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US HWY 24
ACCESS CONTROL PLAN
PETERSON BLVD. to ELBERT HWY.

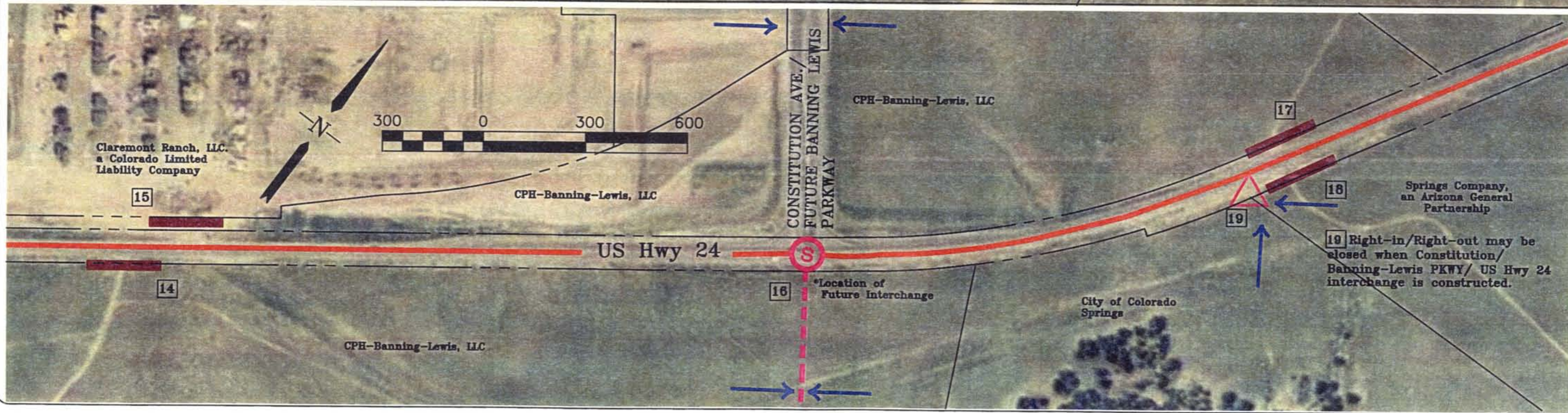
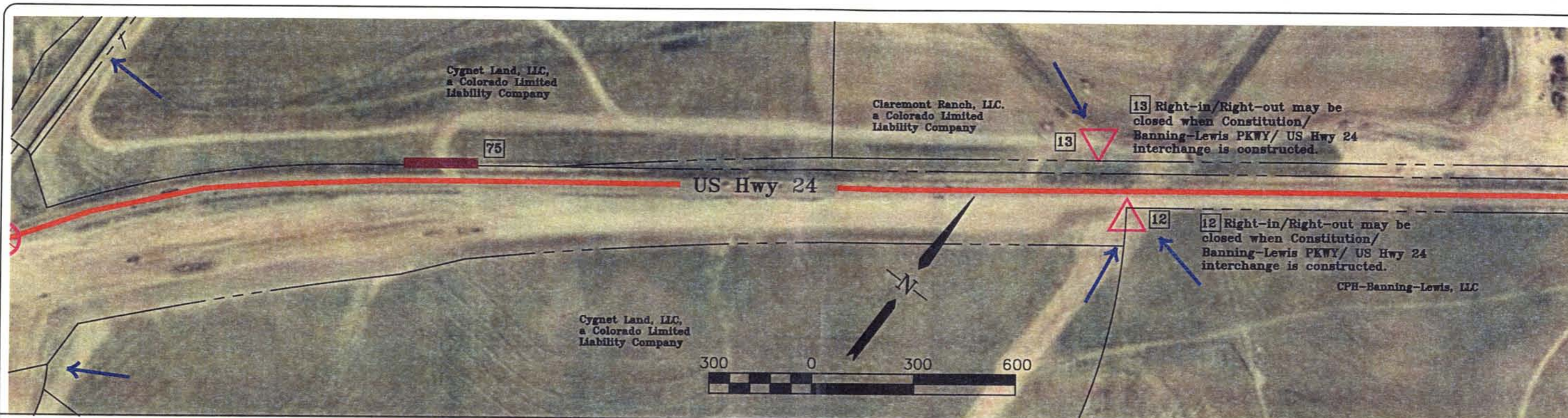


JAN 2005
 Aerial Photo 2003

| LEGEND | |
|--------|-----------------------------------|
| | Current Signalized, Full Movement |
| | Future Signalized, Full Movement |
| | RIRO (right-in-right-out only) |
| | Access I.D. Number |
| | Access Closed |
| | Future Roadway |
| | Median Barrier |
| | Combined Access |
| | Possible Access |



EXHIBIT B
FIGURE 1



US HWY 24
ACCESS CONTROL PLAN
PETERSON BLVD. to ELBERT HWY.



JAN
2005
Aerial Photo 2003

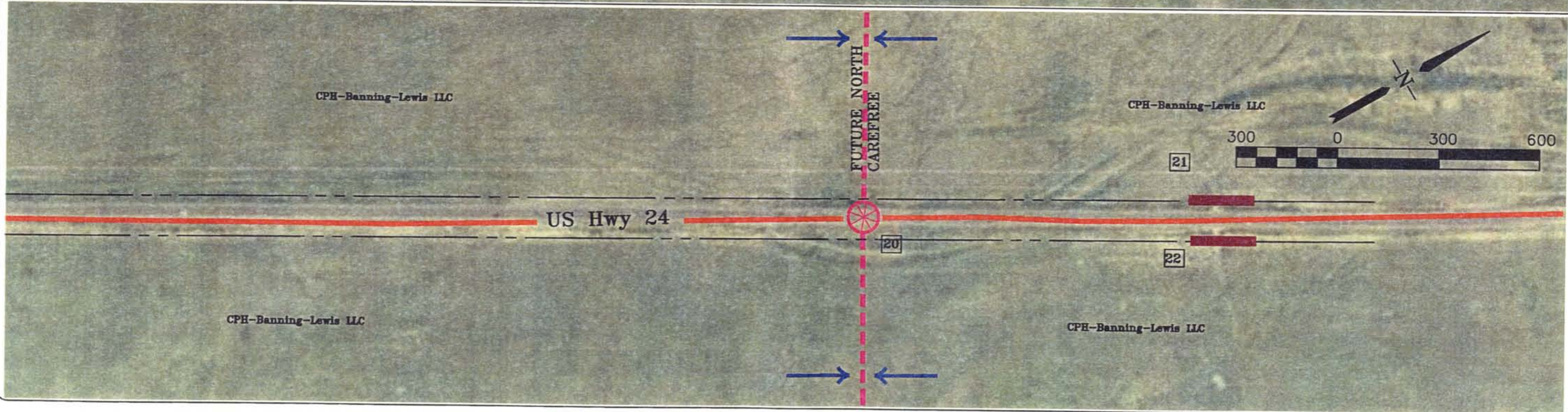
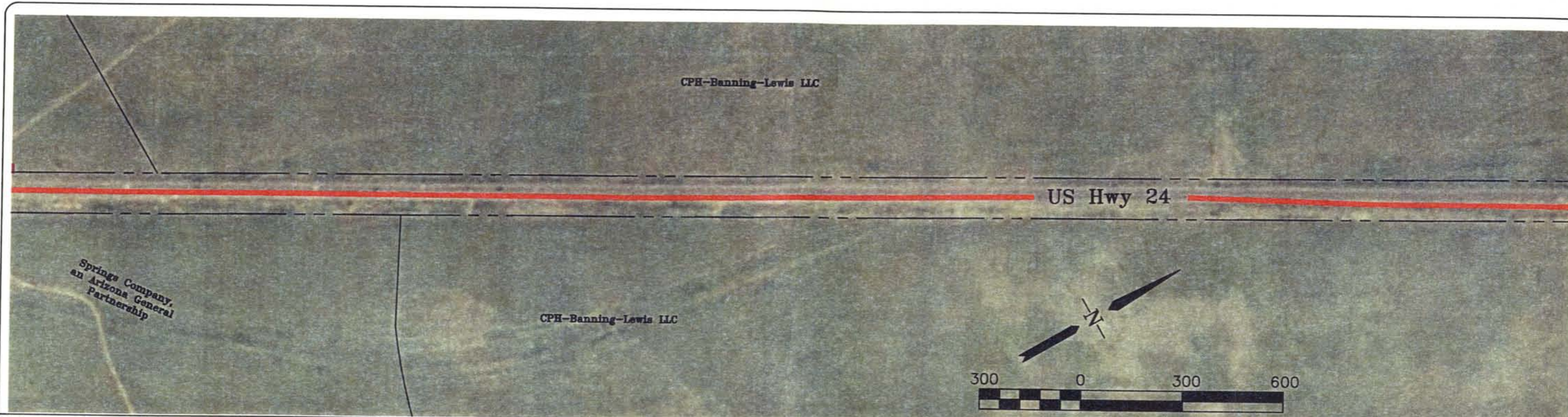
- LEGEND**
- ⊙ Current Signalized, Full Movement
 - ⊗ Future Signalized, Full Movement
 - △ RIRO (right-in-right-out only)

- # Access I.D. Number
- Access Closed
- - - Future Roadway
- Median Barrier
- Combined Access
- Possible Access



EXHIBIT B
FIGURE 2

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US HWY 24
ACCESS CONTROL PLAN
PETERSON BLVD. to ELBERT HWY.



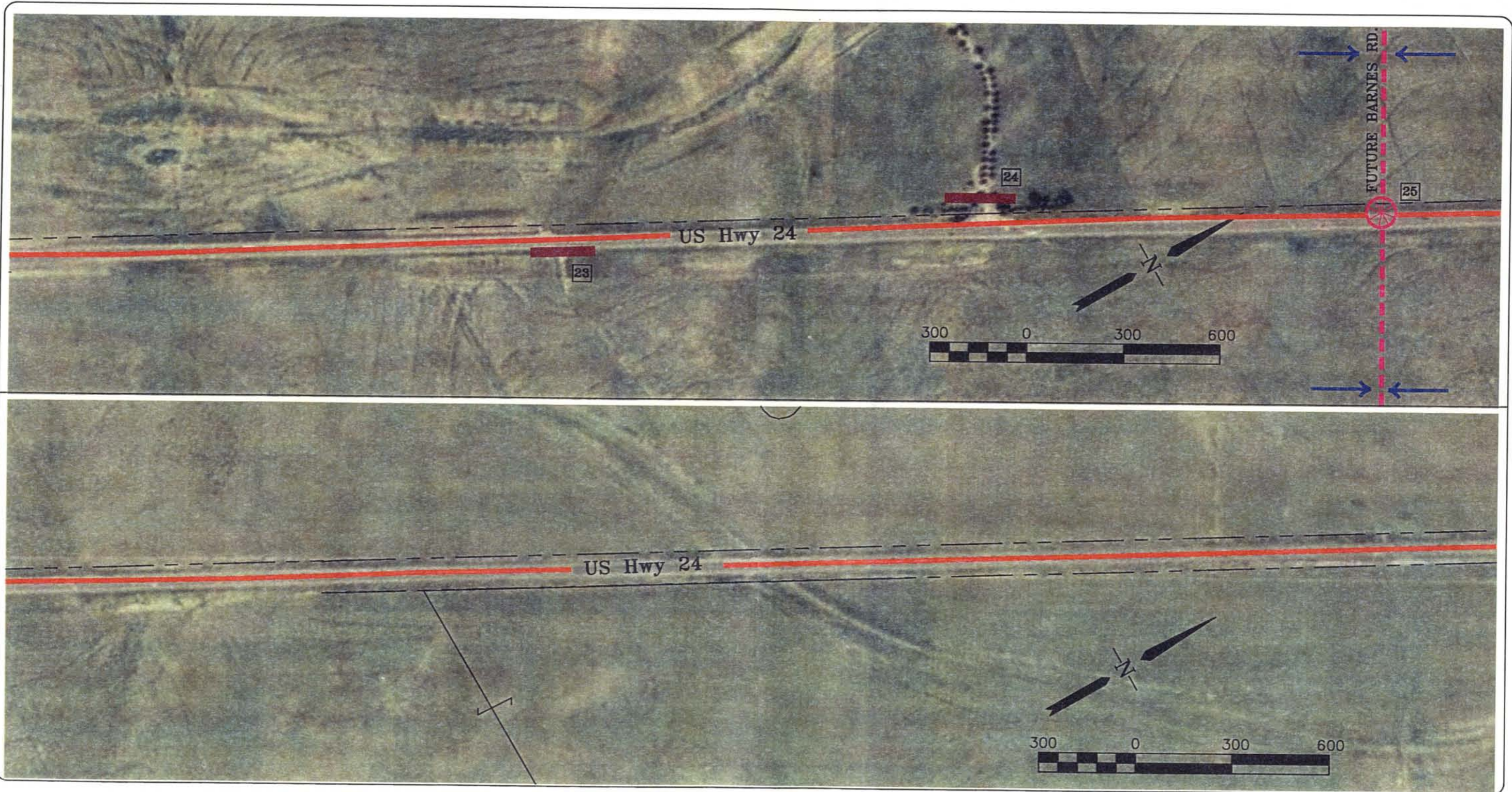
JAN
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Aerial Photo 2003

| LEGEND | |
|--------|-----------------------------------|
| ⊙ | Current Signalized, Full Movement |
| ⊗ | Future Signalized, Full Movement |
| △ | RIRO (right-in-right-out only) |
| # | Access I.D. Number |
| — | Access Closed |
| - - - | Future Roadway |
| — | Median Barrier |
| — | Combined Access |
| → | Possible Access |



EXHIBIT B
FIGURE 3

\\S031ntfile\OUT-OF-STATE\Denver\US24AMP\2004\New Fig 4.dwg 1/25/2005 9:30:43 AM MST



US HWY 24
 ACCESS CONTROL PLAN
 PETERSON BLVD. to ELBERT HWY.

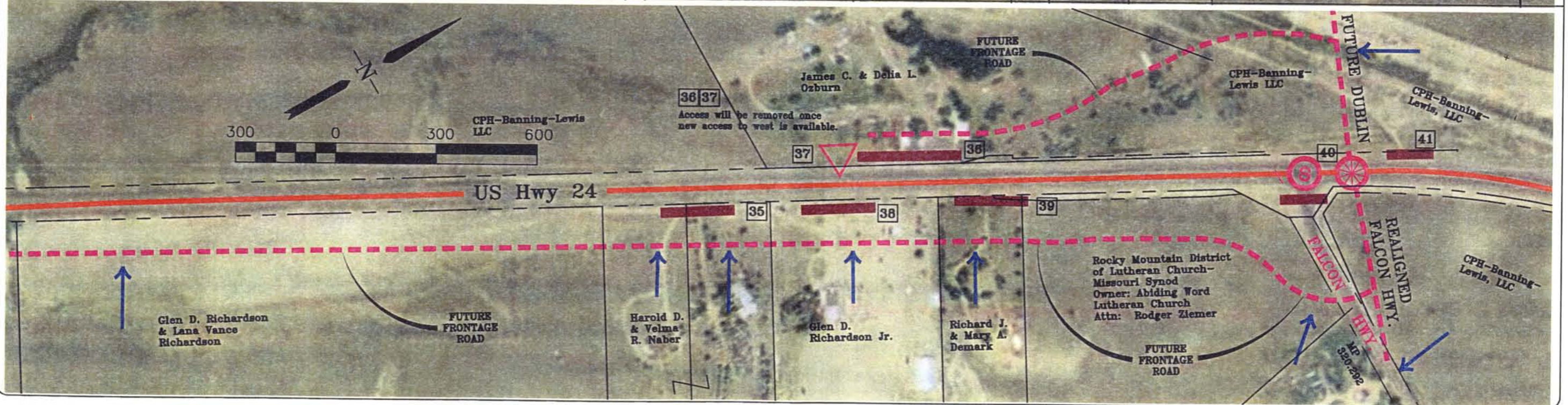
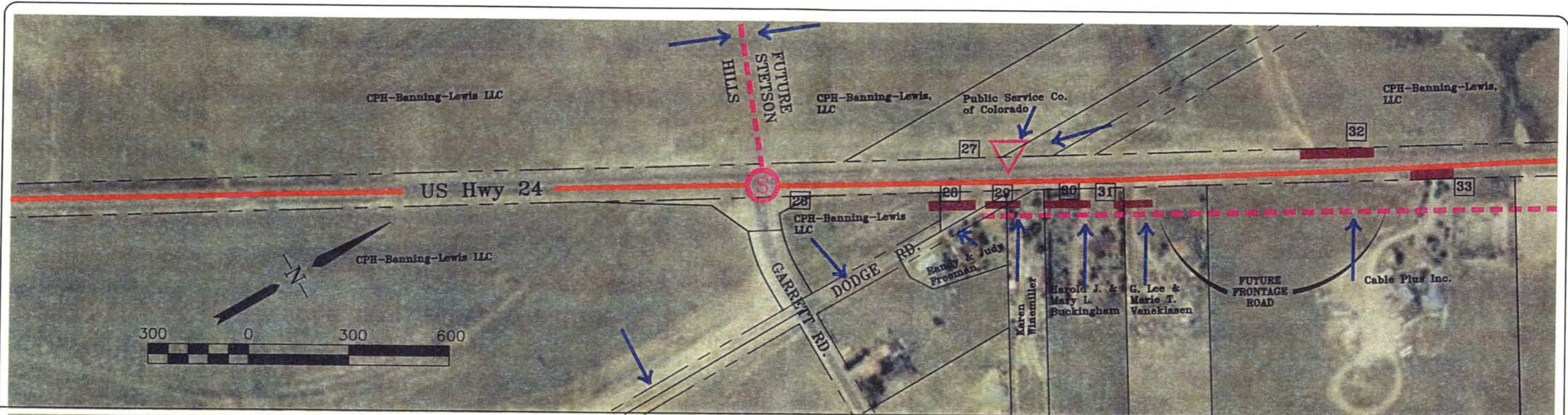


JAN
 2005
 Aerial Photo 2003


| LEGEND | |
|--------|-----------------------------------|
| ⊙ | Current Signalized, Full Movement |
| ⊛ | Future Signalized, Full Movement |
| △ | RIRO (right-in-right-out only) |
| # | Access I.D. Number |
| — | Access Closed |
| - - - | Future Roadway |
| — | Median Barrier |
| — | Combined Access |
| → | Possible Access |



EXHIBIT B
 FIGURE 4



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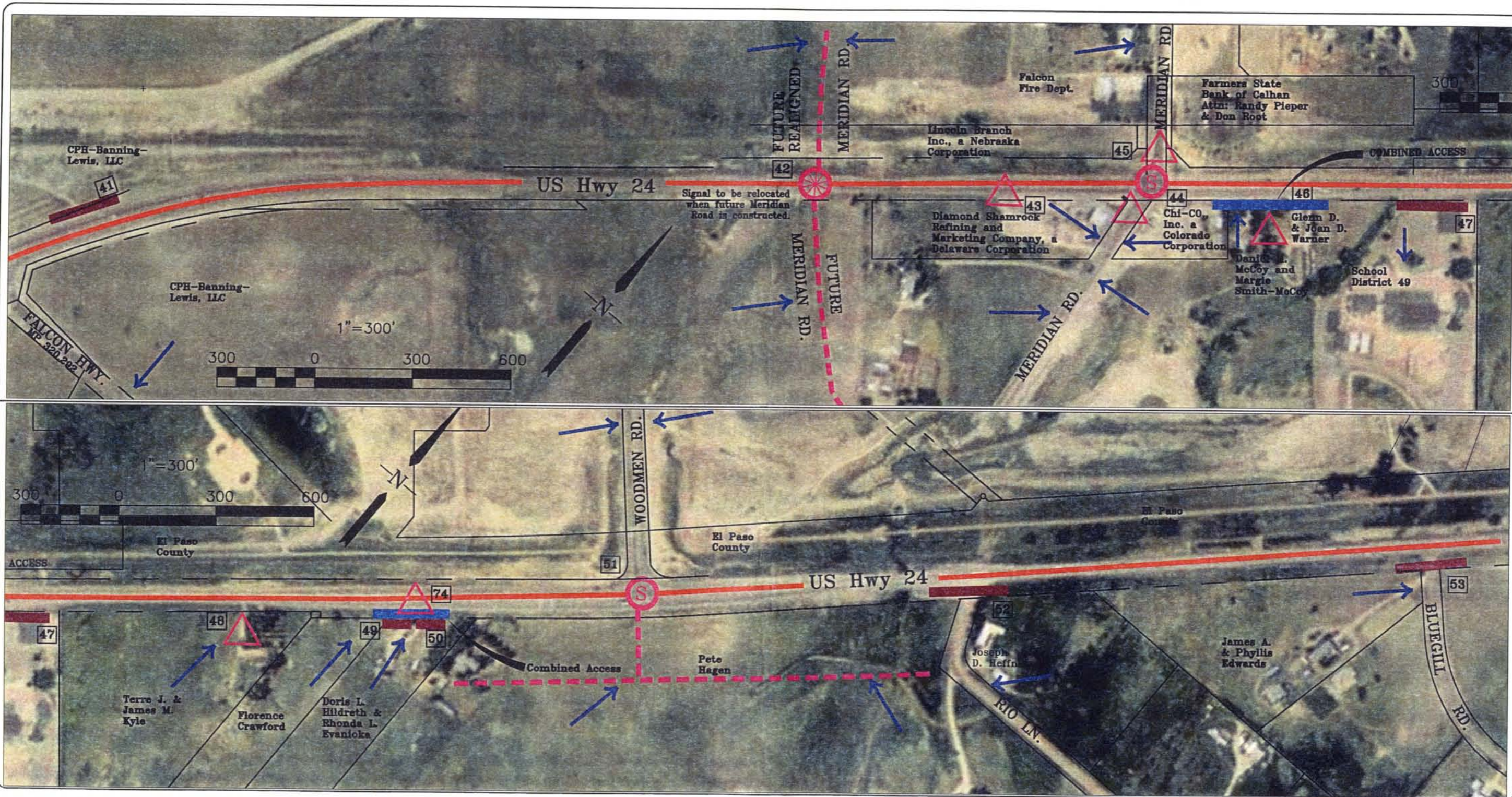

US HWY 24
ACCESS CONTROL PLAN
PETERSON BLVD. to ELBERT HWY.

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2005
 Aerial Photo 2003

| LEGEND | |
|--------|-----------------------------------|
| ⊙ | Current Signalized, Full Movement |
| ⊗ | Future Signalized, Full Movement |
| △ | RIRO (right-in-right-out only) |
| # | Access I.D. Number |
| — | Access Closed |
| - - - | Future Roadway |
| = | Median Barrier |
| — | Combined Access |
| → | Possible Access |



EXHIBIT B
FIGURE 5



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US HWY 24
ACCESS CONTROL PLAN
PETERSON BLVD. to ELBERT HWY.



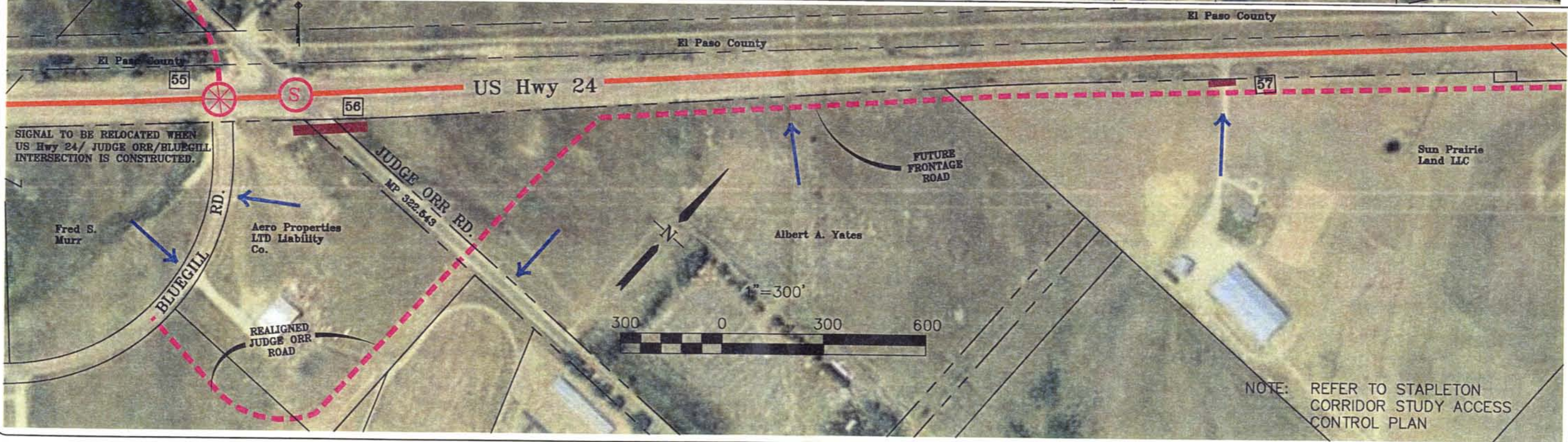
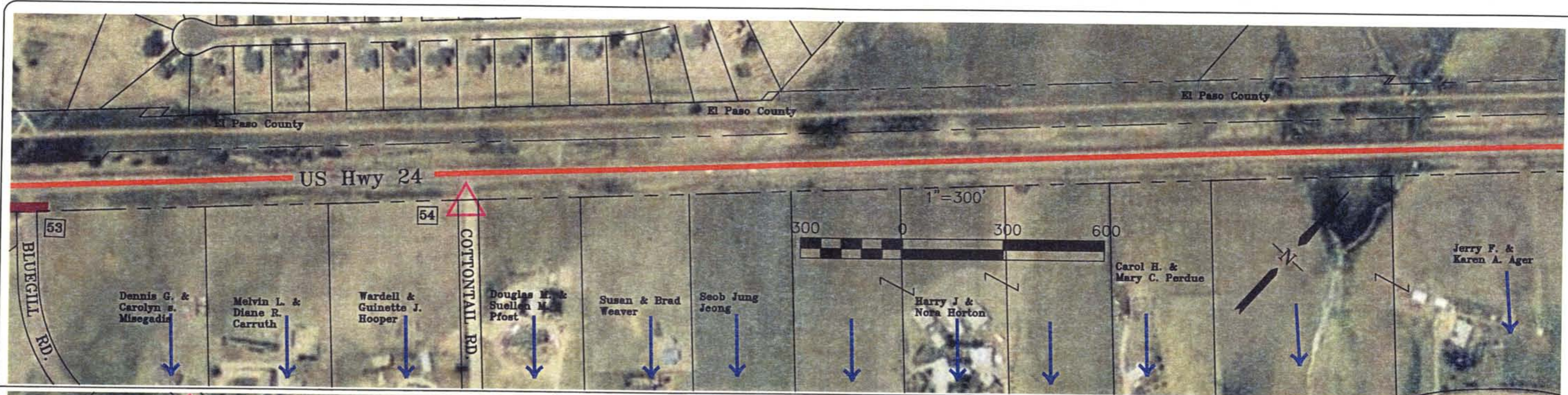
JAN
2005
 Aerial Photo 2003

- LEGEND**
- ⊙ Current Signalized, Full Movement
 - ⊗ Future Signalized, Full Movement
 - △ RIRO (right-in-right-out only)

- # Access I.D. Number
- Access Closed
- - - Future Roadway
- Median Barrier
- Combined Access
- Possible Access



EXHIBIT B
FIGURE 6



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US HWY 24
ACCESS CONTROL PLAN
PETERSON BLVD. to ELBERT HWY.



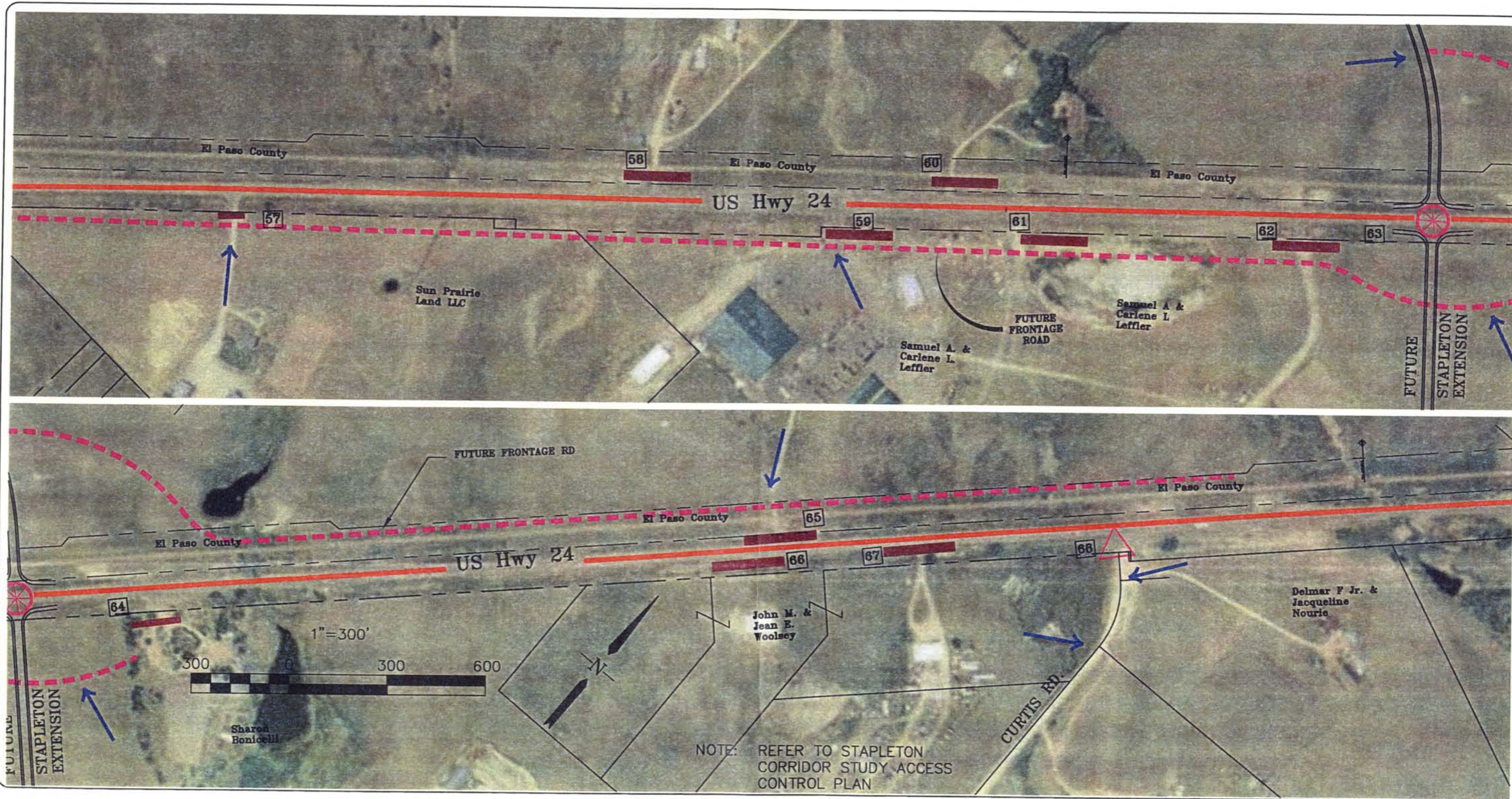
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 Aerial Photo 2003

| LEGEND | |
|--------|-----------------------------------|
| | Current Signalized, Full Movement |
| | Future Signalized, Full Movement |
| | RIRO (right-in-right-out only) |
| | Access I.D. Number |
| | Access Closed |
| | Future Roadway |
| | Median Barrier |
| | Combined Access |
| | Possible Access |



EXHIBIT B
FIGURE 7

NOTE: REFER TO STAPLETON CORRIDOR STUDY ACCESS CONTROL PLAN



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US HWY 24
ACCESS CONTROL PLAN
 PETERSON BLVD. to ELBERT HWY.

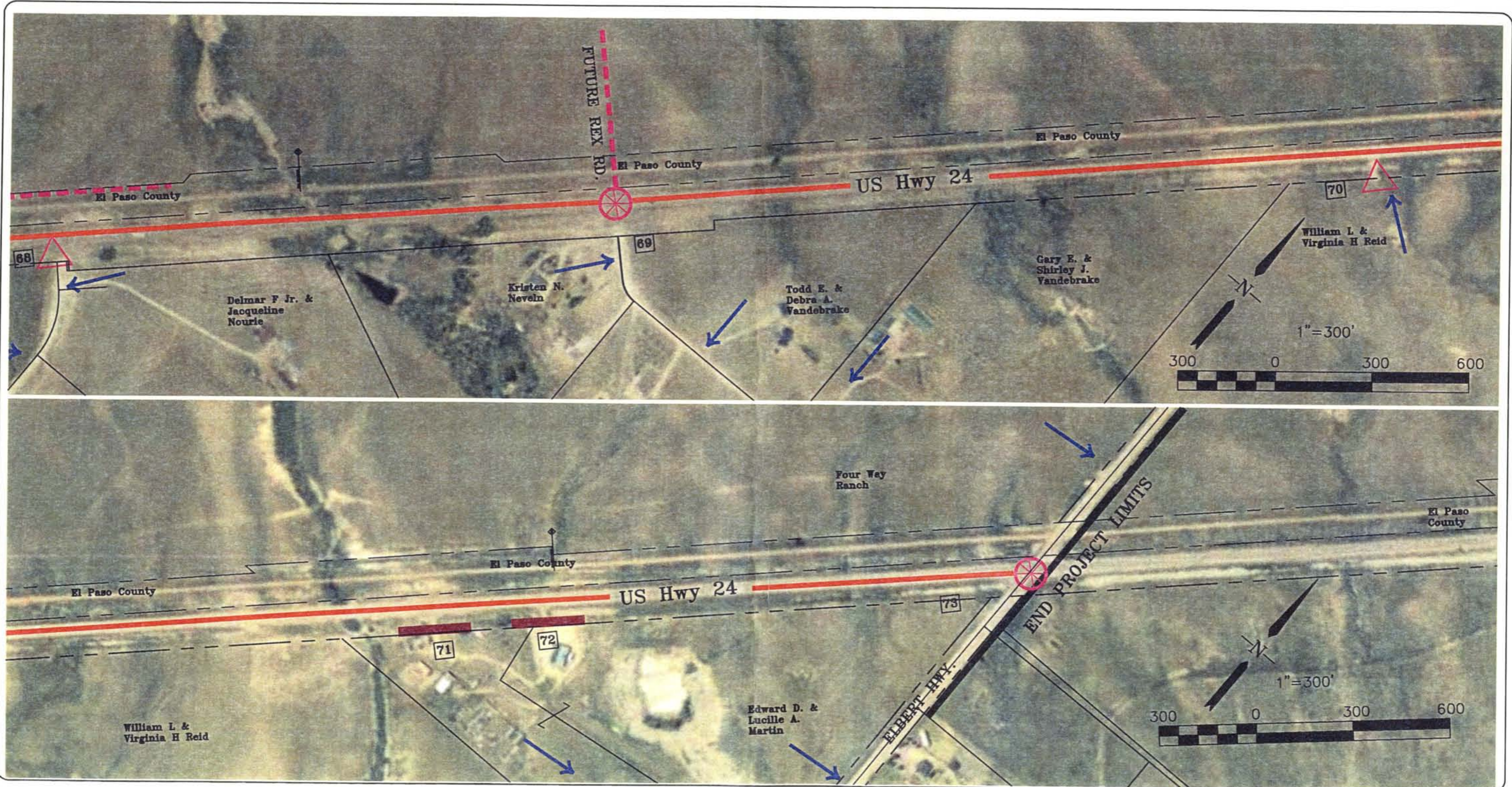


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2005
 Aerial Photo 2003

| LEGEND | |
|-----------|-----------------------------------|
| ⊙ | Current Signalized, Full Movement |
| ⊗ | Future Signalized, Full Movement |
| △ | RIRO (right-in-right-out only) |
| # | Access I.D. Number |
| — | Access Closed |
| - - - | Future Roadway |
| - · - · - | Median Barrier |
| ↔ | Combined Access |
| → | Possible Access |



EXHIBIT B
FIGURE 8



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US HWY 24
ACCESS CONTROL PLAN
PETERSON BLVD. to ELBERT HWY.



JAN 2005
 Aerial Photo 2003

LEGEND

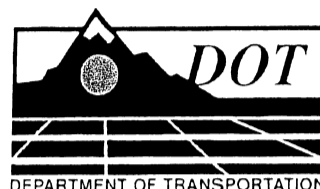
| | |
|-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| <ul style="list-style-type: none"> ⊙ Current Signalized, Full Movement ⊗ Future Signalized, Full Movement △ RIRO (right-in-right-out only) | <ul style="list-style-type: none"> # Access I.D. Number — Access Closed - - - Future Roadway — Median Barrier — Combined Access → Possible Access |
|-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|



EXHIBIT B
FIGURE 9

STATE OF COLORADO

COLORADO DEPARTMENT OF TRANSPORTATION
Region Two - Colorado Springs Residency
1480 Quail Lake Loop, Ste. A (2110) (PPACG1)
Colorado Springs, Colorado 80906
(719) 634-2323 / Fax (719) 227-3298



NH 0242-058
S.A. 12412
US 24 Access Control Plan

Date: August 30, 2006

To: Tim Harris, Region 2 RTD
Dave Poling, North Program Engineer
Sasan Delshad, Region 2 Traffic Engineer

RE: US 24 Access Control Plan and IGA

Please find the attached IGA and Access Control Plan approved by affidavit on June 1, 2006 by the CDOT Chief Engineer, Craig Siracusa and Jennifer Webster, Chief Clerk. Please note that Bob Torres approved the original IGA July 26, 2005. If you have any questions, please give me a call at (719) 227-3205

Sincerely,
COLORADO DEPARTMENT OF TRANSPORTATION

Mark S. Andrew
CDOT Region 2 Resident Engineer

Copies To:

Craig Blewitt, City of Colorado Springs (3)
John McCarty, El Paso County (3)
Carl Schueler, El Paso County Planning (3)
John O'connor, URS (1)
Natalie Lovell, URS (1)
Dave Poling, CDOT North Program Engineer (1)
Tim Harris, CDOT RTD (1)
Sasan Delshad, CDOT Region 2 Traffic Engineer (2)
File: NH 0242-058 (1)



**INTERGOVERNMENTAL AGREEMENT
BETWEEN
EL PASO COUNTY,
THE CITY OF COLORADO SPRINGS,
AND
THE STATE OF COLORADO
DEPARTMENT OF TRANSPORTATION**

THIS AGREEMENT is entered into effective as of the ____ day of _____ 2005, by and among El Paso County, the City of Colorado Springs, (hereafter referred to collectively as the "City and County"), and the State of Colorado, Department of Transportation (hereafter referred to as the "Department"), all of said parties being referred to collectively herein as the "Agencies."

RECITALS:

A. The Agencies are authorized by the provisions of Article XIV, Section 18(2)(a), Colorado Constitution, and Sections 29-1-201, *et. seq.*, C.R.S., to enter into contracts with each other for the performance of functions which they are authorized by law to perform on their own; and

B. Each Agency is authorized by Section 43-2-147(1)(a), C.R.S., to regulate access to public highways within its jurisdiction; and

C. The coordinated regulation of vehicular access to public highways is necessary to maintain the efficient and smooth flow of traffic, to reduce the potential for traffic accidents, to protect the functional level and optimize the traffic capacity, to provide an efficient spacing of traffic signals; and

D. The Agencies desire to provide for the coordinated regulation of vehicular access for the section of US Highway 24 between Peterson Boulevard (MP 311.746) and Elbert Highway MP 325.81 (hereafter referred to as the "Segment"), which is within the jurisdiction of the Agencies; and

E. The Agencies are authorized pursuant to Section 2.12 of the 1998 State Highway Access Code, 2 C.C.R. 601-1(the "Access Code") to enter into a written agreement adopting and implementing a comprehensive and mutually acceptable highway access control plan for the Segment for the purposes above recited; and

F. The Agencies specifically find and determine that this Access Control Plan is a necessary exercise of the Agencies' legislative, governmental or police powers to promote and protect the public health, safety and general welfare of the citizens of the City, County, and the State; and

G. The development of this Access Control Plan adheres to the requirements of the Access Code, Section 2.12.

NOW THEREFORE, for and in consideration of the mutual promises, agreements, and commitments herein contained, the Agencies agree as follows:

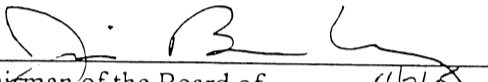
1. The Access Control Plan dated January 2005, for the Segment (hereinafter referred to as the "Access Control Plan") is attached hereto as Exhibit A and incorporated herein. The Access Control Plan illustration, dated January 2005, is attached hereto as Exhibit B, and is incorporated herein by this reference. The Access Control Plan Amendment Process is attached hereto as Exhibit C and incorporated herein by this reference.
2. The Agencies shall regulate access to the Segment in compliance with this Access Control Plan, Section 43-2-147, C.R.S. (the "Access Law"), and the applicable sections of the Access Code. Vehicular access to the Segment may be permitted only when such access is in compliance with this Agreement, this Access Control Plan, the Access Law and the applicable sections of the Access Code. Per section 2.12(a) of the Access Code, design waivers necessary for access design and construction within state highway right-of-way, may be approved by the approved Regional Transportation Director, or his/her approved designee.
3. Accesses that were in existence in compliance with the Access Law prior to the effective date of this Agreement may continue in existence until such time as a change in the access is required by this Access Control Plan, the Access Law, in the course of highway reconstruction, or as determined appropriate in the course of development or subdivision actions by the City and/or County. When closure, modification, or relocation of access is necessary or required, the Agency(ies) having jurisdiction shall utilize appropriate legal process to effect such action.
4. Actions taken by any Agency with regard to transportation planning, transportation facilities, and traffic operations within the areas described in the Access Control Plan shall be in conformity with this Agreement. The City and County agree to develop and adopt the necessary resolutions, ordinances, official documents, plans and maps that are necessary to fulfill their responsibilities under this agreement.
5. Parcels of real property created after the effective date of this Agreement, which adjoin the Segment, shall not be provided with direct access to the Segment unless the location, use and design thereof conform to the provisions of this Agreement except in unforeseen circumstances.
6. This Agreement is based upon and is intended to be consistent with the Access Law and the Access Code.
7. This Agreement does not create any current specific financial obligation for any of the Agencies. Any future financial obligation of any Agency shall be subject to the execution of an appropriate encumbrance document, where required. Agencies involved in or affected by any particular or site-specific undertaking provided for herein will cooperate with each other to agree upon a fair and equitable allocation of the costs associated therewith, but, notwithstanding any provision of this Agreement, no Agency shall be required to expend its public funds for such undertaking without the express prior approval of its governing body or director. All financial obligations of the Agencies hereunder shall be contingent upon sufficient funds therefore being appropriated, budgeted, and otherwise made available.

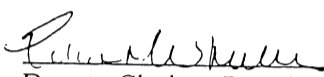
8. Should any section(s) or provision(s) of this Agreement be judicially determined to be invalid or unenforceable, such judgment shall not affect, impair or invalidate the remaining provisions of this Agreement, the intention being that the various provisions hereof are severable.
9. This Agreement supersedes and controls all prior written and oral agreements and representations of the Agencies concerning regulating vehicular access to the segment. No additional or different oral representation, promise or agreement shall be binding on any Agency. This Agreement may be amended or terminated only in writing executed by the Agencies with express authorization from their respective governing bodies or legally designated officials. To the extent that this Access Control Plan, attached as Exhibit A to this Agreement, requires modification because of a change, closure, relocation, consolidation or addition of an access, the Agencies may amend the attached Exhibit A so long as the amendment to the Access Control Plan is executed in writing and amended in accord with the Access Law and Access Code. The Access Control Plan Amendment Process has been included in Exhibit C.
10. By signing this Agreement, the Agencies acknowledge and represent to one another that all procedures necessary to validly contract and execute this Agreement have been performed, and that the persons signing for each Agency have been duly authorized to sign.
11. No portion of this Agreement shall be deemed to constitute a waiver of any immunities the parties or their officers or employees may possess, nor shall any portion of this Agreement be deemed to have created a duty of care which did not previously exist with respect to any person not a party to this Agreement.
12. It is expressly understood and agreed that the enforcement of the terms and conditions of this Agreement, and all rights of action relating to such enforcement, shall be strictly reserved to the undersigned parties and nothing in this Agreement shall give or allow any claim or right of action whatsoever by any other person not included in this Agreement. It is the express intention of the undersigned parties that any entity other than the undersigned parties receiving services or benefits under this Agreement shall be an incidental beneficiary only.

IN WITNESS WHEREOF, the Agencies have executed this Agreement effective as of the day and year first above written.

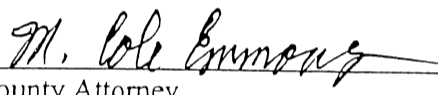
El Paso County, Colorado

ATTEST:


Chairman of the Board of ^{4/7/05}
County Commissioners, El Paso County

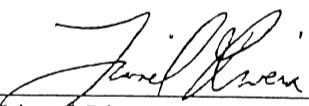

Deputy Clerk to Board

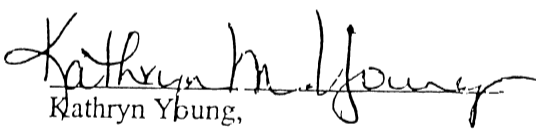
Approved as to Form:


County Attorney

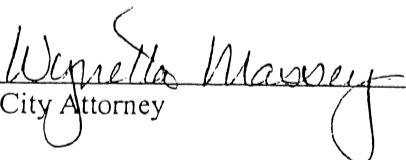
City of Colorado Springs, Colorado

ATTEST:


Lionel Rivera,
Mayor, City of Colorado Springs


Kathryn Young,
City Clerk

Approved as to Form:


City Attorney

State of Colorado
Department of Transportation

ATTEST:

Craig Siracusa,
Chief Engineer

Jennifer Webster,
Chief Clerk

CONCUR:

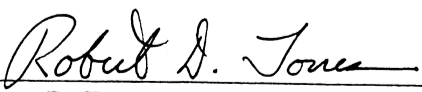

Robert D. Torres,
Regional Transportation Director

EXHIBIT A
US Highway 24 Access Control Plan
Peterson Boulevard Milepost 311.75 to Elbert Highway Approximate Milepost 325.81
El Paso County, Colorado

I. PURPOSE

The purpose of the Access Control Plan (ACP) is to provide the City of Colorado Springs, El Paso County, (hereafter referred to collectively as the "City and County"), and the Colorado Department of Transportation (hereafter referred to as the "Department") with a comprehensive roadway access control plan for US Highway 24 (US HWY 24) from the junction of Peterson Boulevard milepost (MP) 311.75 to Elbert Highway MP 325.81 (hereafter referred to as the "Segment").

II. AUTHORITY

The development of this ACP was completed pursuant to the requirements of the Access Code, section 2.12, and adopted by the attached Agreement.

III. RESPONSIBILITIES

It is the responsibility of each party to this Agreement to ensure that vehicular access to this Segment shall only be in conformance where practicable with this Agreement. The cost of access improvements, closures and modifications shall be determined pursuant to section 43-2-147(6)(b) C.R.S., this Agreement, and this ACP. All access construction shall be consistent with the design criteria and specifications of the Code.

IV. ACCESS LOCATIONS

- A. The following is a listing of each existing and future access point along the Segment including their current status and agreed to changes and future conditions. All access locations are defined by the approximate mile-post (in hundredths of a mile) along US Highway 24 to the centerline of the access as further illustrated on Exhibit B.
- B. Any access described in Section IV, which requires changes or closure as part of this agreement or if significant public safety concerns develop, may be closed, relocated, or consolidated, or turning movements may be restricted, or the access may be brought into conformance with this ACP, when in the opinion of the City and County with Department concurrence, or in the opinion of the Department, and any of the following conditions occur: a) the access is determined to be detrimental to the public's health, safety and welfare, b) the access has developed an accident history that is correctable by restricting the access, or c) the access restrictions are necessitated by a change in the road or traffic conditions, or d) there is a change in the use of the property that would result in a change in the type of access operations or e) a highway reconstruction project provides the

opportunity to make highway and access improvements in support of this ACP. Access construction shall be consistent with the design and specifications of the Access Code.

- C. All highway design and construction where practicable will be based on the assumption that the roadway will have a cross section for a minimum of two through lanes in each direction, right turn auxiliary lanes, single and dual left turn lanes, a non-transverseable center median, and sufficient room to accommodate longitudinal installation of utilities.
- D. Access Point Descriptions:
- 1) Access 1, (Exhibit B, figure 1, MP 311.746): Peterson Boulevard, An existing public road interchange with US HWY 24. This access currently functions with Peterson Boulevard crossing beneath US HWY 24. Access ramps will be modified as necessary to give appropriate traffic operations along US HWY 24 in conjunction with US HWY 94 and Marksheffel Road.
 - 2) Access 2, (Exhibit B, figure 1, MP 311.88): Space Village Avenue, This access located on the southeast side of US HWY 24 functions as a right-in and right-out (RIRO) (no left turns allowed) to gain access to Space Village Avenue. This access will remain.
 - 3) Access 3, (Exhibit B, figure 1, MP 312.15): Existing RIRO access on northwest side of US HWY 24. Access will be closed.
 - 4) Access 4 (Exhibit B, figure 1, MP 312.14): Existing RIRO frontage road access on the southeast side of US HWY 24. Access will be closed. Access will be shared using a frontage road routed to Space Village Avenue.
 - 5) Access 5 (Exhibit B, figure 1, MP 312.19): Existing RIRO access on the southeast side of US HWY 24. Access will be closed. Access will be shared using a frontage road routed to Space Village Avenue.
 - 6) Access 6 (Exhibit B, figure 1, MP 312.483): SH 94, an existing state highway that intersects US HWY 24 on the southeast. This access currently is a signalized T-intersection. A future roadway may extend to the northwest, therefore the intersection may become a signalized full movement intersection. A future interchange may be developed at this location. If so, this access will be modified as necessary to give appropriate traffic operations along US HWY 24 in conjunction with Peterson Boulevard and Marksheffel Road.
 - 7) Access 7 (Exhibit B, figure 1, MP 312.76): Existing private road with full movement access on the southeast side of US HWY 24. Access will be closed.

- 8) Access 8 (Exhibit B, figure 1, MP 312.53): No existing access on northwest side of US HWY 24. Future access will be a RIRO. Access will be closed when access to the Hwy 94 intersection is established.
- 9) Access 9 (Exhibit B, figure 1, MP 312.87): Existing private road with full movement access on the southeast side of US HWY 24. Access will be moved to MP 312.81 and will be modified to a RIRO.
- 10) Access 10 (Exhibit B, figure 1, MP 313.178): Marksheffel Road, An existing public road that intersects US HWY 24. This access is currently being relocated to MP 313.23 (Access 11). Once relocated, access will be closed.
- 11) Access 11 (Exhibit B, figure 1, MP 313.23): Relocated Marksheffel Road, Future signalized public road with full movement access.
- 12) Access 12 (Exhibit B, figure 2, MP 313.93): Existing private road with full movement access on the southeast side of US HWY 24. Access will be modified to a RIRO. Access may be closed when the Constitution/Banning-Lewis Parkway interchange is constructed.
- 13) Access 13 (Exhibit B, figure 2, MP 313.92): Existing private road with RIRO access on northwest side of US HWY 24. Access may be closed when the Constitution/Banning-Lewis Parkway interchange is constructed.
- 14) Access 14 (Exhibit B, figure 2, MP 314.23): Existing private road with full movement access on the southeast side of US HWY 24. Access will be closed.
- 15) Access 15 (Exhibit B, figure 2, MP 314.28): Existing private road with previous full movement access on the northwest side of US HWY 24. Access has been closed and will remain closed.
- 16) Access 16 (Exhibit B, figure 2, MP 314.67): Constitution Avenue/Future Banning-Lewis Parkway, an existing public road that intersect US HWY 24 on the northwest side. This access currently is a signalized full movement T-intersection. It is planned to become an interchange connection with US HWY 24. This interchange is not currently designed but the goal of the interchange is to combine Constitution with the Banning-Lewis Parkway into one US HWY 24 crossing location. The intersection will be signalized until the interchange is built.
- 17) Access 17 (Exhibit B, figure 2, MP 314.98): Existing private road with full movement access on the northwest side of US HWY 24. Access will be closed.
- 18) Access 18 (Exhibit B, figure 2, MP 314.98): Existing private road with full movement access on the southeast side of US HWY 24. Access will be closed and relocated to MP 314.94 (Access 19).

- 19) Access 19 (Exhibit B, figure 2, MP 314.94): No access exists on southeast side of US HWY 24. Future access will be a RIRO. Access may be closed when the Constitution/Banning-Lewis Parkway interchange is constructed.
- 20) Access 20 (Exhibit B, figure 3, MP 316.42): Future North Carefree, Future signalized public road with full movement access.
- 21) Access 21 (Exhibit B, figure 3, MP 316.63): Existing private road with full movement access on the northwest side of US HWY 24. Access will be closed.
- 22) Access 22 (Exhibit B, figure 3, MP 316.63): Existing private road with full movement access on the southeast side of US HWY 24. Access will be closed.
- 23) Access 23 (Exhibit B, figure 4, MP 317.14): Existing private road with full movement access on the southeast side of US HWY 24. Access will be closed.
- 24) Access 24 (Exhibit B, figure 4, MP 317.43): Existing private road with full movement access on the northwest side of US HWY 24. Access will be closed.
- 25) Access 25 (Exhibit B, figure 4, MP 317.69): Future Barnes Road, Future signalized public road with full movement access.
- 26) Access 26 (Exhibit B, figure 5, MP 318.99): Future Stetson Hills/Garrett Road, Garrett Road, an existing public road that intersects US HWY 24 on the southeast. This access is currently a signalized full movement T-intersection. Future Stetson Hills will align with Garrett on the northwest side of US HWY 24. Access will remain signalized and full movement.
- 27) Access 27 (Exhibit B, figure 5, MP 319.14): No access exists on northwest side of US HWY 24. Future access will be a RIRO.
- 28) Access 28 (Exhibit B, figure 5, MP 319.12): Dodge Road, Existing public road on the southeast of US HWY 24. Access will be closed.
- 29) Access 29 (Exhibit B, figure 5, MP 319.15): Existing private road with full movement access on the southeast side of US HWY 24. Access will be closed. Access will be shared using a frontage road routed to Dodge Road and Falcon Highway.
- 30) Access 30 (Exhibit B, figure 5, MP 319.20): Existing private road with full movement access on the southeast side of US HWY 24. Access will be closed. Access will be shared using a frontage road routed to Dodge Road and Falcon Highway.

- 31) Access 31 (Exhibit B, figure 5, MP 319.23): Existing private road with full movement access on the southeast side of US HWY 24. Access will be closed. Access will be shared using a frontage road routed to Dodge Road and Falcon Highway.
- 32) Access 32 (Exhibit B, figure 5, MP 319.34): Existing private road with full movement access on the northwest side of US HWY 24. Access will be closed.
- 33) Access 33 (Exhibit B, figure 5, MP 319.42): Existing private road with full movement access on the southeast side of US HWY 24. Access will be closed. Access will be shared using a frontage road routed to Dodge Road and Falcon Highway.
- 35) Access 35 (Exhibit B, figure 5, MP 319.89): Existing private road with full movement double access on the southeast side of US HWY 24. The two accesses will be closed. Access will be shared using a frontage road routed to Dodge Road and Falcon Highway.
- 36) Access 36 (Exhibit B, figure 5, MP 320.04): Existing private road with full movement double access on the northwest side of US HWY 24. The two accesses will be closed and moved to a RIRO at MP 319.99, (Access 37). Access will also share a frontage road to future Dublin Boulevard. This access may be closed when new access is available to the west.
- 37) Access 37 (Exhibit B, figure 5, MP 319.99): No existing access on northwest side of US HWY 24. Future access will be a RIRO. This access may be closed when new access is available to the west.
- 38) Access 38 (Exhibit B, figure 5, MP 319.99): Existing private road with full movement access on the southeast side of US HWY 24. Access will be closed. Combined access will be shared using a frontage road routed to Dodge Road and Falcon Highway.
- 39) Access 39 (Exhibit B, figure 5, MP 320.08): Existing private road with full movement access on southeast side of US HWY 24. Access will be closed. Combined access will be shared using a frontage road routed to Dodge Road and Falcon Highway.
- 40) Access 40 (Exhibit B, figure 5, MP 320.292): Future Dublin/Falcon Highway, Existing Falcon Highway on the southeast and future Dublin on the northwest. Existing signalized public road with full movement. Existing Falcon Highway is planned to be relocated to MP 320.32. Once relocated, the current access will be closed and the signal will be relocated.
- 41) Access 41 (Exhibit B, figure 5/6, MP 320.34): Existing full movement access on the northwest side of US HWY 24. Access will be closed.

- 42) Access 42 (Exhibit B figure 6, MP 330.66): Future Realigned Meridian Road, Future signalized public road will have full movement access.
- 43) Access 43 (Exhibit B, figure 6, MP 320.74): Existing private road with full movement access on the southeast side of US HWY 24. Access will be modified to a RIRO.
- 44) Access 44 (Exhibit B, figure 6, MP 320.81): Meridian Road, Existing public road with full movement access on the southeast side of US HWY 24. Access will be modified to be a RIRO with mountable curbs for emergency vehicles when the future realigned Meridian is built at Access 42.
- 45) Access 45 (Exhibit B, figure 6, MP 320.81): Existing public road with full movement access on the northwest side of US HWY 24. Access will be modified to be a RIRO with mountable curbs for emergency vehicles when the future realigned Meridian is built at Access 42.
- 46) Access 46 (Exhibit B, figure 6, MP 320.87): Existing private road with full movement access on the southeast side of US HWY 24. Access will be modified to a RIRO.
- 47) Access 47 (Exhibit B, Figure 6, MP 320.93): School access, Full movement access closed in 1999. Access will remain closed.
- 48) Access 48 Exhibit B, figure 6, MP 321.04): Existing private road with full movement access on the southeast side of US HWY 24. Access will be modified to a RIRO.
- 49) Access 49 (Exhibit B, figure 6, MP 321.11): Existing private road with full movement access on the southeast side of US HWY 24. Access will be closed and shared with a RIRO at access 74.
- 50) Access 50 (Exhibit B, figure 6, MP 321.13): Existing private road with full movement access on the southeast side of US HWY 24. Access will be closed and shared with a RIRO at access 74.
- 51) Access 51 (Exhibit B, figure 6, MP 321.22): Woodmen Road, Existing signalized, full movement T-intersection access on the northwest side of US HWY 24. Full movement combined driveway access on southeast side of US HWY 24. The intersection will remain full movement and signalized.
- 52) Access 52 (Exhibit B, figure 6, MP 321.36): Rio Lane, Existing public road with full movement access on the southeast of US HWY 24. Access will be closed and directed to the Woodmen/ US HWY 24 intersection, when development occurs.

- 53) Access 53 (Exhibit B, figure 6/7, MP 321.58): Bluegill Road (south), Existing public road with full movement access on the southeast of US HWY 24. Access will be closed.
- 54) Access 54 (Exhibit B, figure 7, MP 321.77): Cottontail Road, Existing public road with full movement access on the southeast of US HWY 24. Access will be modified to a RIRO.
- 55) Access 55 (Exhibit B, figure 7, MP 322.50): Bluegill Road (north) /Judge Orr Road (west), Existing public road with full movement access on the southeast side of US HWY 24. The access will remain. Judge Orr Road east of US HWY 24 will be realigned to connect with Bluegill Road on the southeast side of US HWY 24. Judge Orr Road on the northwest side of US HWY 24 will be realigned to connect with existing Blue Gill intersection on the southeast side of US HWY 24. The current signal will be relocated to the new full movement intersection.
- 56) Access 56 (Exhibit B, figure 7, MP 322.543): Judge Orr Road (east), Existing public road with full movement access on the southeast of US HWY 24. Access will be closed, when Access 55 is built. Judge Orr Road will be re-routed to Blue Gill Road in a signalized full movement intersection.
- 57) Access 57 (Exhibit B, figure 7/8, MP 323.06): Existing private road with full movement access on the southeast side of US HWY 24. Access will be closed. Access will be shared using a frontage road routed to Judge Orr Road and future Stapleton Road.
- 58) Access 58 (Exhibit B, figure 8, MP 323.33): Existing private road with full movement access on the northwest side of US HWY 24. Access will be closed and routed to future Stapleton Road.
- 59) Access 59 (Exhibit B, figure 8, MP 323.41): Existing private road with full movement access on the southeast side of US HWY 24. Access will be closed. Access will be shared using a frontage road routed to Judge Orr Road and future Stapleton Road.
- 60) Access 60 (Exhibit B, figure 8, MP 323.47): Existing private road with full movement access on the northwest side of US HWY 24. Access will be closed and routed to future Stapleton Road.
- 61) Access 61 (Exhibit B, figure 8, MP 323.52): Existing full movement access on the southeast side of US HWY 24. Access will be closed. Access will be shared using a frontage road routed to Judge Orr Road and future Stapleton Road.
- 62) Access 62 (Exhibit B, figure 8, MP 323.68): Existing private road with full movement access on the southeast side of US HWY 24. Access will be closed.

- Access will be shared using a frontage road routed to Judge Orr Road and future Stapleton Road.
- 63) Access 63 (Exhibit B, figure 8, MP 323.74): Future Stapleton Road Extension, Future signalized public road with full movement access.
 - 64) Access 64 (Exhibit B, figure 8, MP 323.83): Existing private road with full movement access on the southeast side of US HWY 24. Access will be closed and routed to future Stapleton Road.
 - 65) Access 65 (Exhibit B, figure 8, MP 324.20): Existing private road with full movement access on the northwest side of US HWY 24. Access will be closed. Access will be shared using a frontage road to Stapleton Road.
 - 66) Access 66 (Exhibit B, figure 8, MP 324.16): Existing private road with full movement access on the southeast side of US HWY 24. Access will be closed.
 - 67) Access 67 (Exhibit B, figure 8, MP 324.28): Existing private road with full movement access on the southeast side of US HWY 24. Access will be closed.
 - 68) Access 68 (Exhibit B, figure 8/9, MP 324.39): Curtis Road, Existing road with full movement access on the southeast side of US HWY 24. Access will be modified to a RIRO.
 - 69) Access 69 (Exhibit B, figure 9, MP 324.72): Rex Road, Existing private road with full movement access on the southeast side of US HWY 24. Future public roadway extension on the northwest side of US HWY 24. Access will be changed to a full movement signalized intersection.
 - 70) Access 70 (Exhibit B, figure 9, MP 325.19): No existing access on southeast side of US HWY 24. Future access will be a RIRO.
 - 71) Access 71 (Exhibit B, figure 9, MP 325.46): Existing private road with full movement access on the southeast side of US HWY 24. Access will be closed.
 - 72) Access 72 (Exhibit B, figure 9, MP 325.53): Existing private road with full movement access on the southeast side of US HWY 24. Access will be closed.
 - 73) Access 73 (Exhibit B, figure 9, MP 325.81): Elbert Highway, Existing public road with full movement. Access may be signalized in the future, and will remain.
 - 74) Access 74 (Exhibit B, figure 6, MP 321.11): No access currently exists on southeast side of US HWY 24. Future access will be a RIRO shared by closed access 49 and closed access 50.

- 75) Access 75 (Exhibit B, figure 2, MP 313.51): Previous temporary construction access on northwest side of US HWY 24. Access is currently closed and will remain closed.

EXHIBIT B (Access Plan Illustrated)

The attached Exhibit B is for general illustration only and is considered conceptual. The aerial photographs used for this illustration were taken in 2003. Refer to the text of the document for more current information.

EXHIBIT C (Access Control Plan Amendment Process)


1. Any request for amendment of the Access Control Plan must be submitted to the Colorado Department of Transportation staff, and then distributed to the City of Colorado Springs, and El Paso County. The amendment request shall include:
 - Description of changes requested of the Access Control Plan
 - Justification for Amendment
 - Traffic Impact Study or analysis, depending upon the magnitude of the change requested. Any party to the Access Control Plan can request this supporting documentation.
2. The Department shall review the submittal for completeness and for consistency with the access objectives, principles, and strategies described in the US Hwy 24 Access Control Plan and the State Highway Access Code.
3. Once CDOT and the City and/or County, as applicable, approve the request for the amendment, the amendment and all accompanying documentation shall be submitted if necessary to Transportation Commission for final review and approval.

AFFIDAVIT OF ALEX KARAMI

STATE OF COLORADO)
) ss.
COUNTY OF DENVER)

I, Alex Karami, do state:

1. I am currently employed by the Colorado Department of Transportation (“CDOT”) as Access Program Administrator. My work address is Colorado Department of Transportation, Safety & Traffic Engineering, 4201 East Arkansas Ave. EP-770, Denver, CO 80222-3400.
2. I have personal knowledge of the items contained in this affidavit and am competent to testify thereto. As Access Program Administrator, I share responsibility with CDOT administrative staff in overseeing custody of Intergovernmental Agreements concerning access control.
3. CDOT’s records show that on July 25, 2005, Chief Engineer Craig Siracusa executed a copy of the accompanying Intergovernmental Agreement (IGA) that had already been executed by the City of Colorado Springs and El Paso County. The original document, as executed by CDOT, El Paso County, and the City of Colorado Springs, was lost or destroyed subsequent to July 26, 2005.
4. The attached IGA is a true and accurate copy of the IGA as originally executed by the City of Colorado Springs and El Paso County before the July 25, 2005 execution of Mr. Siracusa.



Alex Karami

Subscribed and sworn to before me in the County of Denver, State of Colorado,
this 6th day of June, 2006.



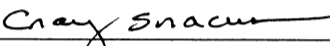
NOTARY PUBLIC

My Commission expires: 9/29/09

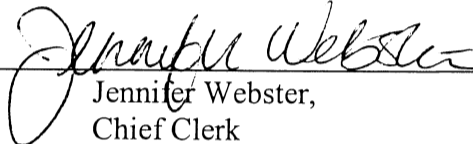
IN WITNESS WHEREOF, the original Intergovernmental Agreement document having been executed by all of the Agencies on July 25, 2005, and the original document with original signatures of all Agencies having been lost or destroyed pursuant to the attached affidavit of Alex Karami, this true and accurate copy of said document as executed by El Paso County and the City of Colorado Springs is executed by the Colorado Department of Transportation this 1 day of JUNE 2006 effective as of July 25, 2005.

State of Colorado
Department of Transportation

ATTEST:



Craig Siracusa,
Chief Engineer



Jennifer Webster,
Chief Clerk

CONCUR:

Robert D. Torres,
Regional Transportation Director