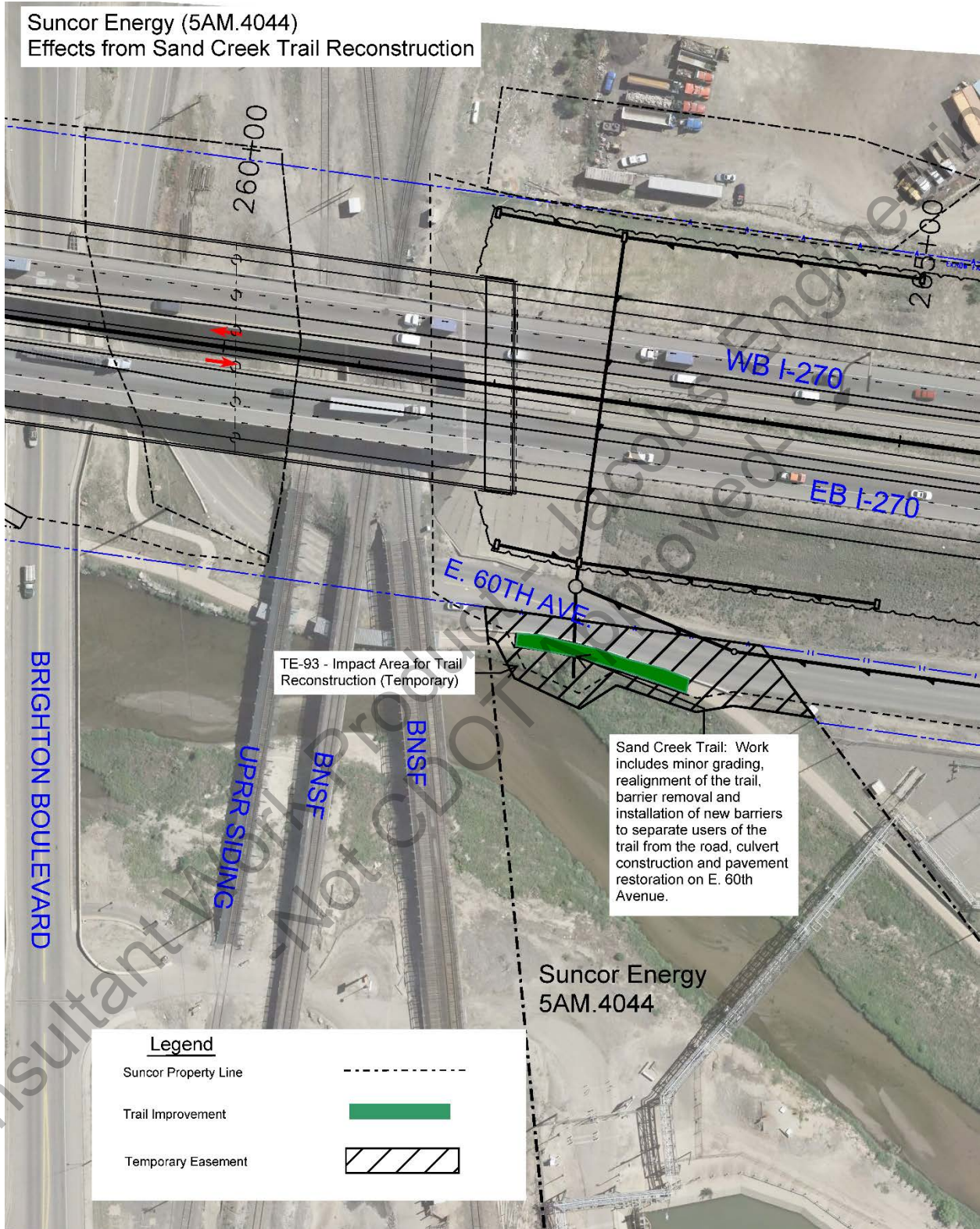


Figure 91. Location of temporary easement (TE-93) in relation to the historic boundary of Suncor Energy (5AM.4044) with description of proposed trail work.

The improvement of the recreational trail would have a temporary direct effect within a small portion of the historic boundary of the property, but this does not permanently change or modify the character-defining features of the property. No buildings, structures, or other character-defining features related to the Suncor refinery would be impacted, resulting in no potential to diminish the property's historic integrity. Suncor would still be able to convey its potential significance under *Criteria A* and *C*. As a result, the project would have *No Adverse Effect* to Suncor Energy (5AM.4044). For more details, please refer the Right-of-way Acquisition or Easement Map in Appendix B (inset 4 of 6, map label #20).

(10) 5AM.4047 – Plastics, Inc. (map label 34)

The building located at 5701 Dexter Street was constructed around 1947 as an industrial production shed by the Pepper Tank Company. The building has housed a series of manufacturing concerns since its construction around 1947, representing industrial and commercial growth trends in Commerce City in the decades following World War II. The property is surrounded by commercial properties to the north, west, and south, and is buffered from I-270 by these properties.

The building is a representative, intact, and early example of a postwar concrete block and metal manufacturing production shed in Commerce City. The manufacturing production shed at 5701 Dexter Street retains all of the character-defining features of this property type and is one of a small pool of surviving postwar production sheds in Commerce City. The building embodies the distinctive characteristics of a postwar manufacturing production shed and, as such, is determined eligible under *Criterion C*.



Figure 92. Plastics, Inc. building, front (east) facade and side (north) elevation. View facing southwest.

Proposed improvements in the vicinity of Plastics, Inc. include alterations to the ramp from southbound Vasquez Boulevard to westbound I-270. Alterations to southbound Vasquez Boulevard would include repaving and regrading, with the potential for guardrail replacement. All work would be limited to the east boundary of Dexter Street, with no alterations to Dexter Street including local business access to the Plastics property. Alterations the I-270 ramp would include widening the ramp and flattening the fill slopes, with potential for a new drainage ditch north of the ramp. The ramp height would remain approximately the same. Plastics, Inc. would be buffered from this work by the property at 5699 Dexter Street (5AM.4046 - not eligible). In addition, the northwest clover leaf ramp from westbound I-270 to southbound Vasquez Boulevard would be removed. See Figure 93 for details.

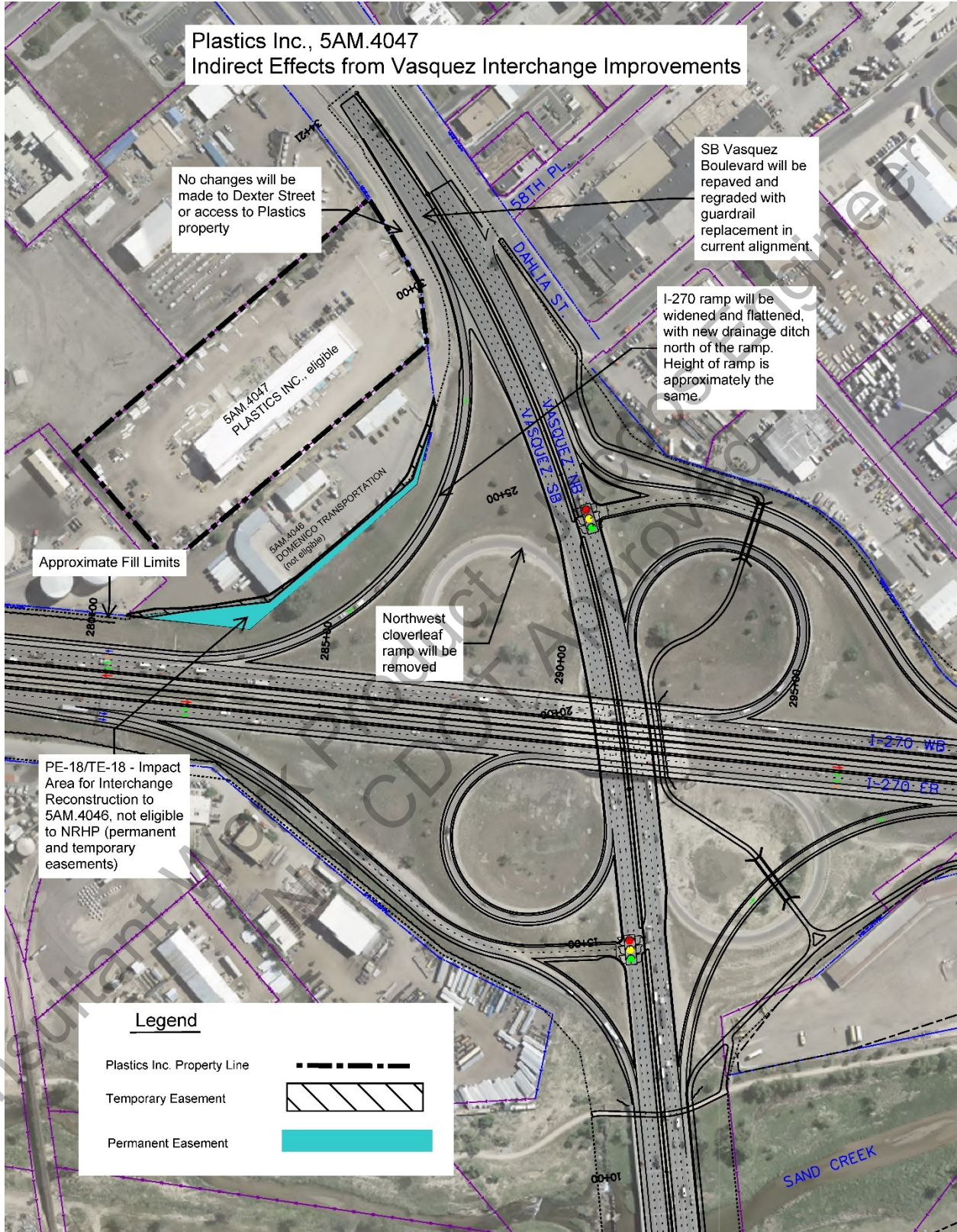


Figure 93. Effects of the project on Plastics, Inc., 5AM.4047.

There are no proposed direct effects to this property as no easements or acquisitions are required, access would not change, and the property would be buffered from I-270 by other parcels to the west and south. No buildings, structures, or other character-defining features related to the property would be impacted, resulting in no diminishment of the property's historic integrity. There would be an indirect effect to the setting but this does not change or modify any of the character-defining features of the property. Plastics, Inc. would still be able to convey its potential significance under *Criterion C*. As a result, the Section 106 determination is *No Adverse Effect*.

(11) 5AM.3924.2 – Vasquez Boulevard/U.S. Highway 6/U.S. Highway 85/State Highway 2 (map label (36))

Vasquez Boulevard/US 6/US 85/SH 2 originally functioned as a farm-to-market road providing local farmers access to the factories and railroads located in Denver and surrounding towns in the nineteenth and early twentieth centuries. This segment was widened multiple times in the mid-twentieth century, with the most significant change occurring between 1960 and 1965 when a cloverleaf interchange was built at the present-day junction with I-270. This segment crosses I-270 east of Brighton Boulevard/SH 265 and north of E. 56th Avenue. It is located between E. 52nd Avenue and I-270. The length is 4,073 feet and the width is 105 feet.

The section of US 6 in northeastern Colorado is significant under *Criterion A* as an early farm-to-market road that provided primary access for farmers, particularly in Phillips, Logan, Washington, and Morgan Counties, during the 1910s and 1920s at the local level. This segment has been widened and altered multiple times and no longer retains integrity of setting, design, materials, workmanship, feeling, or association as a farm-to-market road. It is determined to be a non-supporting segment of Vasquez Boulevard.



Figure 94. View of Segment 5AM.3924.2 at Vasquez Boulevard/US 6/US 85/SH 2 and I-270, view facing southwest. A new partial-clover leaf interchange with signalized intersections would be constructed at I-270 and Vasquez Boulevard.

Proposed improvements to Vasquez Boulevard/US 6/US 85/SH 2 include rebuilding the interchange with I-270. The I-270/Vasquez Boulevard interchange would be reconfigured by removing the westbound I-270 to southbound Vasquez Boulevard and eastbound I-270 to northbound Vasquez Boulevard exit loop ramps. Those movements would be provided via new signalized intersections at the new ramp terminal locations. A new ramp would also connect northbound Vasquez Boulevard to eastbound I-270, a movement not currently provided by the existing interchange configuration. These proposed improvements would result in total impact area of 6.86 acres between segments 5AM.3924.2 and 5AM.3924.3. Due to its lack of integrity this segment does not support the National Register eligibility of the overall resource. As a result, the project would result in a *No Adverse Effect* determination to Vasquez Boulevard/US 6/US 85/SH 2 (5AM.3924, which includes the non-supporting segment 5AM.4101.2).

(12) 5AM.3924.3 – U.S. Highway 6 (Vasquez Boulevard, CDOT Route 006H, map label 35)

Vasquez Boulevard/US 6/US 85/SH 2 originally functioned as a farm-to-market road providing local farmers access to the factories and railroads located in Denver and surrounding towns in the nineteenth and early twentieth centuries. This segment was widened and realigned multiple times in the mid-twentieth century, with the most significant changes occurring between 1957 and 1965 when the alignment through Commerce City was shifted and a cloverleaf interchange was built at the present-day junction with I-270. This segment is located between I-270 and I-76. It crosses I-270 East of Brighton Boulevard/SH 265 and north of E. 56th Avenue. The length is approximately 3.49 miles and the width is 102 feet.

The section of US 6 in northeastern Colorado is significant under *Criterion A* as an early farm-to-market road that provided primary access for farmers, particularly in Phillips, Logan, Washington, and Morgan Counties, during the 1910s and 1920s at the local level. This segment has been widened and altered multiple times and no longer retains integrity of location, setting, design, materials, workmanship, feeling, or association as a farm-to-market road. It is determined to be a non-supporting segment of Vasquez Boulevard.



Figure 95. View of Segment 5AM.3924.3 near the junction with I-270. A new partial-clover leaf interchange with signalized intersections would be constructed at I-270 and Vasquez Boulevard.

Proposed improvements to Vasquez Boulevard/US 6/US 85/SH 2 include rebuilding the interchange with I-270. The I-270/Vasquez Boulevard interchange would be reconfigured by removing the westbound I-270 to southbound Vasquez Boulevard and eastbound I-270 to northbound Vasquez Boulevard exit loop ramps. Those movements would be provided via new signalized intersections at the new ramp terminal locations. A new ramp would also connect northbound Vasquez Boulevard to eastbound I-270, a movement not currently provided by the existing interchange configuration. These proposed improvements would result in total impact area of 6.86 acres between segments 5AM.3924.2 and 5AM.3924.3. Due to its lack of integrity this segment does not support the National Register eligibility of the overall resource. As a result, the project would result in a *No Adverse Effect* determination to Vasquez Boulevard/US 6/US 85/SH 2 (5AM.3924, which includes the not supporting segment 5AM.3924.3).

C. Section 4(f)

Section 4(f) of the U.S. Department of Transportation (USDOT) Act of 1966 requires special consideration when a potential project requires use of park and recreation lands, wildlife and waterfowl refuges, and/or *historic sites* is identified during transportation project development. The law is implemented by the FHWA through the regulation 23 CFR 774.

(1) Historic transportation facility exception notification

The APE includes three historic roads (5AM.4101, including York Street segment 5AM.4101.2; 5AM.3924, including Vasquez Boulevard segments 5AM.3824.2/5AM.3924.4; and 5AM.2410, including Brighton Boulevard segment 5AM.2410.2), and four historic railroads (5AM.464, including Chicago Burlington & Quincy/Burlington Northern Santa Fe segment 5AM.464.21; 5AM.472, Denver Pacific/Union Pacific Railroad segment 5AM.472.41; 5AM.4119, Sand Creek Junction, and 5AM.1298, including Chicago, Burlington & Quincy Market Street Line Segment 5AM.1298.3) that qualify for a Section 4(f) historic transportation facility exception. For historic sites, FHWA can make a Section 4(f) transportation exception finding for use of historic transportation facilities when the project impacts have resulted in determinations of *No Adverse Effect* for these resources, and SHPO has not objected with these findings (see 23 CFR 774.13(a)(3)). The findings of *No Adverse Effect* for these properties under Section 106 reflects CDOT's conclusion that the effects would not "alter, directly or indirectly, any of the characteristics of [the] historic property that qualify the property for inclusion in the National Register in a manner that would diminish the integrity of the property's location, design, setting, materials, workmanship, feeling, or association" as described in 36 CFR § 800.5(a)(1). Given the efforts undertaken by the I-270 EA to minimize impacts to historic properties and CDOT's *No Adverse Effect* determinations, FHWA may apply the Section 4(f) historic transportation facility exceptions to the following seven resources:

- York Street, 5AM.4101 (including segment 5AM.4101.2)
- Vasquez Boulevard, 5AM.3924 (including segments 5AM.3924.2/5AM.3924.3)
- Brighton Boulevard, 5AM.2410 (including segment 5AM.2410.2)

- Chicago Burlington & Quincy/Burlington Northern Santa Fe Railroad, 5AM.464, including segment 5AM.464.21)
- Denver Pacific/Union Pacific Railroad, 5AM.472 (including segment 5AM.472.41)
- Sand Creek Junction 5AM.4119
- Chicago, Burlington & Quincy Railroad Market Street Line, 5AM.1298 (including 5AM.1298.3).

The roadway impacts for York Street and Vasquez Boulevard are associated with roadway improvements involving new pavement, curb and gutters, sidewalks, grading, and landscaping. For York Street the area of impact within the historical linear boundary is 1.71 acres and for Vasquez Boulevard the impact area within the historical boundary is 6.86 acres. For Brighton Boulevard, the area of impact within the historic boundary is 0.03 acres associated with removing the west abutment and slope paving.

The impacts for Chicago Burlington & Quincy/Burlington Northern Santa Fe Railroad, Denver Pacific/Union Pacific Railroad, Sand Creek Junction, and Chicago, Burlington & Quincy Railroad Market Street Line are associated with right-of-entry or other access agreements with the railroads that may be necessary during the bridge construction to work above the railroad tracks within the historic boundaries.

(2) Temporary occupancy exception notification

The Proposed Action would include a temporary easement for two non-transportation related National Register eligible properties: 5AM.4044, Suncor Energy, and 5AM.465.9, Burlington Ditch/O'Brien Canal.

(a) 5AM.4044 – Suncor Energy

This property is being treated as National Register eligible for Section 106 purposes. The temporary easement for Suncor Energy is approximately 0.252 acre; the property is assumed National Register eligible for the purposes of Section 106. FHWA can make a Section 4(f) temporary occupancy finding when it has been determined the temporary occupancies of land are so minimal as to not constitute a use within the meaning of Section 4(f) (see 23 CFR 774.13(d)). The following conditions must be satisfied:

- 1) Duration must be temporary, *i.e.*, less than the time needed for construction of the project and there should be no change in ownership of the land;
- 2) Scope of the work must be minor, *i.e.*, both the nature and the magnitude of the changes to the Section 4(f) property are minimal;
- 3) There are no anticipated permanent adverse physical impacts, nor will there be interference with the protected activities, features, or attributes of the property, on either a temporary or permanent basis;
- 4) The land being used must be fully restored, *i.e.*, the property must be returned to a condition which is at least as good as that which existed prior to the project; and
- 5) There must be documented agreement of the officials with jurisdiction over the Section 4(f) resource regarding the above conditions

The work consists of minor improvements to a small section of Sand Creek Trail that is located on the Suncor property. These improvements would necessitate an easement of approximately 0.252 acre in size of the large 45.67-acre Suncor property. Overall project construction is expected to take approximately two years. Work on the Suncor property to improve the trail would be temporary and is not expected to last more than six months. The proposed improvements would consist of minor grading, realignment of the trail, barrier removal and installation of new barriers to separate users of the trail from the road, culvert construction, and pavement restoration on E. 60th Avenue. The property would be fully restored and returned to a condition which is at least as good as that which existed before construction.

(b) 5AM.465.9 – Burlington Ditch/O'Brien Canal

The temporary easement for Burlington Ditch/O'Brien Canal is approximately 0.15 acres. FHWA can make a Section 4(f) temporary occupancy finding when it has been determined the temporary occupancies of land are so minimal as to not constitute a use within the meaning of Section 4(f) (see 23 CFR 774.13(d)). The following conditions must be satisfied:

- 1) Duration must be temporary, *i.e.*, less than the time needed for construction of the project and there should be no change in ownership of the land;
- 2) Scope of the work must be minor, *i.e.*, both the nature and the magnitude of the changes to the Section 4(f) property are minimal;
- 3) There are no anticipated permanent adverse physical impacts, nor will there be interference with the protected activities, features, or attributes of the property, on either a temporary or permanent basis;
- 4) The land being used must be fully restored, *i.e.*, the property must be returned to a condition which is at least as good as that which existed prior to the project; and
- 5) There must be documented agreement of the officials with jurisdiction over the Section 4(f) resource regarding the above conditions

A temporary construction easement for right-of-entry to the ditch access roads would be required to remove the existing bridges and build the new bridges. These improvements would necessitate an easement of approximately 0.18-acre on the west side of the ditch, and 0.17-acre feet on the east side of the ditch, for a total of 0.35 acres in size of the 35-acre ditch property. Overall project construction is expected to take approximately two years. Work in the vicinity of the ditch would be temporary and will last between nine and twelve months, depending on project phasing. The property would be fully restored and returned to a condition that is at least as good as that which existed before construction.

The removal of the existing bridges would be conducted with minimal disturbance to the ditch embankments and no disturbance to the Burlington Ditch channel. The existing bridge pier columns would be removed to 1 foot below the natural ground surface then buried in accordance with CDOT Standard Specs Section 202.08 so that the existing bridge columns would no longer be visible. The bottom portions of the existing bridge columns as well as the existing bridge foundations (consisting of pile caps with driven piles) would be buried following structure removal. The removal of the existing bridges would be conducted from the ditch access roads with minimal disturbance to the ditch embankments and no disturbance to the channel though a project special provision may be necessary to

specify the specific requirements to protect the channel. The project specifications would include instructions for contractors that any additional changes or modifications to the ditch within the historic boundary would require SHPO consultation.

The removal of the existing bridge piers within the historic boundary would result in a direct effect to the ditch. Indirect effects would include placing the abutments for the two new bridges would be outside of the historic boundary. While there are changes to the setting of the historic ditch, the project would not change or alter the character-defining features of the ditch. The Burlington Ditch/O'Brien Canal has historically been crossed by several bridges and the current I-270 bridges have been in place since the 1960s. Replacing one set of non-historic bridges with another would not significantly impact the resource's integrity of setting, feeling, or association. Removal of the current piers may improve these aspects of integrity by removing a non-historic element from the historic boundary. There would be no change to the integrity of location, materials, design, or workmanship to the ditch. Because the historic integrity would not be significantly impacted, the ditch would still be able to convey its significance under *Criterion A*. As a result, the project would result in a *No Adverse Effect* determination to the Burlington Ditch/O'Brien Canal (5AM.465, which includes the supporting segment 5AM.465.9).

The findings of *No Adverse Effect* for these properties under Section 106 reflects CDOT's conclusion that the effects would not "alter, directly or indirectly, any of the characteristics of [the] historic property that qualify the property for inclusion in the National Register in a manner that would diminish the integrity of the property's location, design, setting, materials, workmanship, feeling, or association" as described in 36 CFR § 800.5(a)(1). Given the efforts undertaken by the I-270 EA to minimize impacts to historic properties and CDOT's *No Adverse Effect* findings for the Suncor Energy property and Burlington Ditch/O'Brien Canal, FHWA plans to make a temporary occupancy Section 4(f) finding for these properties.

(3) Section 4(f) *de minimis* notification

Consistent with Section 4(f) regulations [23 CFR 774.5(a)(ii) and 774.7 (b)], FHWA can make a *de minimis* finding when a use of a historic property results in a *No Adverse Effect* determination under the Section 106 process. Widening of the I-270 mainline and storm sewer drainage improvements over the segment of the Gardener's Ditch (4AM.1292.2) may require removal of the current culvert, which is piped under I-270 (removal of approximately 1,600 square feet, or 345 feet by 4.52 feet of the segment, which totals 5,190 square feet). Additionally, a small temporary easement of 94 square feet is required on the south side of I-270 to realign the York Street Entrance Ramp to eastbound I-270 for a total impact of 1,694 square feet. The finding of *No Adverse Effect* to 5AM.1292, Gardener's Ditch, including segment 5AM.1292.2, reflect a conclusion that the effects would not "alter, directly or indirectly, any of the characteristics of [the] historic property that qualify the property for inclusion in the National Register in a manner that would diminish the integrity of the property's location, design, setting, materials, workmanship, feeling or association as described in 36 CFR § 800.5(a)(1). Given the efforts undertaken by the I-270 EA to minimize impacts to historic properties and CDOT's *No Adverse Effect* finding for the Gardener Ditch, FHWA plans to make a *de minimis* finding for this property.

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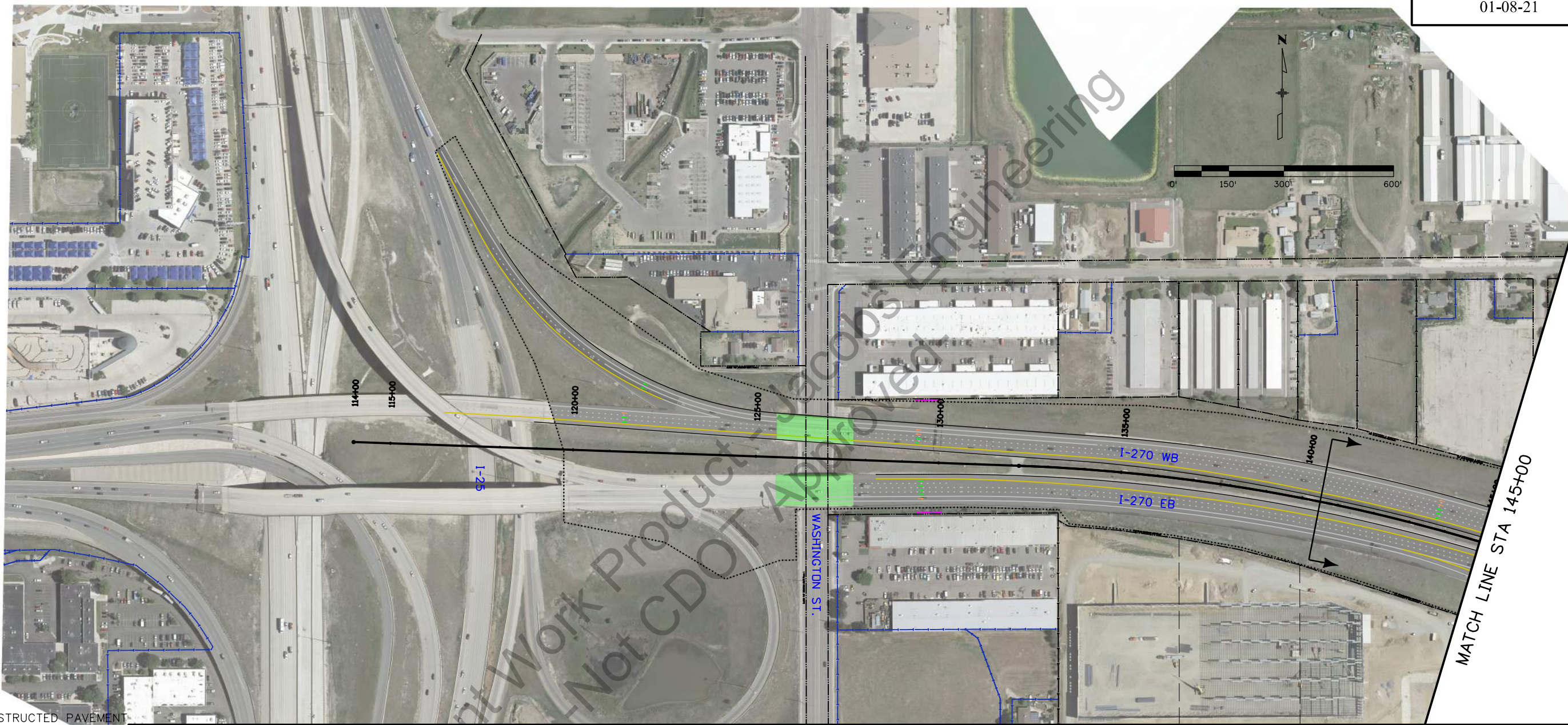
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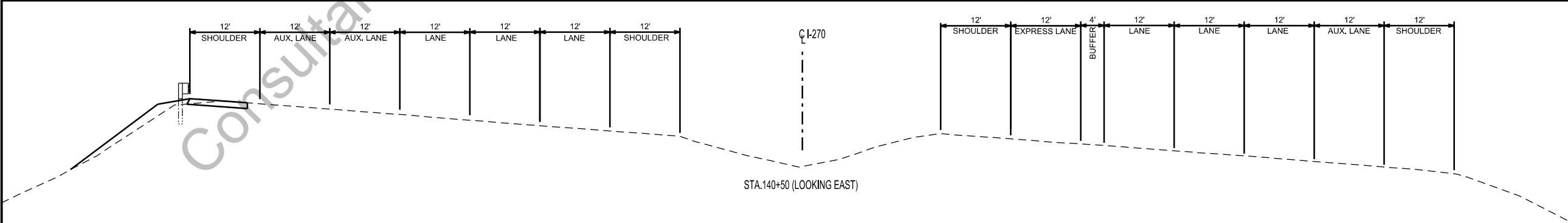
Appendix A. Project Plans

Consultant Work Product - Jacobs Engineering
-Not CDOT Approved-



LEGEND

- RECONSTRUCTED PAVEMENT
- PAVEMENT TO REMAIN
- ASPHALT PAVING
- MILL/OVERLAY
- RECONSTRUCTED BRIDGE
- BRIDGE TO REMAIN
- WATER QUALITY
- EXPRESS LANE
- GENERAL PURPOSE LANE
- AUXILIARY LANE
- ACCEL/DECEL LANE



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Unit Information	Unit Leader Initials

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Colorado Department of Transportation

4670 N. Holly St.
Denver, CO 80216
Phone: 303-398-6730 FAX: 303-398-6781

Region 1 AMP

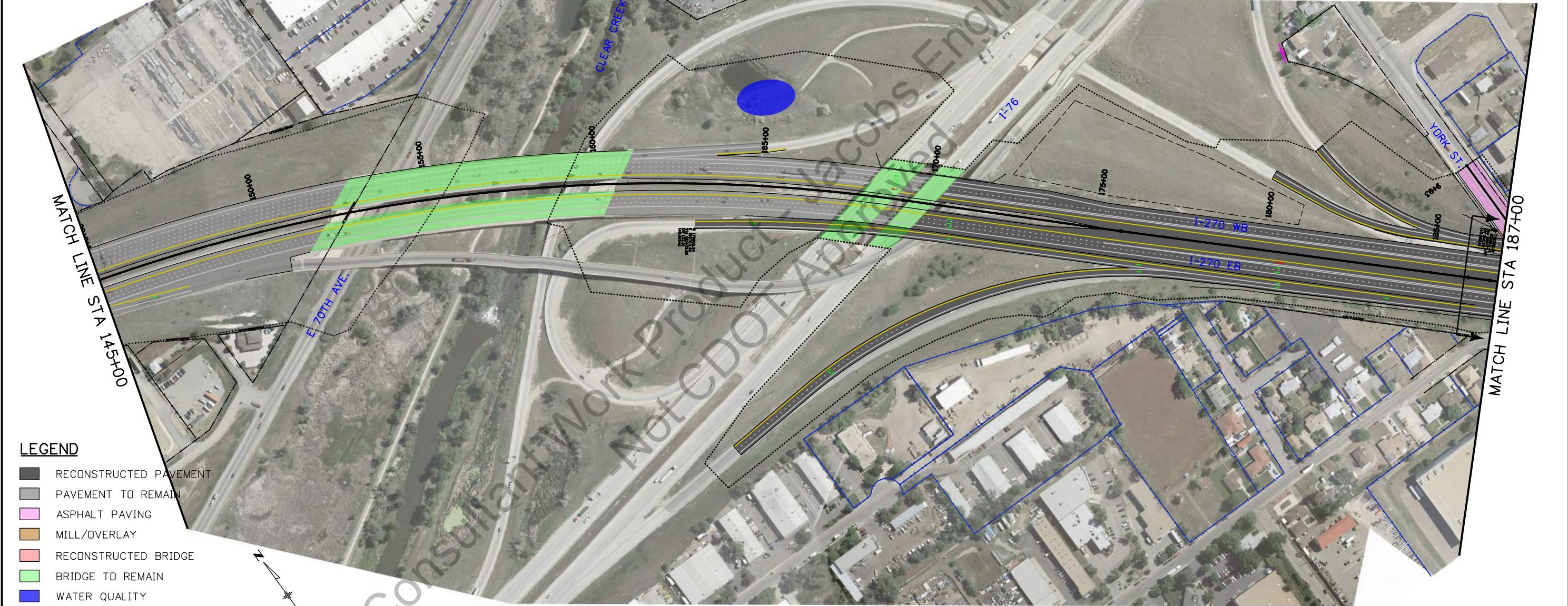
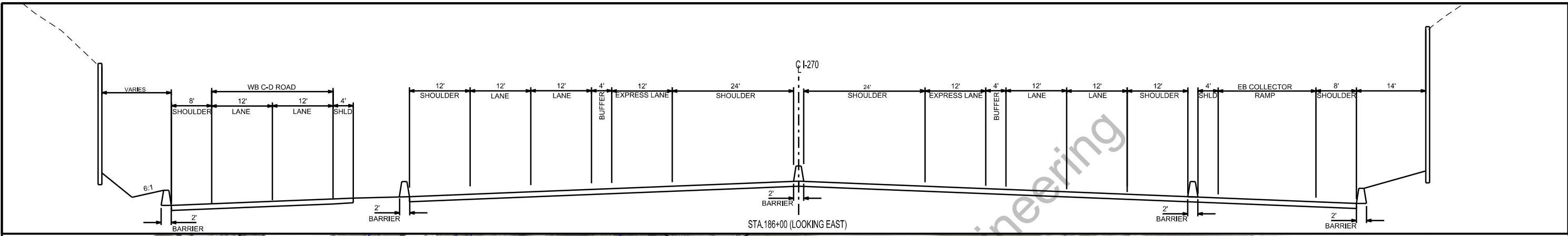
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LEGEND

- RECONSTRUCTED PAVEMENT
- PAVEMENT TO REMAIN
- ASPHALT PAVING
- MILL/OVERLAY
- RECONSTRUCTED BRIDGE
- BRIDGE TO REMAIN
- WATER QUALITY
- EXPRESS LANE
- GENERAL PURPOSE LANE
- AUXILIARY LANE
- ACCEL/DECEL LANE



CONCEPT
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 Region 1 AMP

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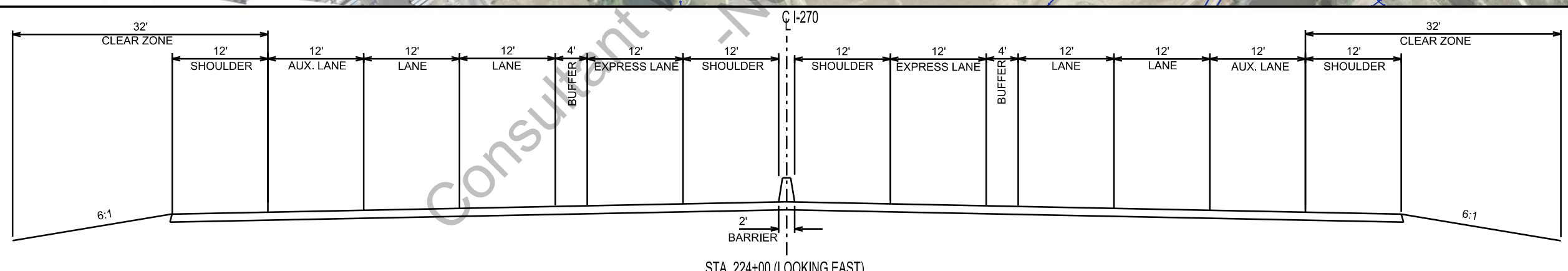


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CONCEPT

01-08-21




LEGEND

- RECONSTRUCTED PAVEMENT
- PAVEMENT TO REMAIN
- ASPHALT PAVING
- MILL/OVERLAY
- RECONSTRUCTED BRIDGE
- BRIDGE TO REMAIN
- WATER QUALITY
- EXPRESS LANE
- GENERAL PURPOSE LANE
- AUXILIARY LANE
- ACCEL/DECEL LANE

Print Date: 1/11/2021
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 Unit Information Unit Leader Initials

Sheet Revisions		
Date:	Comments	Init.

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 4670 N. Holly St.
 Denver, CO 80216
 Phone: 303-398-6730 FAX: 303-398-6781
 Region 1 AMP

As Constructed
 No Revisions:
 Revised:
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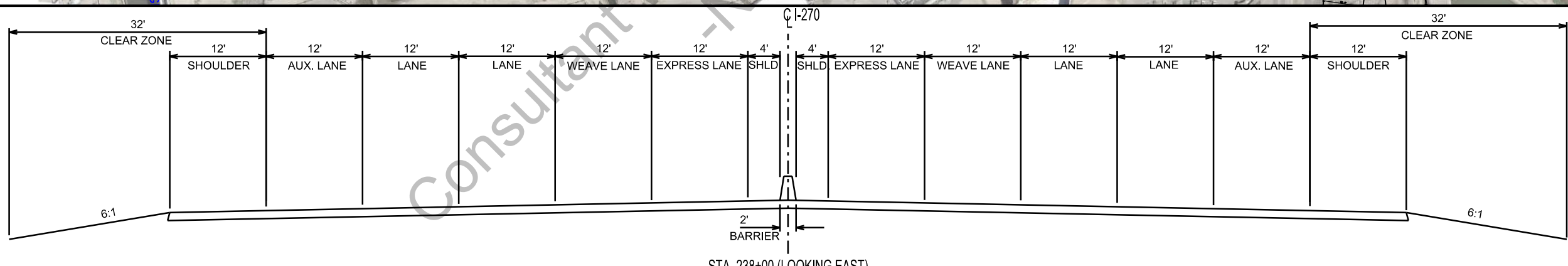
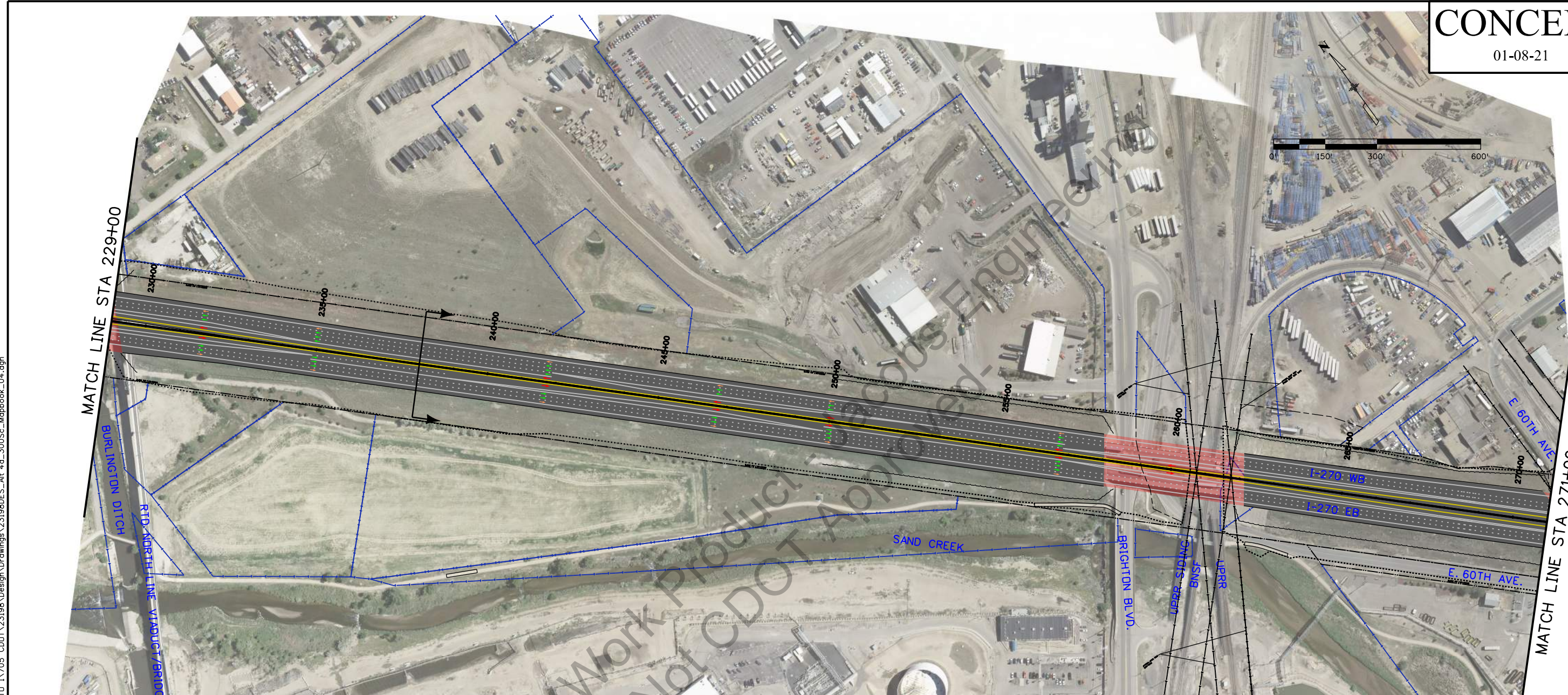


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CONCEPT

01-08-21



LEGEND

- RECONSTRUCTED PAVEMENT
- PAVEMENT TO REMAIN
- ASPHALT PAVING
- MILL/OVERLAY
- RECONSTRUCTED BRIDGE
- BRIDGE TO REMAIN
- WATER QUALITY
- EXPRESS LANE
- GENERAL PURPOSE LANE
- AUXILIARY LANE
- ACCEL/DECEL LANE

STA. 238+00 (LOOKING EAST)

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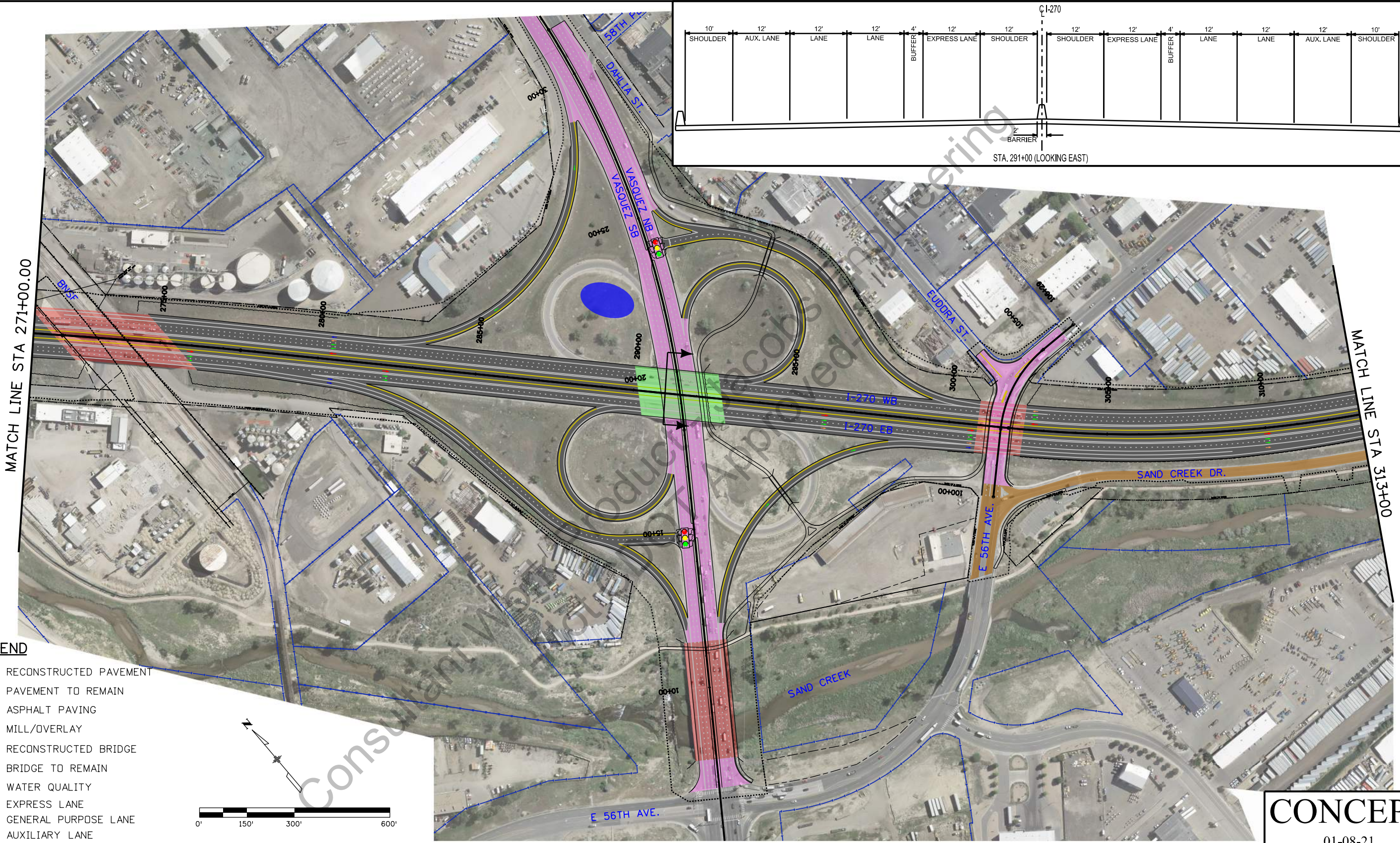
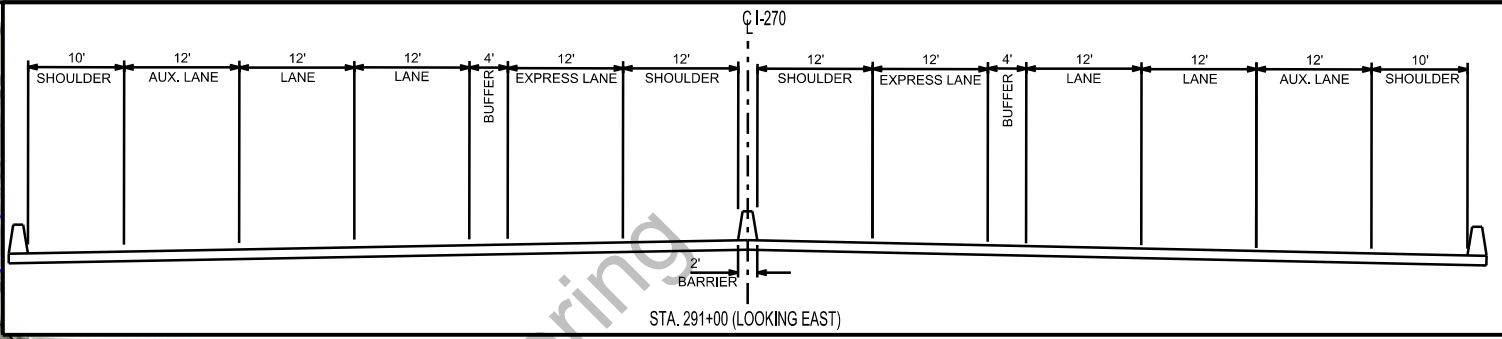
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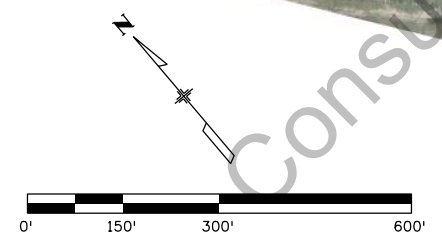


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- LEGEND**
- RECONSTRUCTED PAVEMENT
 - PAVEMENT TO REMAIN
 - ASPHALT PAVING
 - MILL/OVERLAY
 - RECONSTRUCTED BRIDGE
 - BRIDGE TO REMAIN
 - WATER QUALITY
 - EXPRESS LANE
 - GENERAL PURPOSE LANE
 - AUXILIARY LANE
 - ACCEL/DECEL LANE



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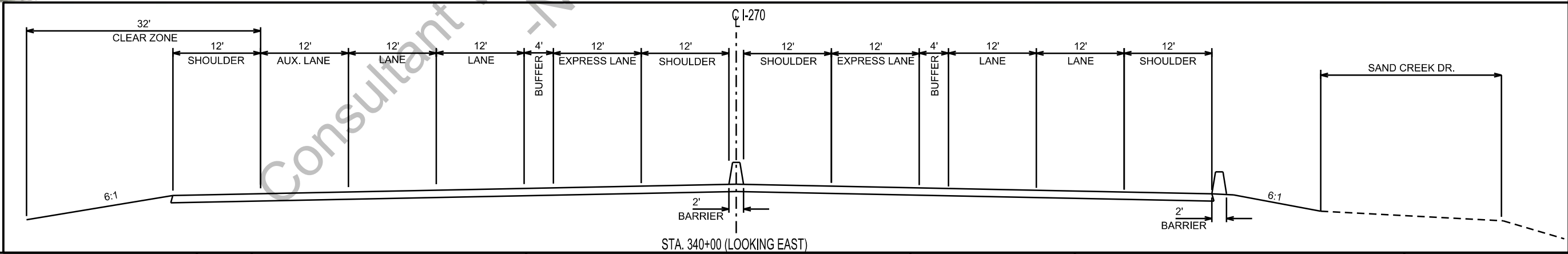
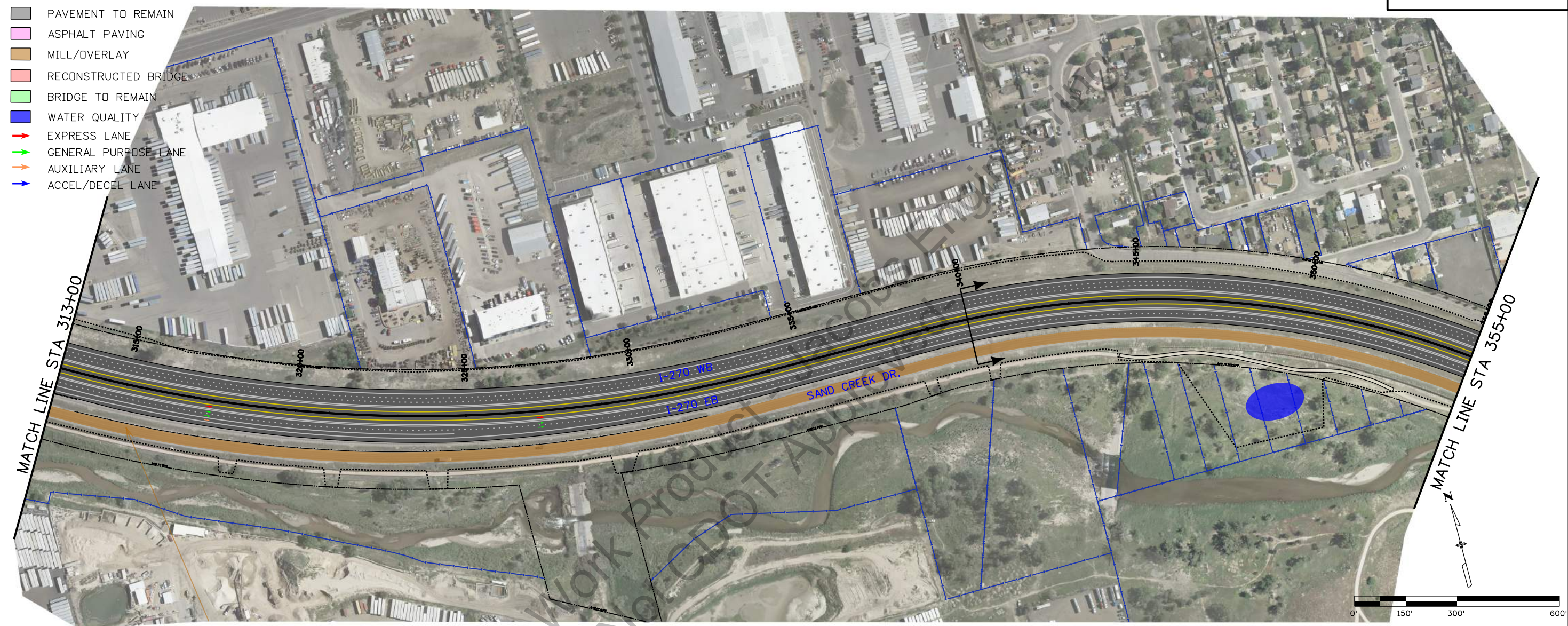
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- ASPHALT PAVING
- MILL/OVERLAY
- RECONSTRUCTED BRIDGE
- BRIDGE TO REMAIN
- WATER QUALITY
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- ➔ GENERAL PURPOSE LANE
- ➔ AUXILIARY LANE
- ➔ ACCEL/DECEL LANE




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Sheet Revisions		
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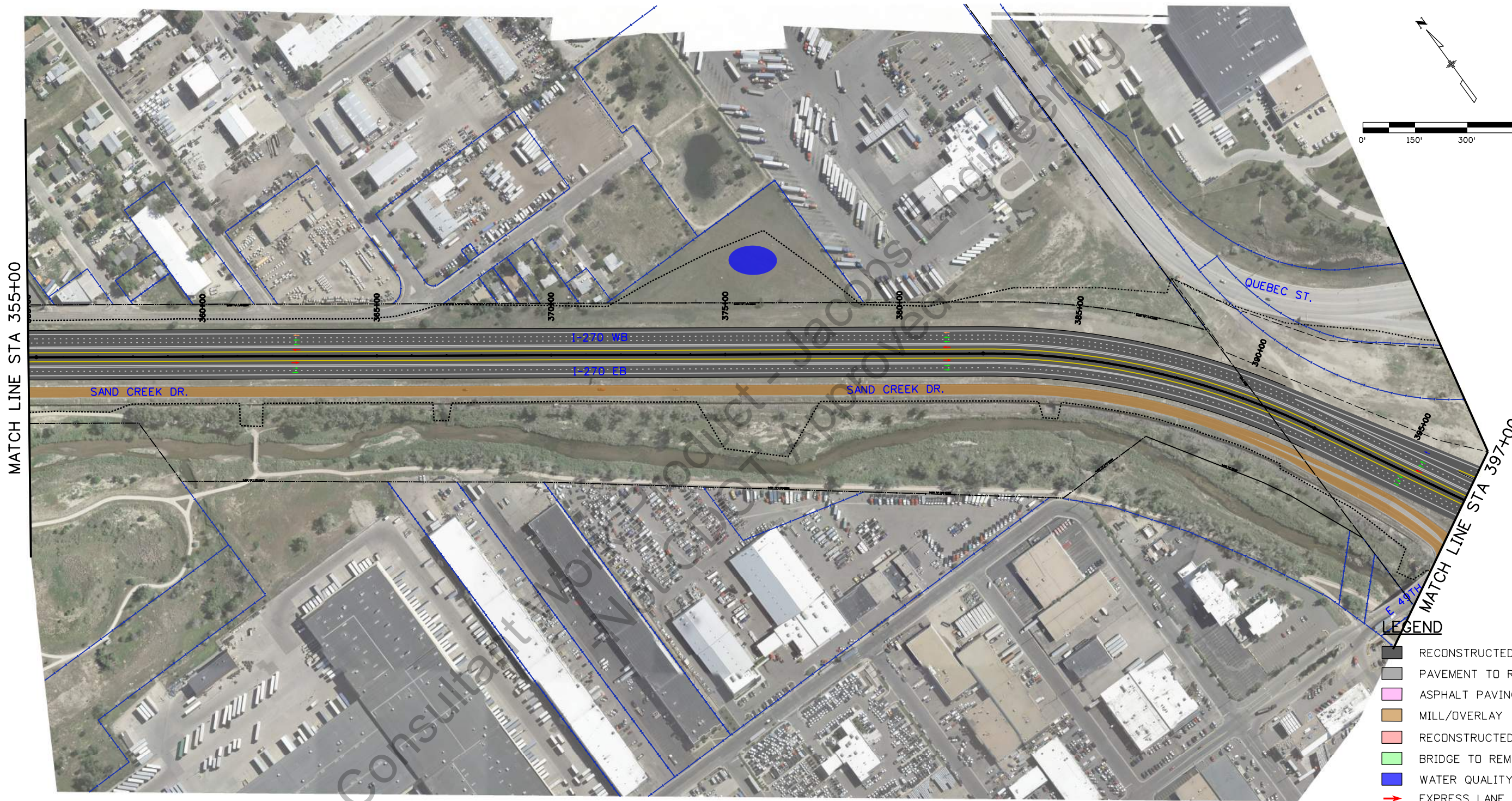
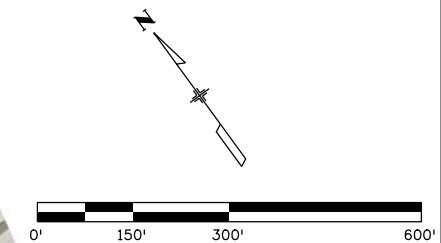
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CONCEPT

01-08-21



- LEGEND**
- RECONSTRUCTED PAVEMENT
 - PAVEMENT TO REMAIN
 - ASPHALT PAVING
 - MILL/OVERLAY
 - RECONSTRUCTED BRIDGE
 - BRIDGE TO REMAIN
 - WATER QUALITY
 - EXPRESS LANE
 - GENERAL PURPOSE LANE
 - AUXILIARY LANE
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Unit Information Unit Leader Initials
Jacobs

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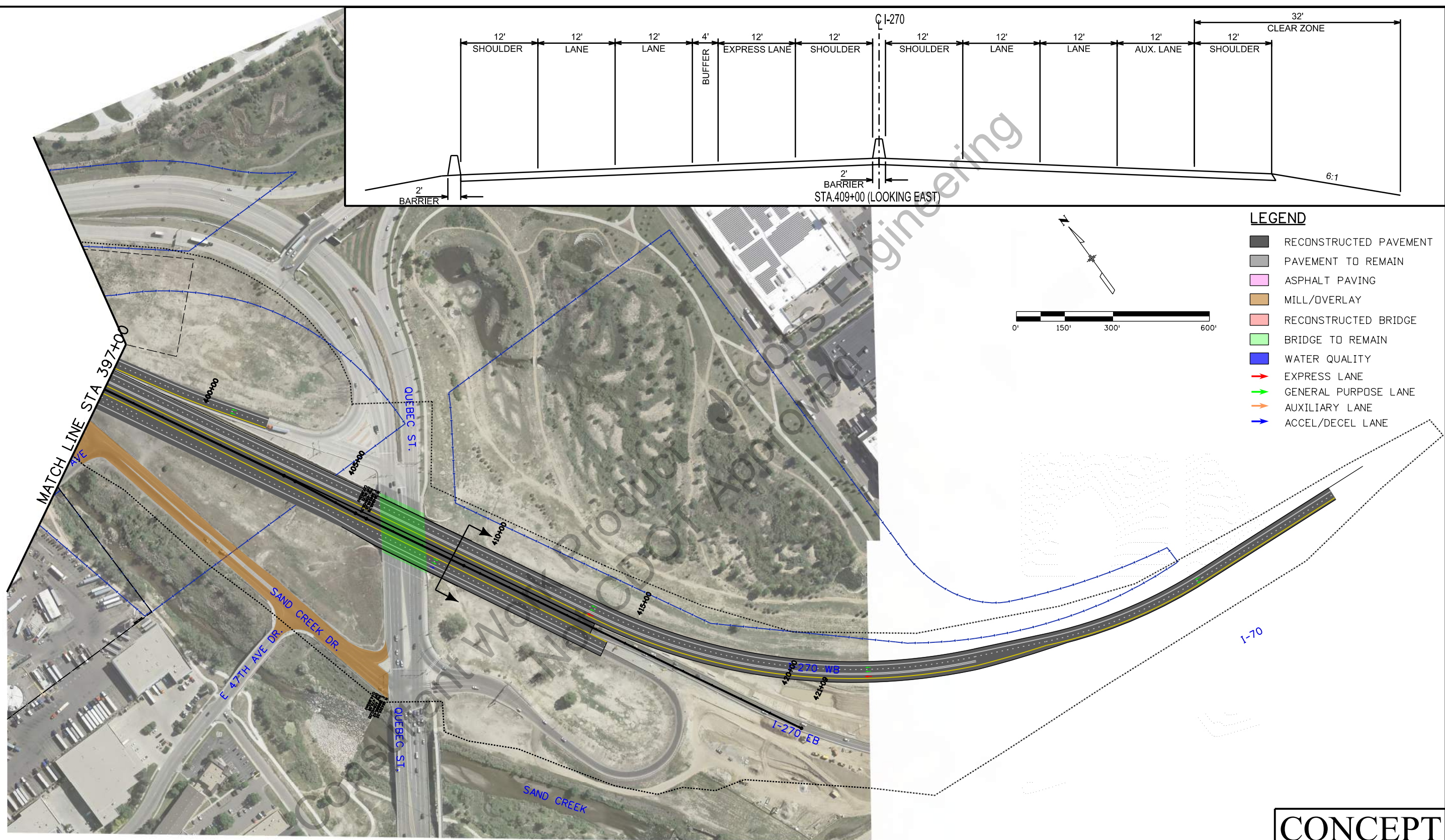
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Print Date: 1/11/2021
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Unit Information Unit Leader Initials
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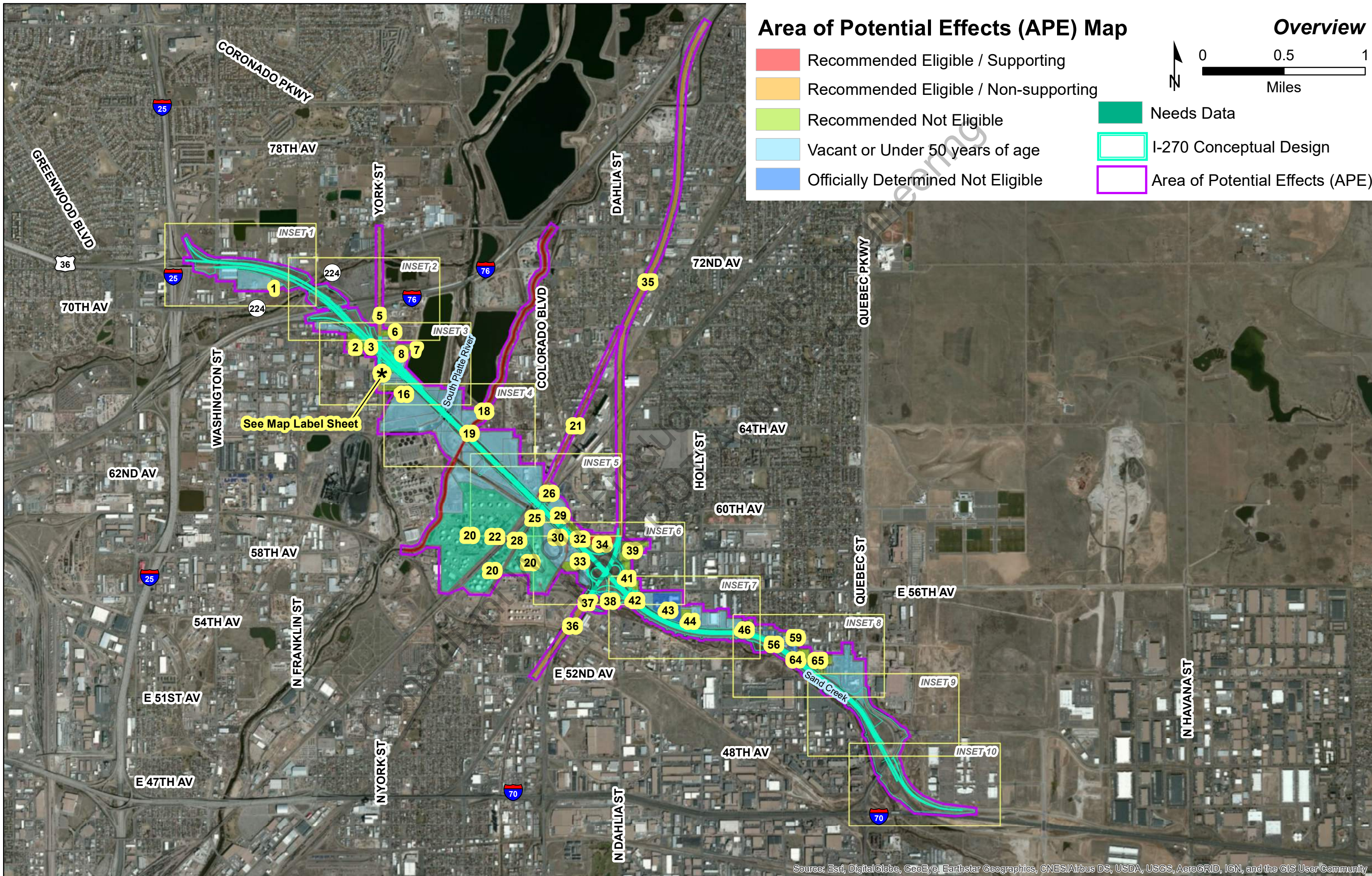
Appendix B. Area of Potential Effects Maps

Consultant Work Product - Jacobs Engineering
-Not CDOT Approved-

Area of Potential Effects (APE) Map

Overview

- Recommended Eligible / Supporting
- Recommended Eligible / Non-supporting
- Recommended Not Eligible
- Vacant or Under 50 years of age
- Officially Determined Not Eligible
- Needs Data
- I-270 Conceptual Design
- Area of Potential Effects (APE)



Source: Esri, DigitalGlobe, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, and the GIS User Community

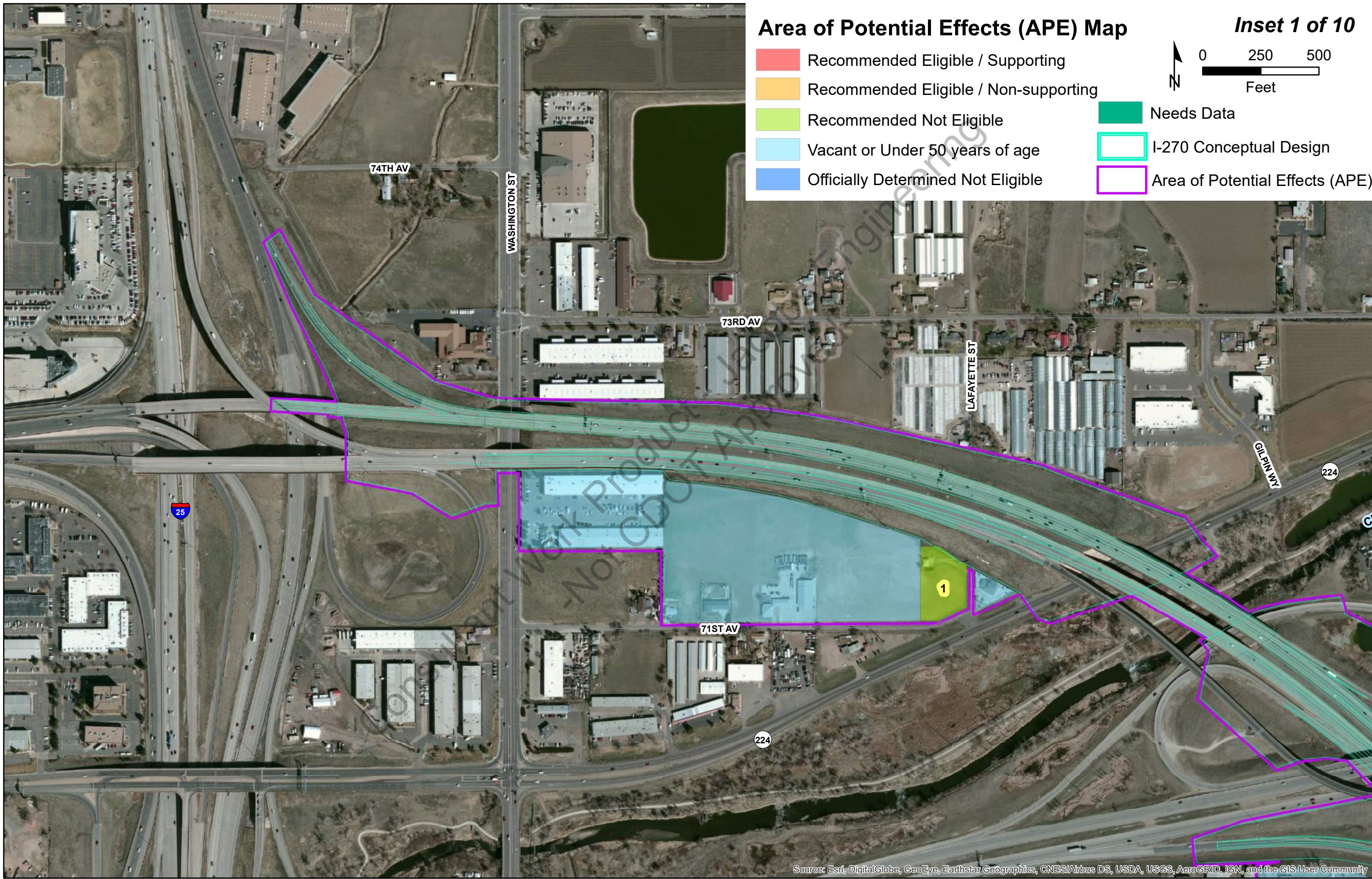
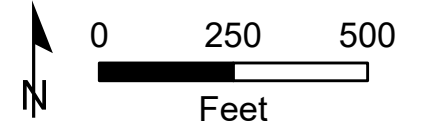
Map Label Sheet

Map Label	Site Name or Address	Site Number	Map Label	Site Name or Address	Site Number
1	7141 Lafayette St	5AM.4107	37	Sand Creek Bridge - E-17-AT (Vazquez Boulevard/US 6)	5AM.1363
2	Larusso Subdivision	5AM.4031	38	5555 Dahlia St	5AM.1850
3	2101 E 68th Ave	5AM.4032	39	5820 Dahlia St	5AM.4112
4	2180 E 68th Ave	5AM.4034	40	5790 Dahlia St	5AM.4070
5	York St	5AM.4101.2	41	5665 Eudora St, 5775 Eudora St, 5675 Eudora St	5AM.4122
6	6875 Columbine St	5AM.4108	42	5678 Eudora St	5AM.4113
7	6824 Columbine St	5AM.4109	43	5200 E 56th Ave	5AM.1849
8	6820 York St	5AM.4035	44	5455 Holly St	5AM.4114
9	6776 York St	5AM.4036	45	5431 Krameria St	5AM.1899
10	6780 York St	5AM.4037	46	5421 Krameria St	5AM.4115
11	6750 York St	5AM.4038	47	5450 Krameria St	5AM.4144
12	6702 York St	5AM.4039	48	5410 Krameria St	5AM.1845
13	6700 York St	5AM.4040	49	6320 E 54th Ave	5AM.1847
14	6690 York St	5AM.4041	50	6330 E 54th Ave	5AM.1848
*	6680 York St (property not surveyed)	-	51	5401 Locust St	5AM.1904
15	6620 York St	5AM.4043	52	6411 E 53rd Way	5AM.1893
16	6610 York St	5AM.4042	53	5400 Locust St	5AM.4145
17	Gardner's Ditch Segment	5AM.1292.2	54	6415 E 53rd Way	5AM.1894
18	Burlington Ditch/O'Brien Canal	5AM.465.9	55	6481 E 53rd Way	5AM.1895
19	E 64th Ave Bridge Over Burlington Ditch/O'Brien Canal	5AM.4118	56	6470 E 53rd Way	5AM.1846
20	5801 Brighton Blvd	5AM.4044	57	5360 Monaco St	5AM.1907
20	5800 Brighton Rd	5AM.4044	58	5401 Magnolia St	5AM.4146
20	3875 E 56th Ave	5AM.4044	59	5350 Magnolia St	5AM.1905
21	Brighton Boulevard / State Highway (SH) 265	5AM.2410.2	60	5358 Magnolia St	5AM.4147
22	Union Pacific Railroad Siding At Suncor Energy	5AM.4121.1	61	5366 Magnolia St	5AM.1906
23	Union Pacific Railroad Bridge - COMC-4A-02-RR (UP Suncor Siding)	5AM.1416	62	5360 Magnolia St	5AM.1842
24	Union Pacific Railroad Bridge - COMC-4A-02-RR (DP/UP)	5AM.1417	63	5311 Niagara St	5AM.1843
25	Baltimore Northern Santa Fe Railroad Bridge - COMC-5A-03-RR (CB&Q/BNSF)	5AM.1418	64	5345 Newport St	5AM.4116
26	Chicago Burlington & Quincy BNSF - Brush Line Segment	5AM.464.21	65	5280 Newport St	5AM.4117
27	Sand Creek Junction	5AM.4119	66	6820 E 52nd PI	5AM.1891
28	Denver Pacific / Union Pacific Railroad Segment—Greeley Line	5AM.472.41	67	6830 E 52nd PI	5AM.1892
29	4100 E 60th Ave	5AM.4045	68	6860 E 52nd PI	5AM.4048
30	Chicago, Burlington & Quincy BNSF - Market St Line Segment	5AM.1298.3	69	6870 E 52nd PI	5AM.4148
31	4306 E 60th Ave	5AM.4111	70	5251 Oneida St	5AM.4049
32	4310 E 60th Ave	5AM.4110	71	5250 Oneida St	5AM.4050
33	5699 Dexter St	5AM.4046			
34	5701 Dexter St	5AM.4047			
35	Vasquez Boulevard / U.S. Highway (US) 6 / CDOT Route 006H	5AM.3924.3			
36	Vasquez Boulevard / U.S. Highway (US) 6 / U.S. Highway (US) 85 / State Highway (SH) 2	5AM.3924.2			

Area of Potential Effects (APE) Map

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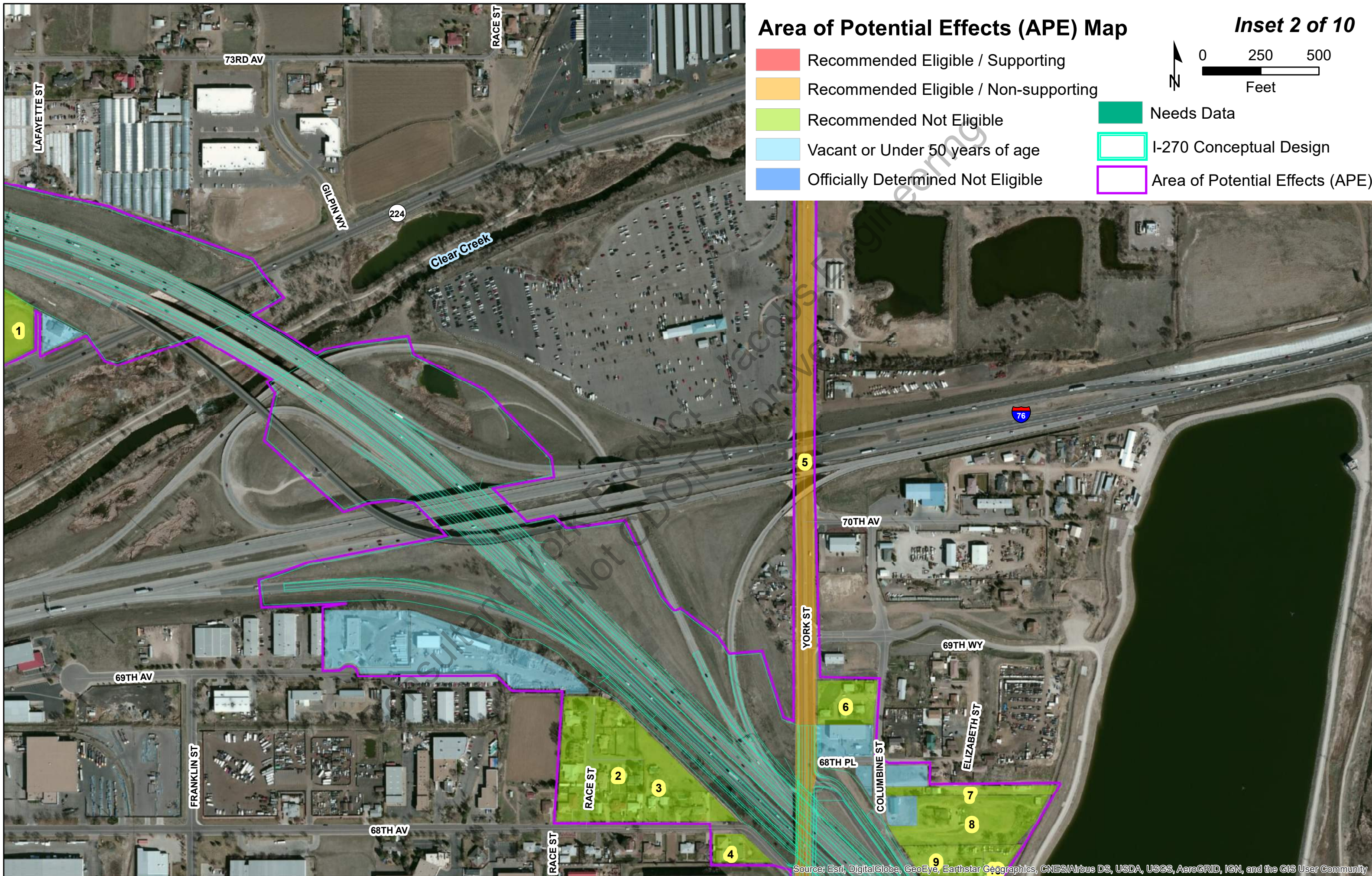
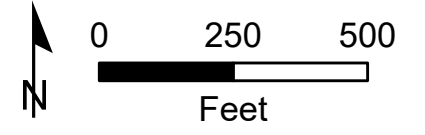
- Recommended Eligible / Supporting
- Recommended Eligible / Non-supporting
- Recommended Not Eligible
- Vacant or Under 50 years of age
- Officially Determined Not Eligible
- Needs Data
- I-270 Conceptual Design
- Area of Potential Effects (APE)



Area of Potential Effects (APE) Map

Inset 2 of 10

- Recommended Eligible / Supporting
- Recommended Eligible / Non-supporting
- Recommended Not Eligible
- Vacant or Under 50 years of age
- Officially Determined Not Eligible
- Needs Data
- I-270 Conceptual Design
- Area of Potential Effects (APE)

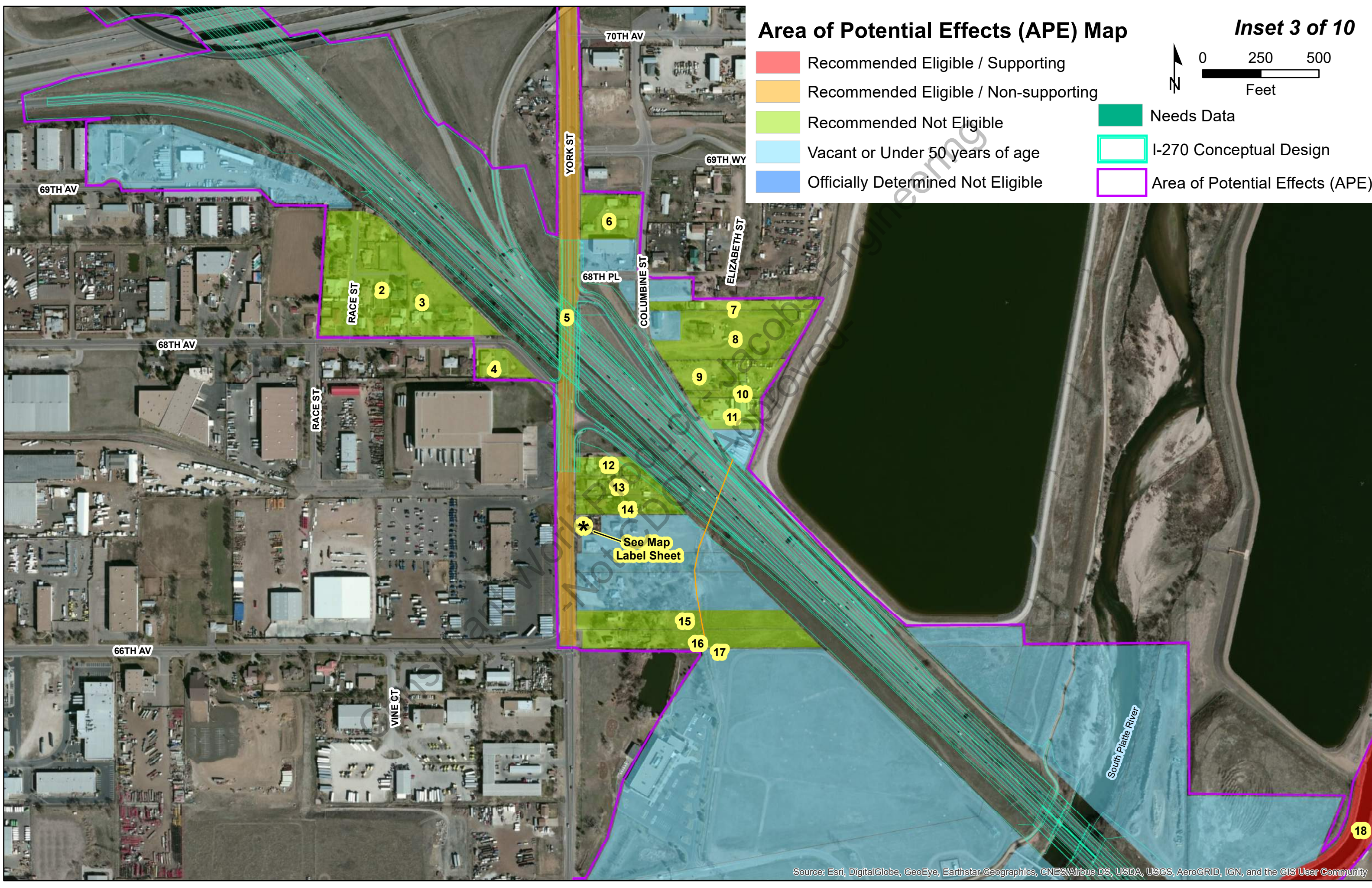
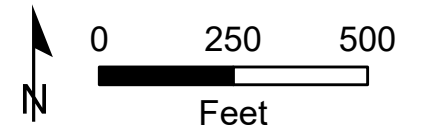


Source: Esri, DigitalGlobe, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, and the GIS User Community

Area of Potential Effects (APE) Map

Inset 3 of 10

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- Vacant or Under 50 years of age
- Officially Determined Not Eligible
- Needs Data
- I-270 Conceptual Design
- Area of Potential Effects (APE)

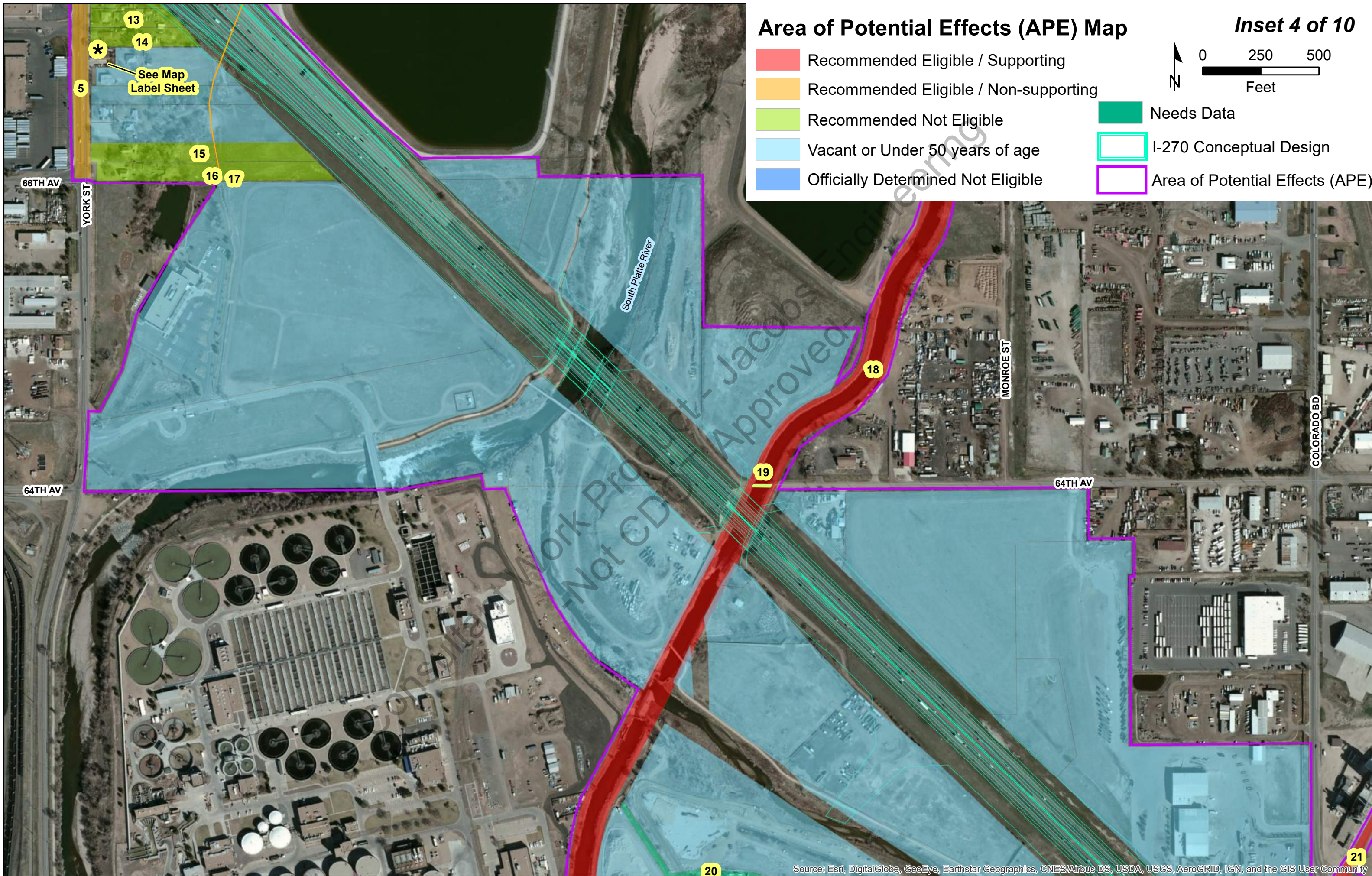
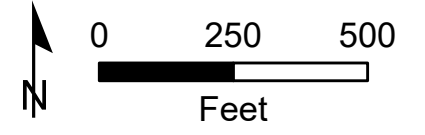


* See Map Label Sheet

Area of Potential Effects (APE) Map

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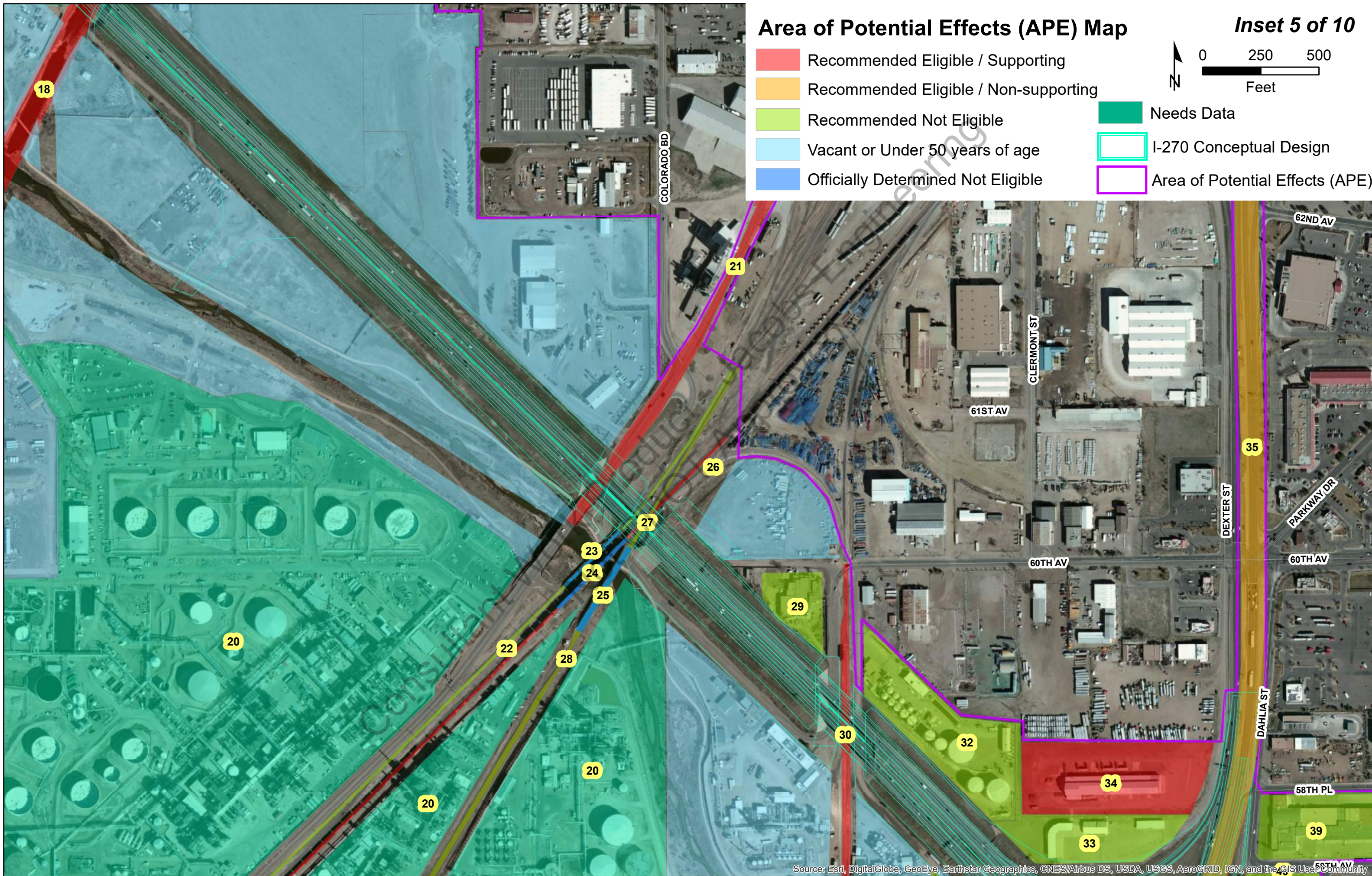
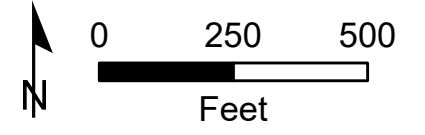
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- Vacant or Under 50 years of age
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- Needs Data
- I-270 Conceptual Design
- Area of Potential Effects (APE)



Area of Potential Effects (APE) Map

Inset 5 of 10

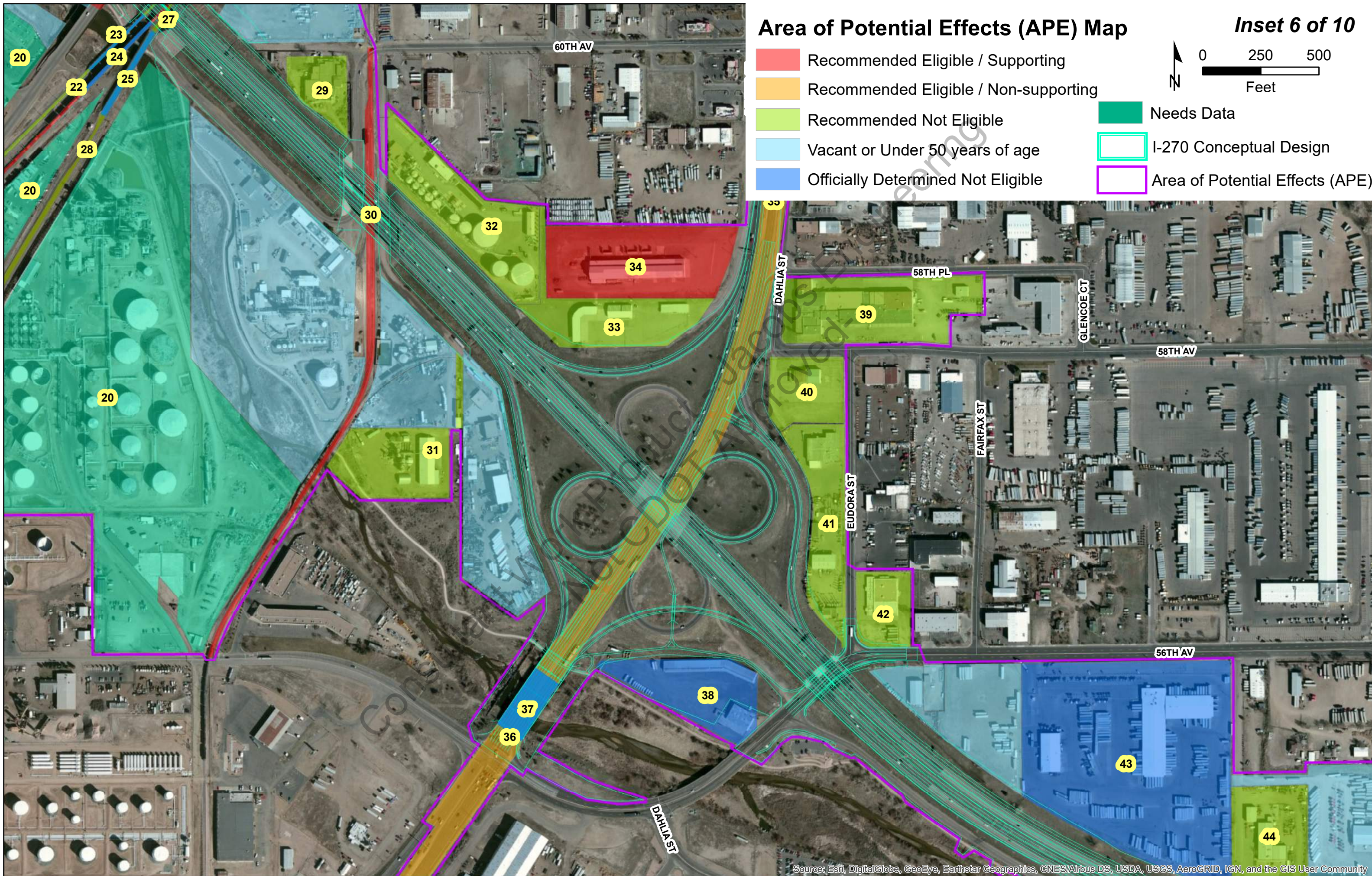
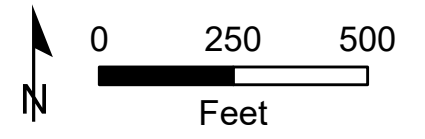
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- Needs Data
- I-270 Conceptual Design
- Area of Potential Effects (APE)



Area of Potential Effects (APE) Map

Inset 6 of 10

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- I-270 Conceptual Design
- Area of Potential Effects (APE)

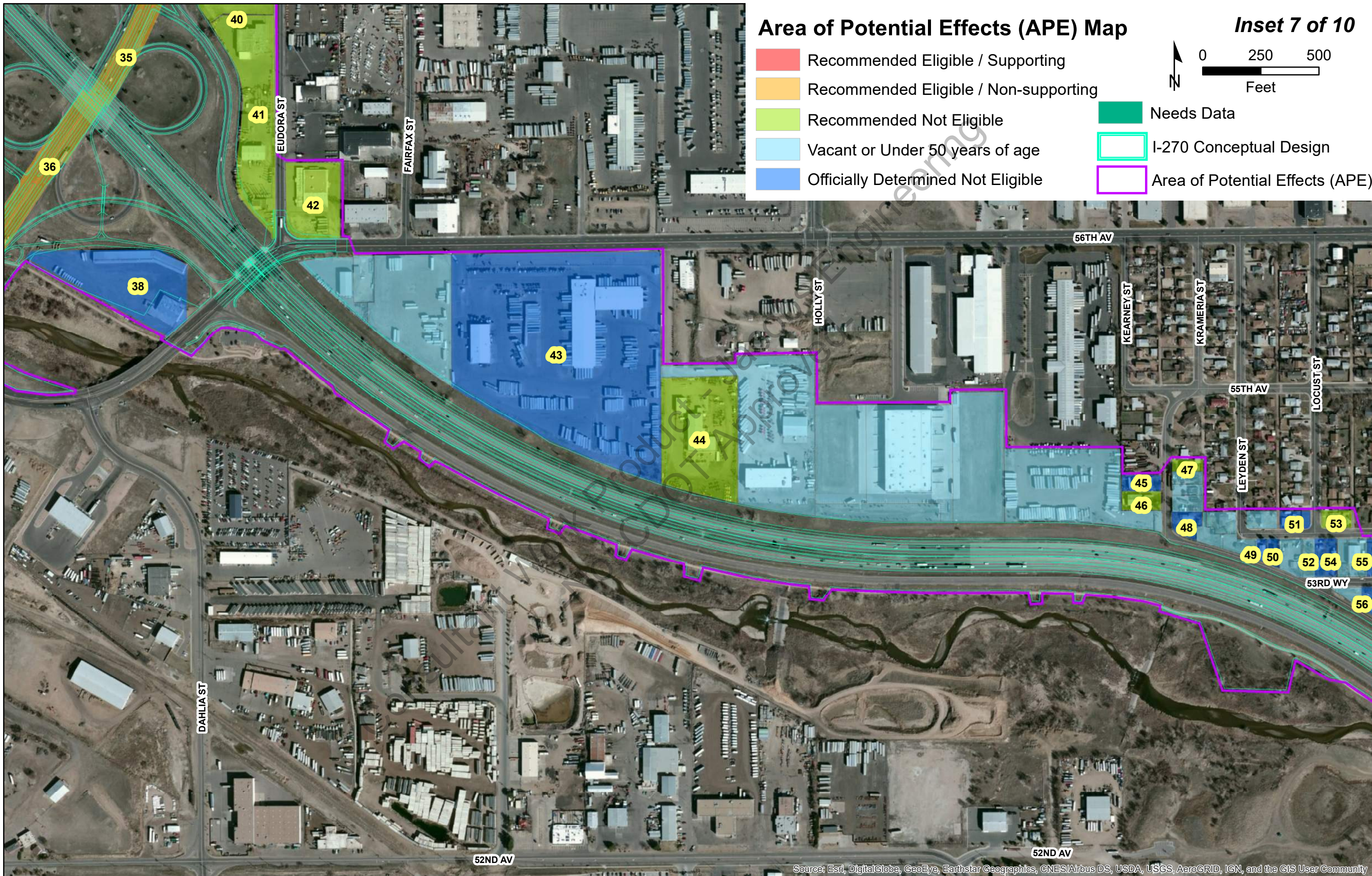
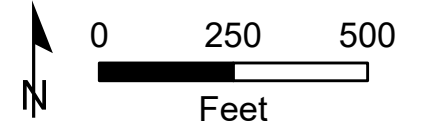


Source: Esri, DigitalGlobe, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, and the GIS User Community

Area of Potential Effects (APE) Map

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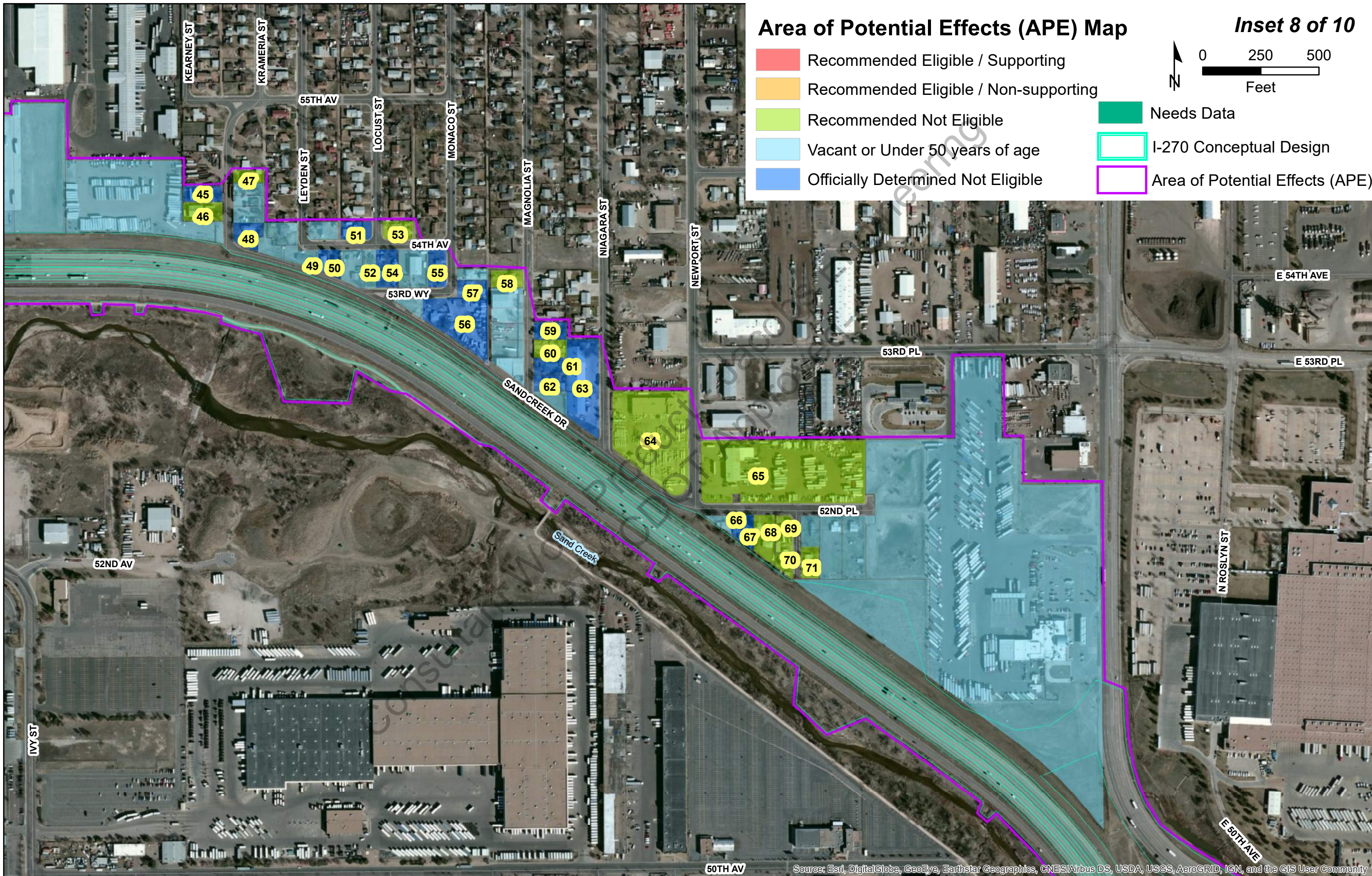
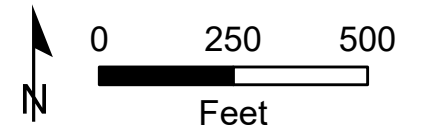
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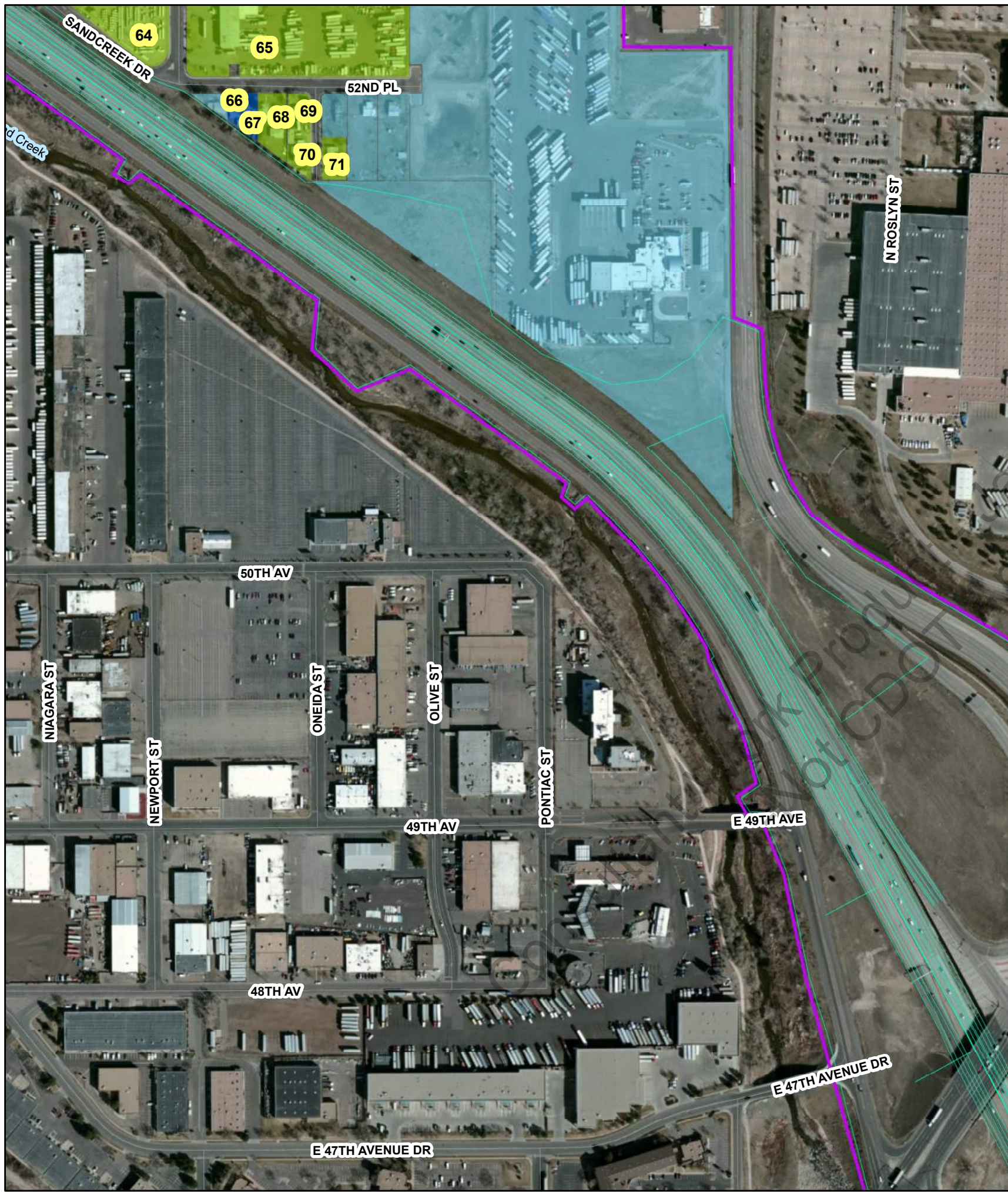


Area of Potential Effects (APE) Map

Inset 8 of 10

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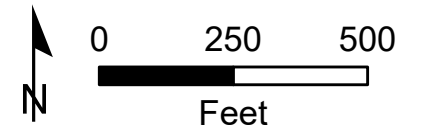




Area of Potential Effects (APE) Map

Inset 9 of 10

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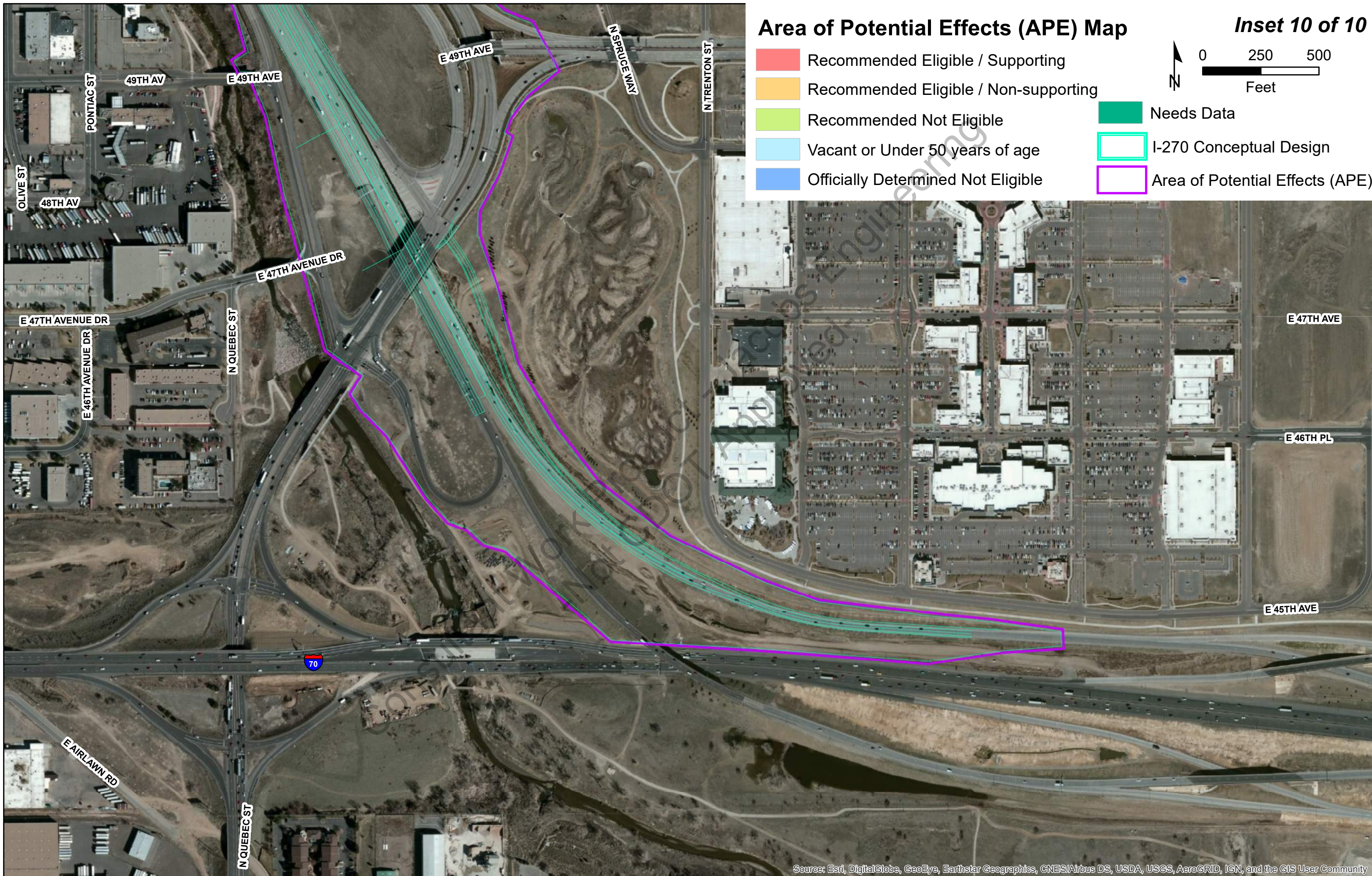
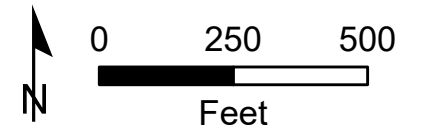


Source: Esri, DigitalGlobe, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, and the GIS User Community

Area of Potential Effects (APE) Map

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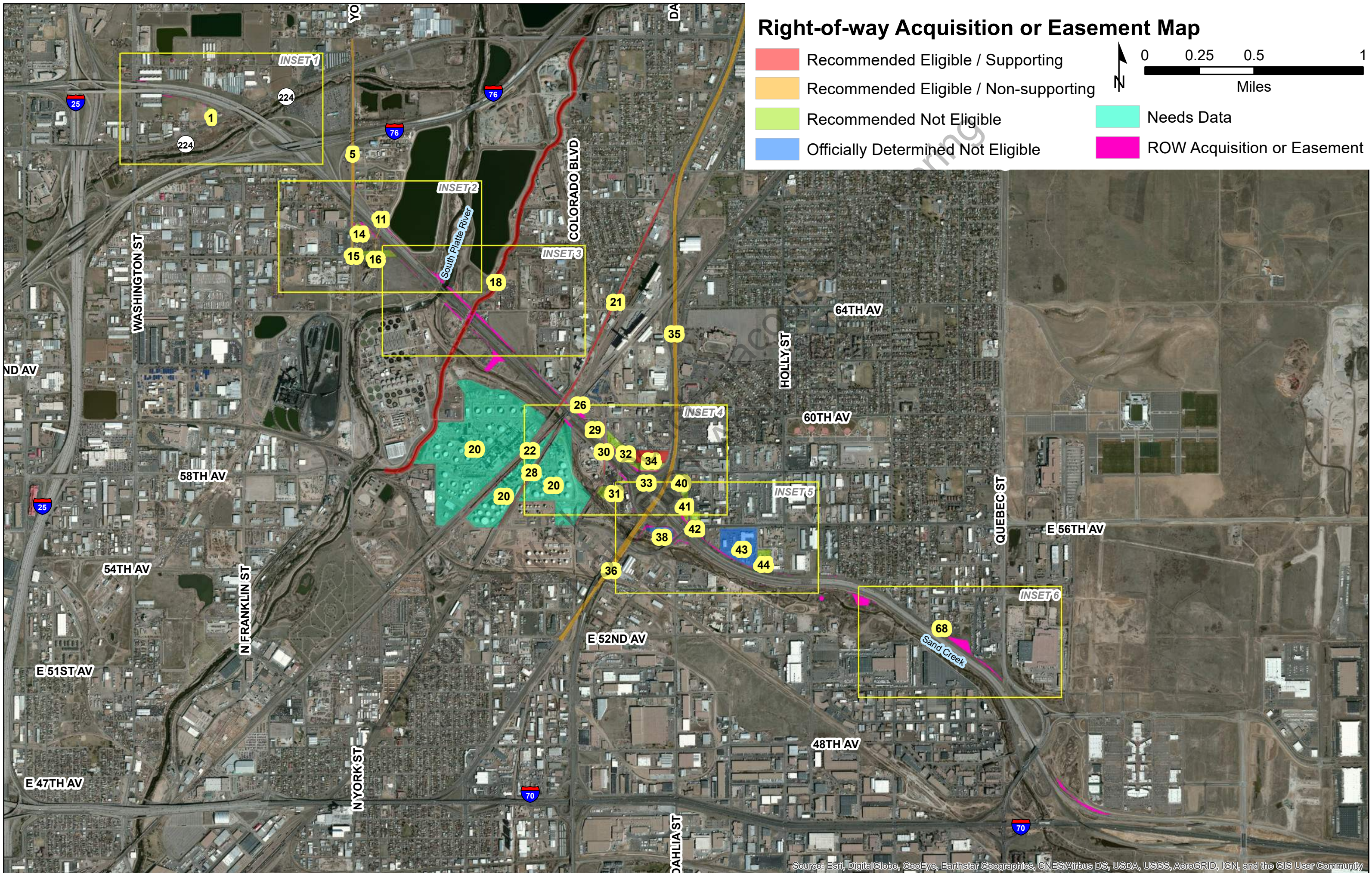
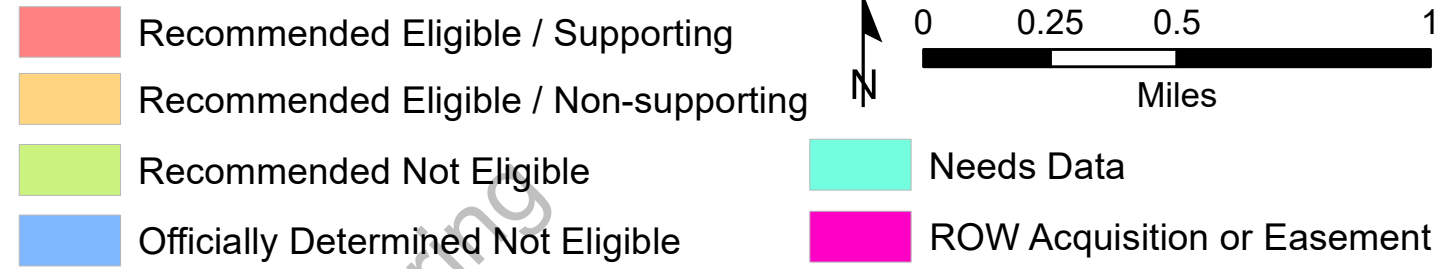
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- Vacant or Under 50 years of age
- Officially Determined Not Eligible
- Needs Data
- I-270 Conceptual Design
- Area of Potential Effects (APE)



Appendix C. Right-of-way Acquisition or Easement Map

Consultant Work Product - Jacobs Engineering
-Not CDOT Approved-

Right-of-way Acquisition or Easement Map



Map Label Sheet

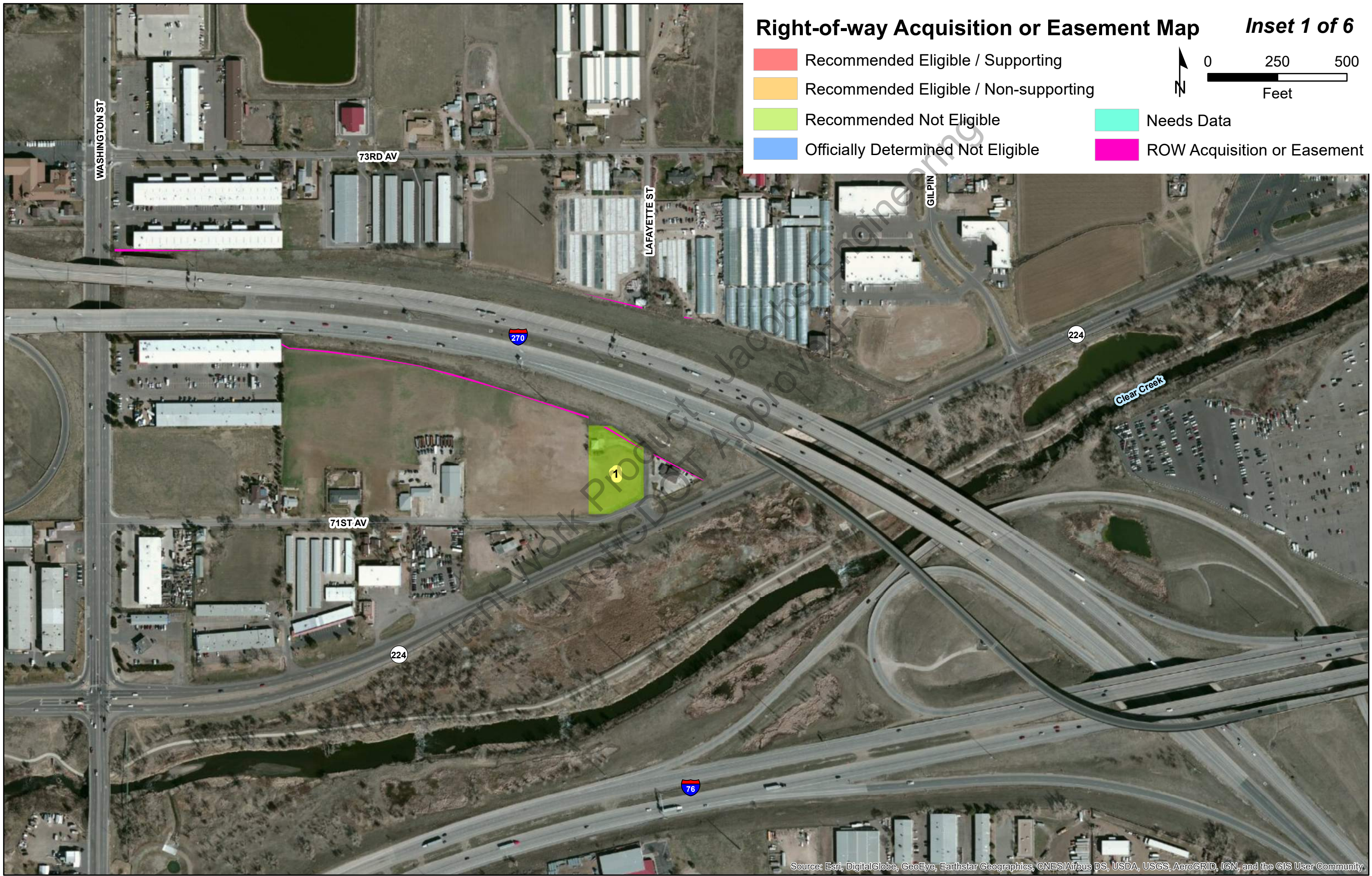
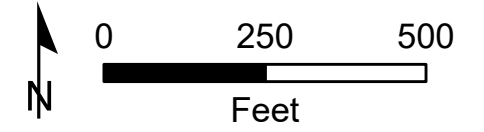
Sites that are greyed out are not shown on map.

Map Label	Site Name or Address	Site Number	Map Label	Site Name or Address	Site Number
1	7141 Lafayette St	5AM.4107	37	Sand Creek Bridge - E-17-AT (Vasquez Boulevard/US 6)	5AM.1363
2	Larusso Subdivision	5AM.4031	38	5555 Dahlia St	5AM.1850
3	2101 E 68th Ave	5AM.4032	39	5820 Dahlia St	5AM.4112
4	2180 E 68th Ave	5AM.4034	40	5790 Dahlia St	5AM.4070
5	York St	5AM.4101.2	41	5665 Eudora St, 5775 Eudora St, 5675 Eudora St	5AM.4122
6	6875 Columbine St	5AM.4108	42	5678 Eudora St	5AM.4113
7	6824 Columbine St	5AM.4109	43	5200 E 56th Ave	5AM.1849
8	6820 York St	5AM.4035	44	5455 Holly St	5AM.4114
9	6776 York St	5AM.4036	45	5431 Krameria St	5AM.1899
10	6780 York St	5AM.4037	46	5421 Krameria St	5AM.4115
11	6750 York St	5AM.4038	47	5450 Krameria St	5AM.4144
12	6702 York St	5AM.4039	48	5410 Krameria St	5AM.1845
13	6700 York St	5AM.4040	49	6320 E 54th Ave	5AM.1847
14	6690 York St	5AM.4041	50	6330 E 54th Ave	5AM.1848
*	6680 York St (property not surveyed)	-	51	5401 Locust St	5AM.1904
15	6620 York St	5AM.4043	52	6411 E 53rd Way	5AM.1893
16	6610 York St	5AM.4042	53	5400 Locust St	5AM.4145
17	Gardner's Ditch Segment	5AM.1292.2	54	6415 E 53rd Way	5AM.1894
18	Burlington Ditch/O'Brien Canal	5AM.465.9	55	6481 E 53rd Way	5AM.1895
19	E 64th Ave Bridge Over Burlington Ditch/O'Brien Canal	5AM.4118	56	6470 E 53rd Way	5AM.1846
20	5801 Brighton Blvd	5AM.4044	57	5360 Monaco St	5AM.1907
20	5800 Brighton Rd	5AM.4044	58	5401 Magnolia St	5AM.4146
20	3875 E 56th Ave	5AM.4044	59	5350 Magnolia St	5AM.1905
21	Brighton Boulevard / State Highway (SH) 265	5AM.2410.2	60	5358 Magnolia St	5AM.4147
22	Union Pacific Railroad Siding At Suncor Energy	5AM.4121.1	61	5366 Magnolia St	5AM.1906
23	Union Pacific Railroad Bridge - COMC-4A-02-RR (UP Suncor Siding)	5AM.1416	62	5360 Magnolia St	5AM.1842
24	Union Pacific Railroad Bridge - COMC-4A-02-RR (DP/UP)	5AM.1417	63	5311 Niagara St	5AM.1843
25	Baltimore Northern Santa Fe Railroad Bridge - COMC-5A-03-RR (CB&Q/BNSF)	5AM.1418	64	5345 Newport St	5AM.4116
26	Chicago Burlington & Quincy BNSF - Brush Line Segment	5AM.464.21	65	5280 Newport St	5AM.4117
27	Sand Creek Junction	5AM.4119	66	6820 E 52nd Pl	5AM.1891
28	Denver Pacific / Union Pacific Railroad Segment—Greeley Line	5AM.472.41	67	6830 E 52nd Pl	5AM.1892
29	4100 E 60th Ave	5AM.4045	68	6860 E 52nd Pl	5AM.4048
30	Chicago, Burlington & Quincy BNSF - Market St Line Segment	5AM.1298.3	69	6870 E 52nd Pl	5AM.4148
31	4306 E 60th Ave	5AM.4111	70	5251 Oneida St	5AM.4049
32	4310 E 60th Ave	5AM.4110	71	5250 Oneida St	5AM.4050
33	5699 Dexter St	5AM.4046			
34	5701 Dexter St	5AM.4047			
35	Vasquez Boulevard / U.S. Highway (US) 6 / CDOT Route 006H	5AM.3924.3			
36	Vasquez Boulevard / U.S. Highway (US) 6 / U.S. Highway (US) 85 / State Highway (SH) 2	5AM.3924.2			

Right-of-way Acquisition or Easement Map

Inset 1 of 6

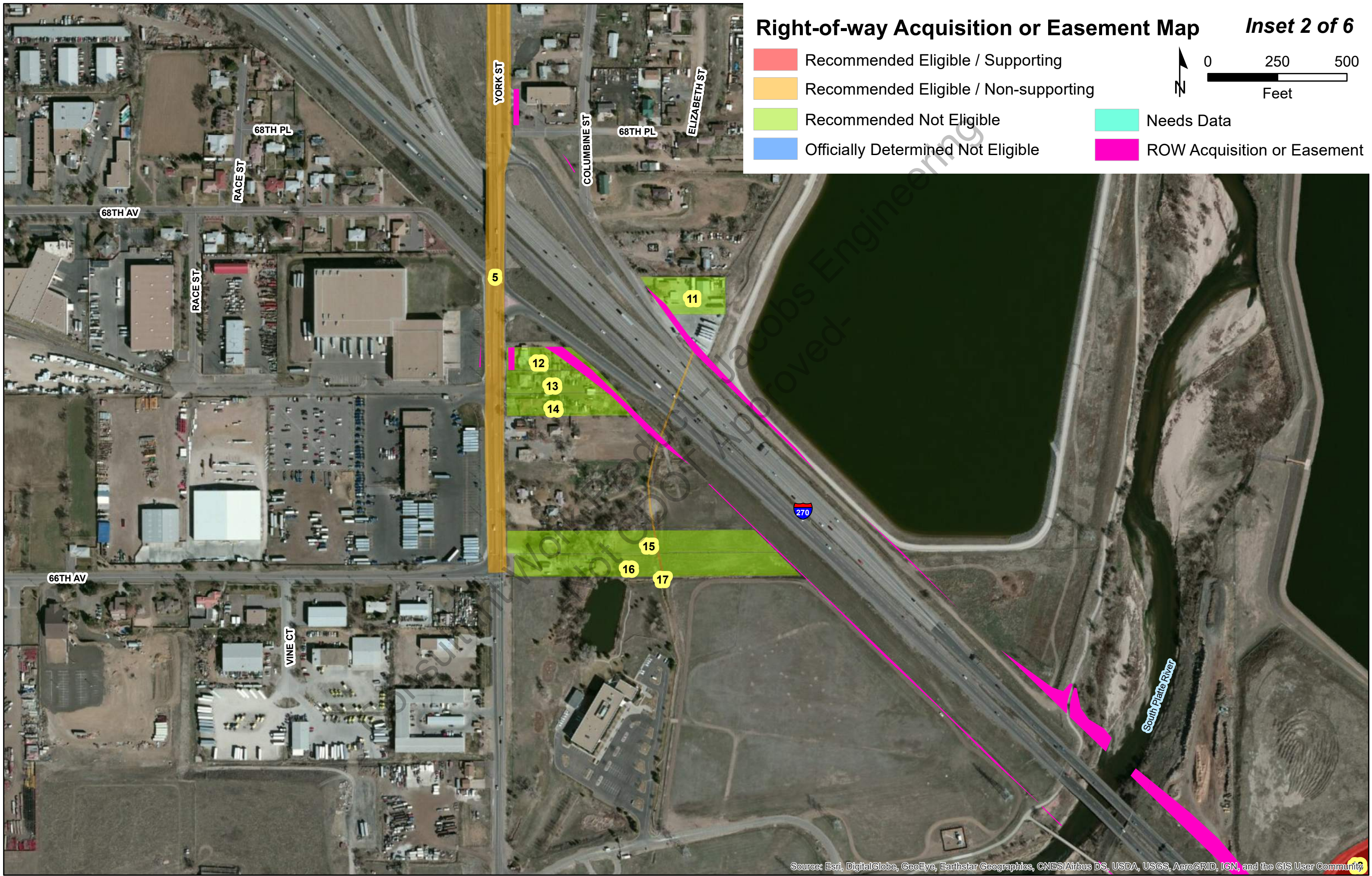
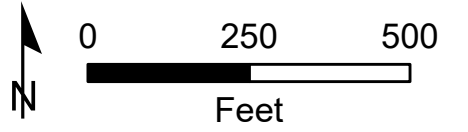
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- █ Recommended Eligible / Non-supporting
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- █ Needs Data
- █ ROW Acquisition or Easement



Right-of-way Acquisition or Easement Map

Inset 2 of 6

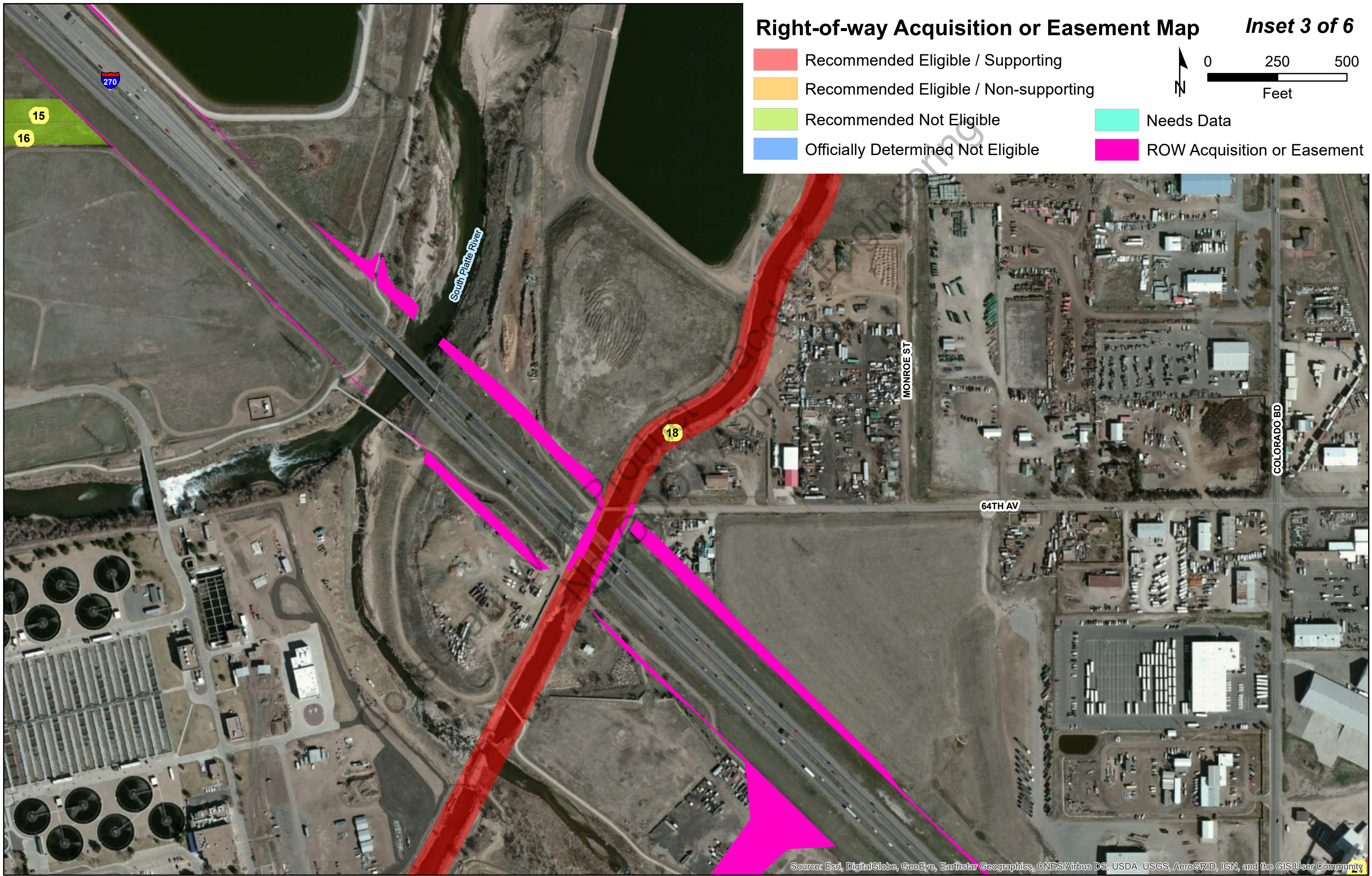
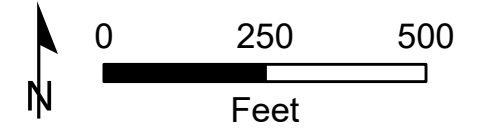
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- Needs Data
- ROW Acquisition or Easement



Right-of-way Acquisition or Easement Map

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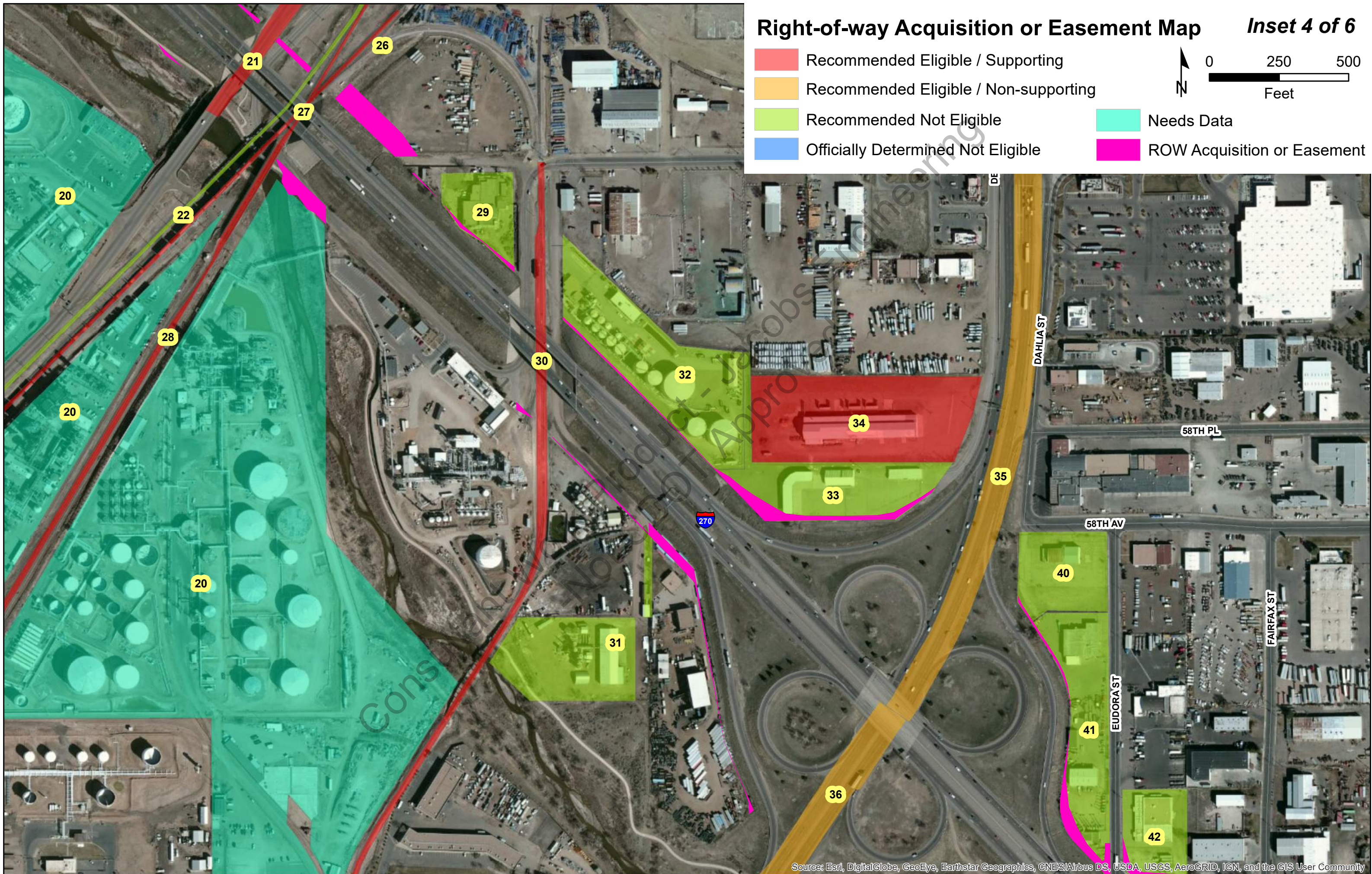
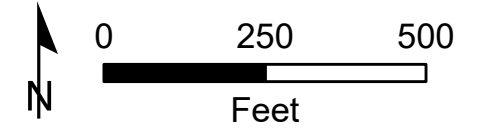
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- Needs Data
- ROW Acquisition or Easement



Right-of-way Acquisition or Easement Map

Inset 4 of 6

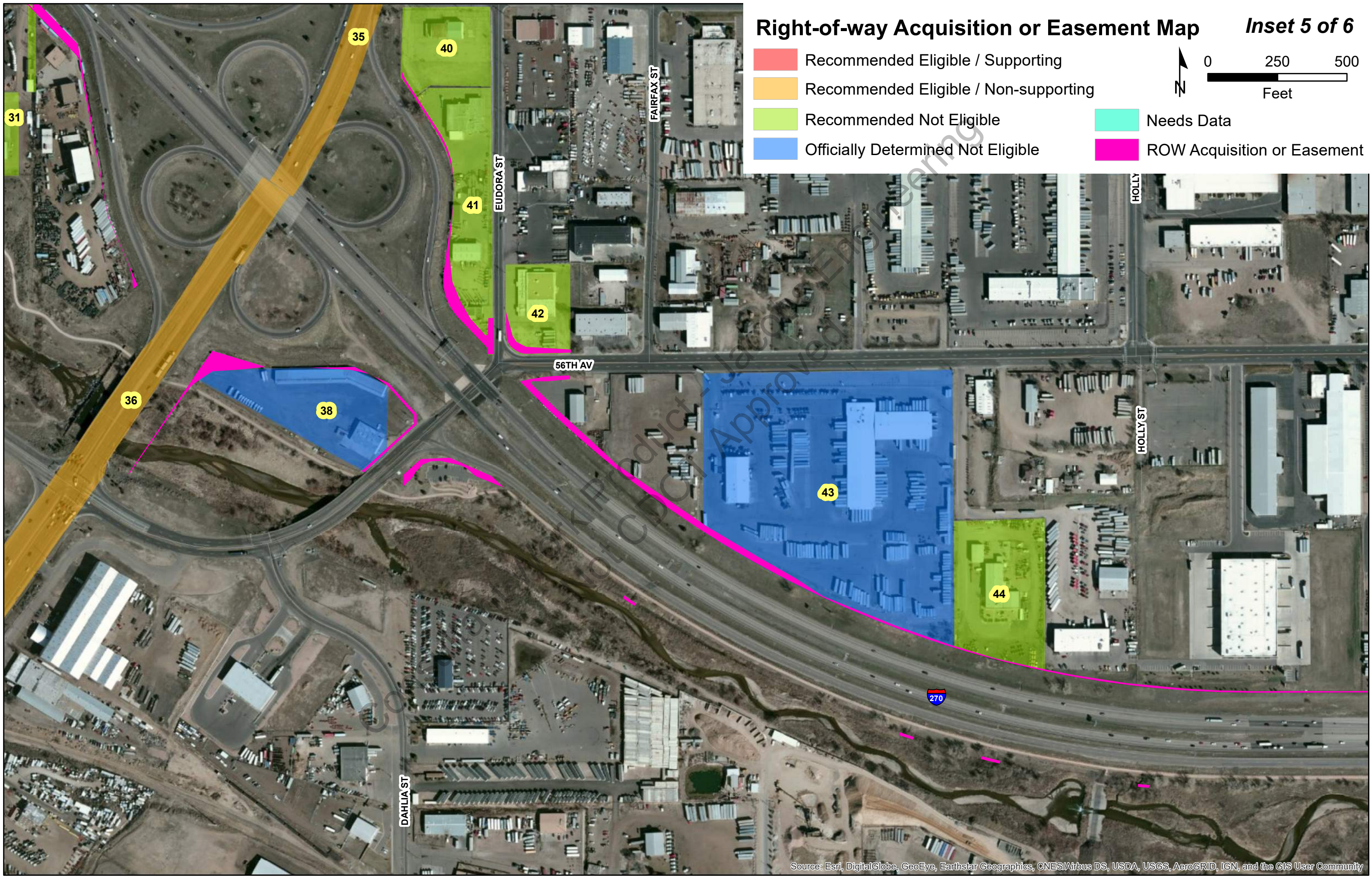
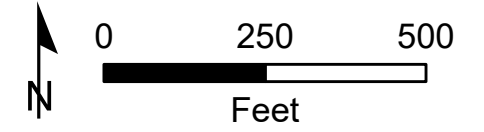
- █ Recommended Eligible / Supporting
- █ Recommended Eligible / Non-supporting
- █ Recommended Not Eligible
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- █ Needs Data
- █ ROW Acquisition or Easement



Right-of-way Acquisition or Easement Map

Inset 5 of 6

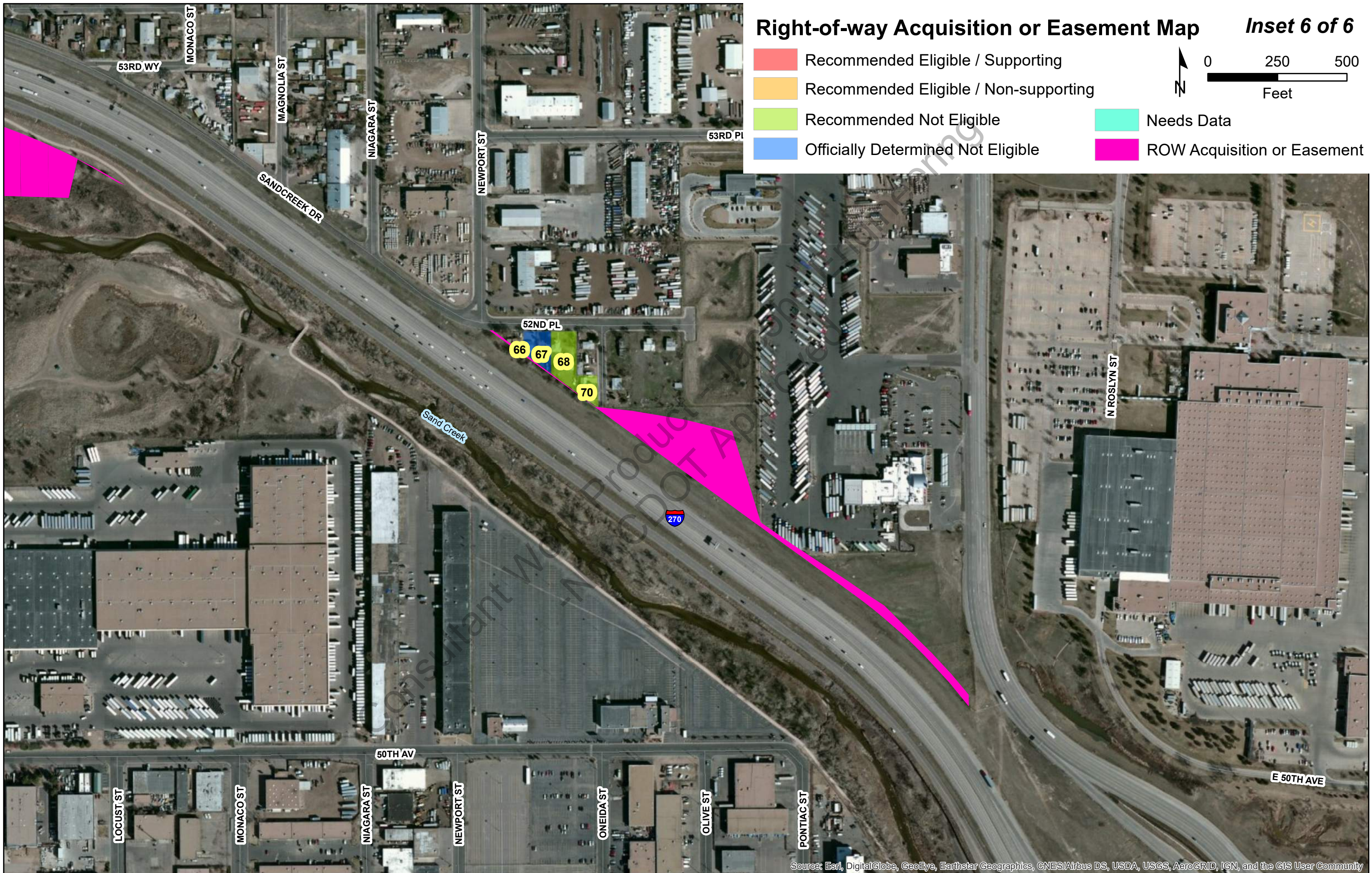
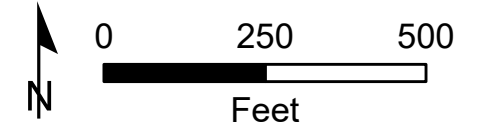
- Recommended Eligible / Supporting
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- Recommended Not Eligible
- Officially Determined Not Eligible
- Needs Data
- ROW Acquisition or Easement



Right-of-way Acquisition or Easement Map

Inset 6 of 6

- Recommended Eligible / Supporting
- Recommended Eligible / Non-supporting
- Recommended Not Eligible
- Officially Determined Not Eligible
- Needs Data
- ROW Acquisition or Easement



Appendix D. OAHF Site Forms (submitted separately)

Consultant Work Product - Jacobs Engineering
-Not CDOT Approved-