

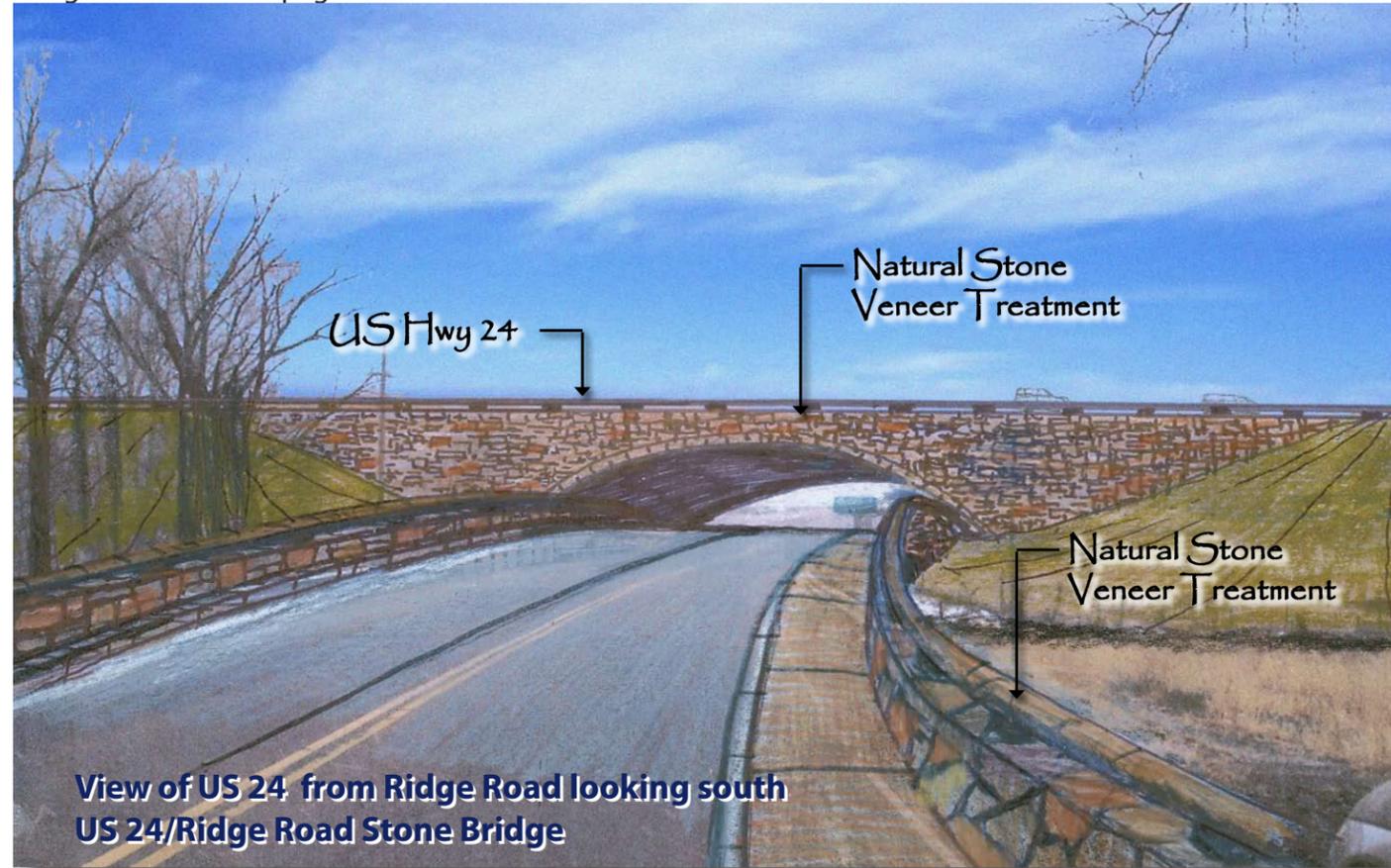
B. Bridges

Ridge Road

Only one US 24 bridge is proposed within this design segment. This bridge is over Ridge Road and should have a stone veneer, gentle side slopes and appear to be an arch. Please see the US 24/Ridge Road Stone Bridge sketch on this page.

Bridges Over Fountain Creek Typical Bridge Treatment (Ridge Road)

One Fountain Creek vehicular bridge is proposed within the Rural Design segment. Because of its association with the creek, much consideration was given to creating a bridge whose architecture relates with the



**View of US 24 from Ridge Road looking south
US 24/Ridge Road Stone Bridge**

This bridge should be designed to match the historic stone bridges in Manitou Springs and the Westside. The colors, lines, forms and textures should relate back to the proposed Midland Greenway and Fountain Creek.

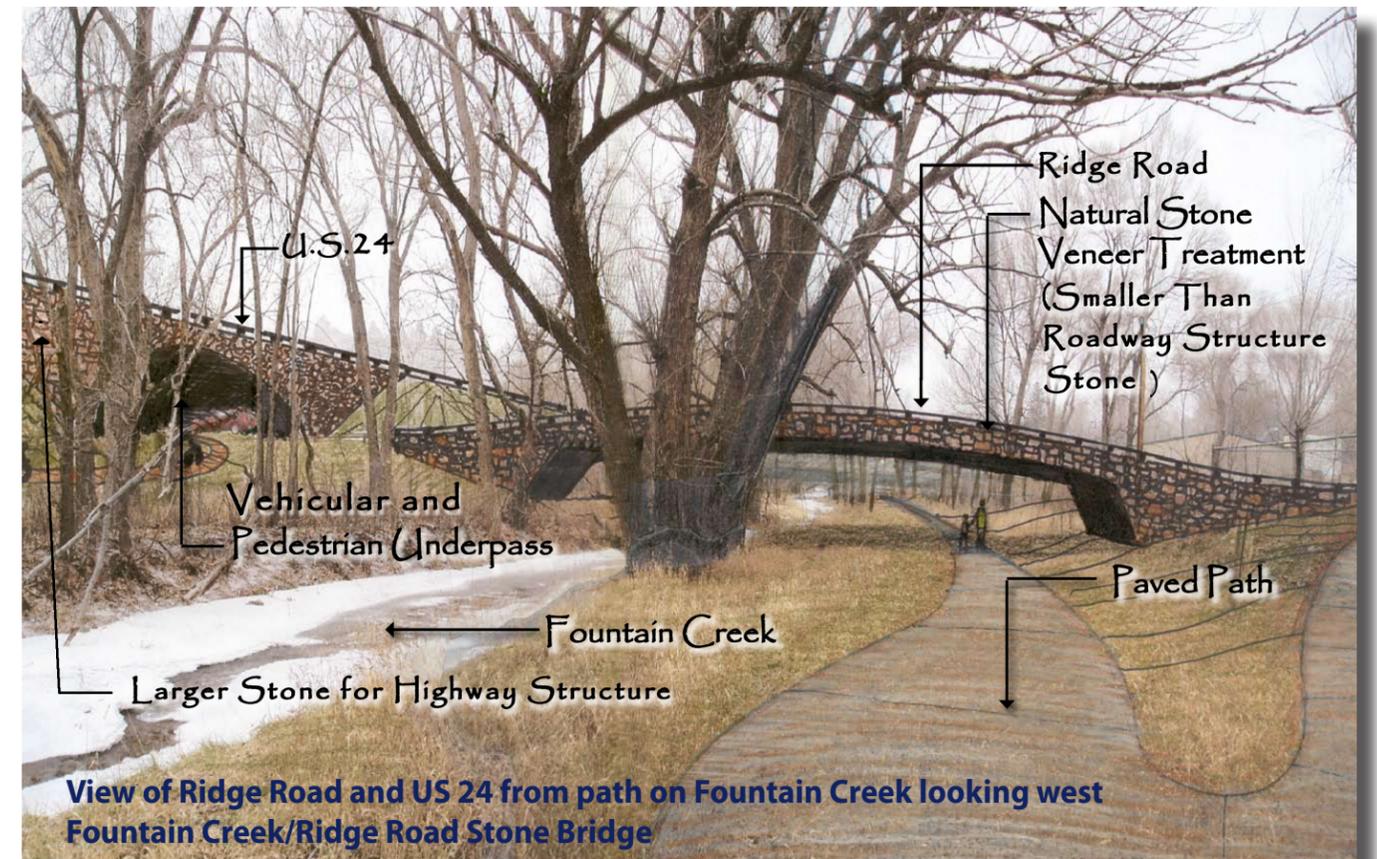
The use of the unifying natural accent rock is desired. The scale of the stone should be in keeping with where it is placed. For example, stone used as facing for US 24 should be larger than the stone proposed for the bridge that crosses Fountain Creek.

Bridge supports should also reflect the natural character of the area. Stone veneer for use on the abutments should incorporate the stone used for the bridge structure itself. Abutment lines should be straight and joints for the stone should be tight and deeply raked.

natural surroundings, whose size is in keeping with its surroundings and will not overpower the natural beauty of the creek corridor.

An arched concept was developed that pays tribute to the historic stone bridges of Manitou Springs and the Westside. The use of the unifying natural accent stone is desired.

Stone size should be small and more of a human scale. Since the bridge will be part of a park-like setting with pedestrian trails, the stone should exhibit an ashlar pattern as is proposed for both the signage into the Red Rock Canyon Open Space and the larger bridge structure over Ridge Road.



**View of Ridge Road and US 24 from path on Fountain Creek looking west
Fountain Creek/Ridge Road Stone Bridge**



Existing Bridge over Fountain Creek

C. Sound Walls

Sound walls in the Rural Design Segment should be designed so that they do not compete with the natural beauty of the Rural Segment.



Sound Wall in Rural Design Segment

The recommended material is precast or cast-in-place concrete panels that are earth tone in color. Wall ornamentation should be kept at a minimum. These walls should be simple and should step back into the landscape, rather than dominate it. Sound walls should be constructed using the unifying natural accent rock and matching the size and jointing of the US 24/Ridge Road Bridge. Please see the Sound Wall in Rural Design Segment sketch on this page for the desired appearance of these walls.

D. Landscape Elements

Proposed new landscaping within this design segment should consider the implications to the natural setting. Ideally, new plantings should enhance and contribute to the existing character of the corridor and not compete with it. The implementation of any new landscaping should aid in blending the highway with the adjoining Fountain Creek and Red Rock Canyon landscape.

Highway Right-of-Way

Native grasses will be used along all roadway shoulders. Where stabilization is required and in all areas where other aesthetic treatments have not been identified, xeric plantings from the Highway R.O.W. Xeric Plantings schedule should be installed to blend with the existing non-irrigated landscape. See the Appendix.

If plantings of trees and shrubs are desired, they should be installed in a naturalized unpatterned way. An informal planting scheme that may consist of a mixture of evergreen trees and shrubs should be used in these areas. Plantings should be in keeping with what was removed. If implemented accordingly, maintenance of these areas will be minimal.

Riparian/Drainage Areas

Landscape materials installed along US 24 adjacent to Fountain Creek should consist of native grasses that can take advantage of local runoff.

To preserve the natural appearance of this corridor, the areas where US 24 encroaches into Fountain Creek and the associated riparian areas should be revegetated with riparian and native plantings.

Plants for these areas should be selected from the Naturalized Plantings and Riparian Plantings schedules found in the Appendix. Please see the Riparian/Drainage Area Planting Concept on this page to understand where the different planting schedules apply.

As a best management practice, and in order to capture runoff before it enters Fountain Creek, swales will be constructed adjacent to the highway. These swales should be vegetated with grasses and herbaceous plants selected from the Riparian Planting schedule found in the Appendix.



E. Irrigation

Irrigation has been identified for installation only at the proposed gateway into Red Rock Canyon Open Space. All other landscaped areas will rely on natural rainfall and natural drainage patterns. As identified on the plant schedules located in the Appendix of this document, plants and vegetation have been carefully selected to withstand periods of drought and to rely solely on natural rainfall for their source of water.

F. Color and Materials

Color and materials for the Rural Design Segment will focus on the surrounding area. Newly constructed elements are intended to bleed into the natural surroundings. No accent colors are to be used. The natural red rock outcrops of the Garden of the Gods and Red Rocks Canyon Open Space are more predominant in this segment. Stone should be the dominant finish for all elements in this design segment. Stone will help to integrate the roadway with the surrounding area.



Riparian/Drainage Area Planting Concept

Stone should be installed as a rough ashlar appearance, with wider joints between stones and deep raked joints. Grout between stones should be black or very dark grey. Stone should match the rock of adjacent land forms utilizing the same red rock from the area. It is the intent of these design guidelines to use the same unifying natural accent rock for both the Urban and Rural Design Segments. The unifying accent rock should be Navajo Ruby Rock. Cultured Stone can also be used.



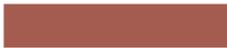
Natural Rock

Cultured Stone should be a rough cut ashlar rock pattern with a deep red color similar to natural rock above. Deep raked, grouted joints are desired.



Random Ashlar Pattern

1. Color Palette

<u>Reds/Browns</u>	
	Federal Color-30070
	Federal Color-30075
	Federal Color-30049

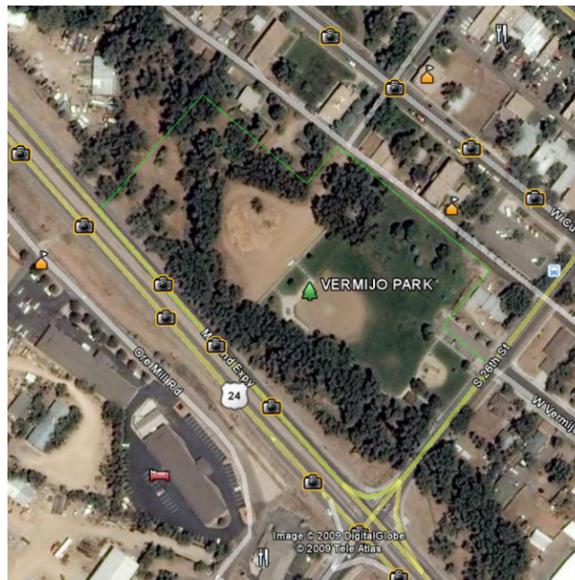
SECTION FOUR: Unique Areas and Elements

The unique areas being affected by US 24 West Corridor improvements are Vermijo Park, the rock face adjacent to Red Rock Canyon Open Space, Midland Greenway (See Appendix. This plan was produced as part of a separate greenway planning effort) and the trail and walls adjacent to Safeway just west of 31st Street. Unique elements include trails, site furnishings and lighting.

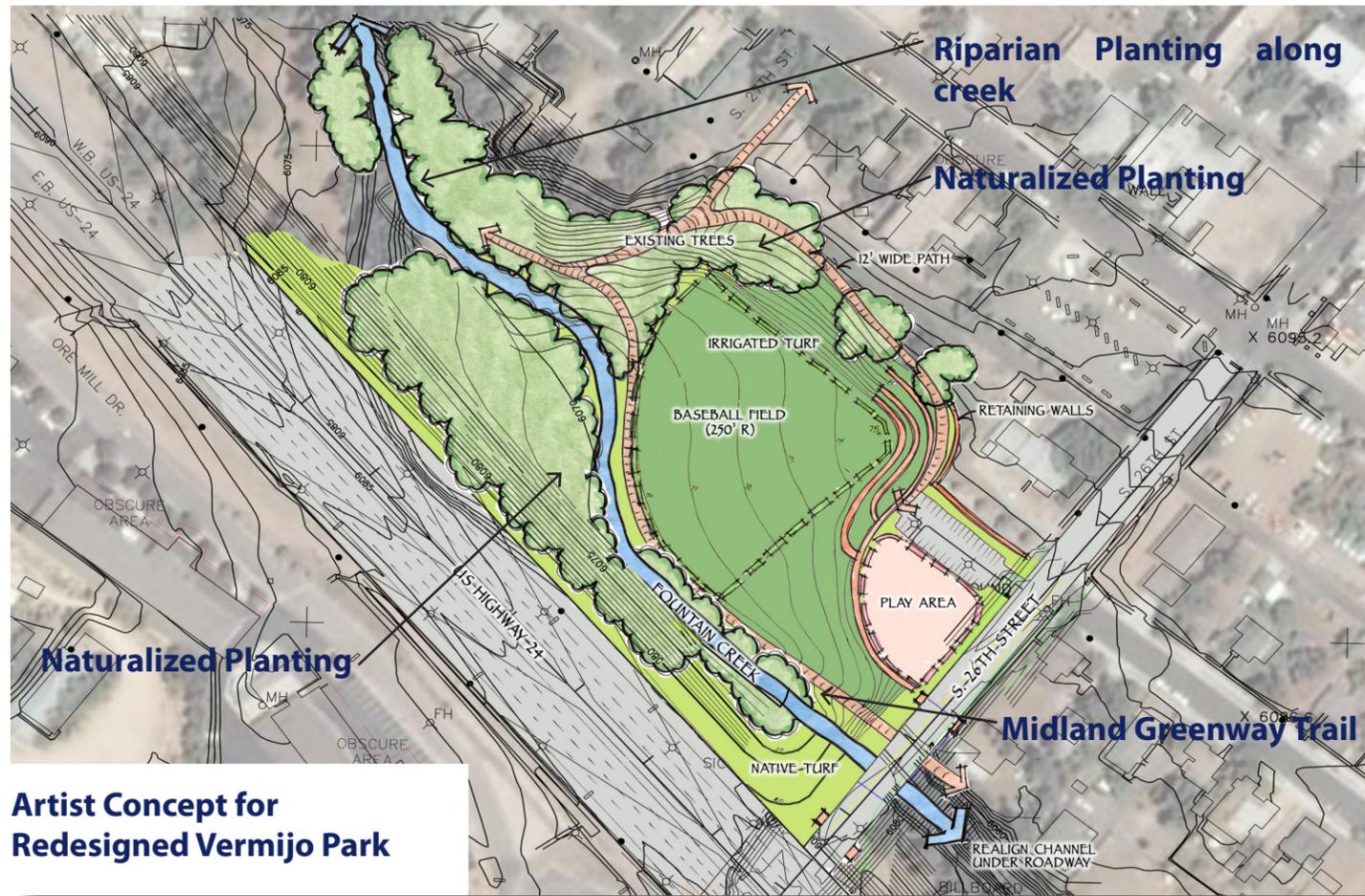
Unique Areas

1. Vermijo Park

Presently Vermijo Park is situated northwest of US 24 at its intersection with 26th Street. The park is lower in elevation than US 24. Fountain Creek runs between the southern edge of the park and the highway and provides a natural buffer between vehicular traffic and the park. The park contains a tot lot area, baseball field, multi-use field and parking. Vehicular access to the park is off of South 26th Street.



The park currently sits adjacent to US 24 Right-of-Way. Due to the proposed highway improvements, an additional travel lane is being added for both east and west bound traffic, it has become necessary to move Fountain Creek farther north into the park. As a result, the



**Artist Concept for
Redesigned Vermijo Park**

park and creek will be redesigned, resulting in a change from their present locations.

One concept for the proposed park involves shifting the ball field, parking, tot lot and creek channel to the north. The creek channel will be realigned to pass beneath South 26th Street. In an effort to accommodate the proposed infrastructure changes, some of the existing vegetation associated with the creek riparian areas will be removed. Grading of the new ball field will necessitate the addition of retaining walls around the backstop. The proposed retaining walls will allow for the addition of a paved 12' wide trail to meander around the ball field, eventually paralleling Fountain Creek to the south. These retaining walls should be constructed to match the stone bridges across Fountain Creek. See Page 19. Additional concepts for the park will be generated with input from neighbors and park and recreation staff.

Landscaping will focus on restoring the disturbed areas within the park. To bring it back to its natural native appearance, riparian plantings should be used along Fountain Creek. Supplemental deciduous and ornamental trees and deciduous and evergreen shrubs

should be provided to help unify the new park within its residential surroundings.

These plants should be selected from the Naturalized Plantings schedule in the Appendix. Native seeding of the graded and disturbed areas is essential to control erosion.

2. Red Rock Open Space Rock Face Alternatives

As a result of US 24 being expanded from four lanes to six lanes within the Rural Design Segment, the existing rock face, west of 31st Street and south of the highway, will be impacted. The rock in this location will have to be cut back in order to accommodate additional travel lanes.



Several aesthetic treatment alternatives for the rock cut have been proposed. Further design and engineering of this area however, will be necessary as no geotechnical information was available at the time these alternatives were prepared. A thorough review of the geology of this hogback will provide valuable information on the design alternatives that are possible at this location.

The first alternative includes rock sculpting to remove rock, but done in a way that the face looks natural when complete. This will require rock staining. This will only work if the rock has the structural integrity to stand on its own. Additional evergreen trees will be installed where practical and the existing fence will remain.

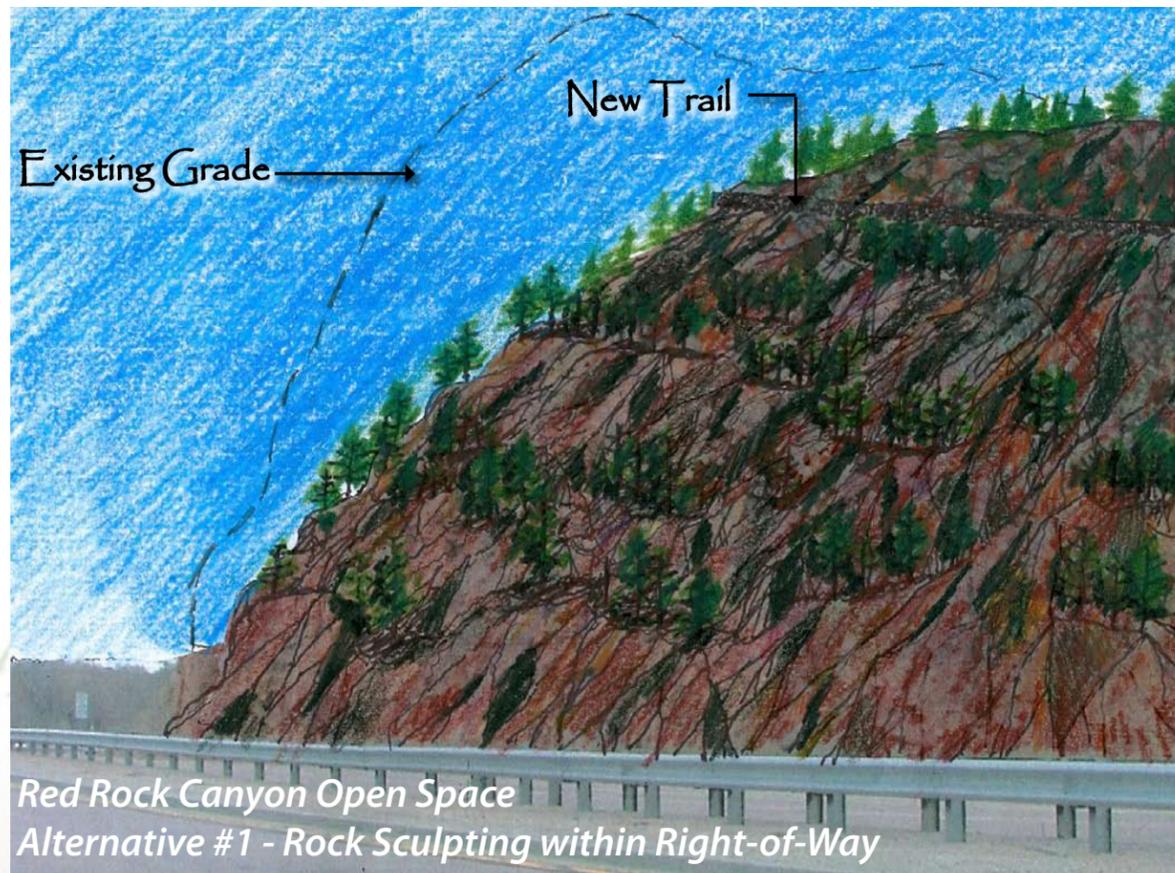
The second alternative proposes an angular pre-cast/cast-in-place stained concrete retaining wall. This alternative resembles the lines and colors from the Garden of the Gods Park. By providing an angular design, the rock face is in keeping with the natural setting. The angular form is augmented with planting beds to allow for greenery, as well as to control run-off from the face of the wall. The bands of color should be a natural stone palette and should be large enough so that the detail is not lost to neighboring homes and businesses.

See page 21 for the color palette.

The third alternative provides construction of a modular block wall with texture variations. In this concept, different block face textures would be arranged to create visual interest. Color variations in the blocks could

also be used to create an accent pattern in the wall. See page 21 for the color palette.

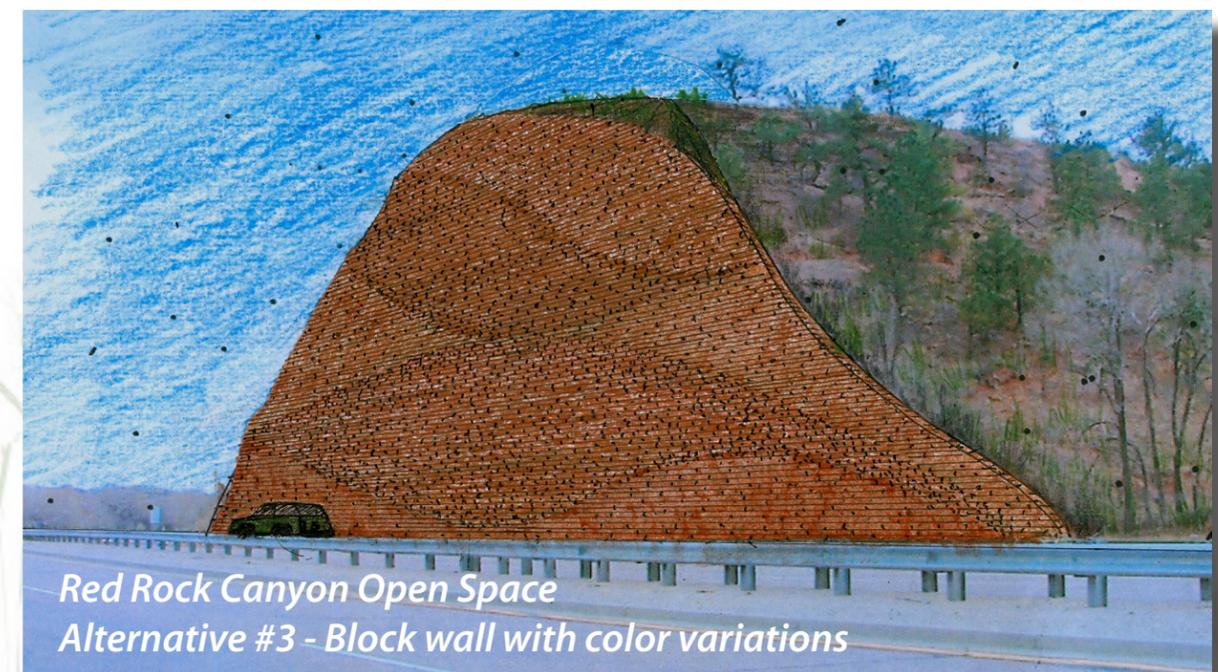
Alternatives #2 and #3 would be required if the rock face, after being cut back, did not exhibit the structural integrity needed to stand on its own.



Red Rock Canyon Open Space
Alternative #1 - Rock Sculpting within Right-of-Way



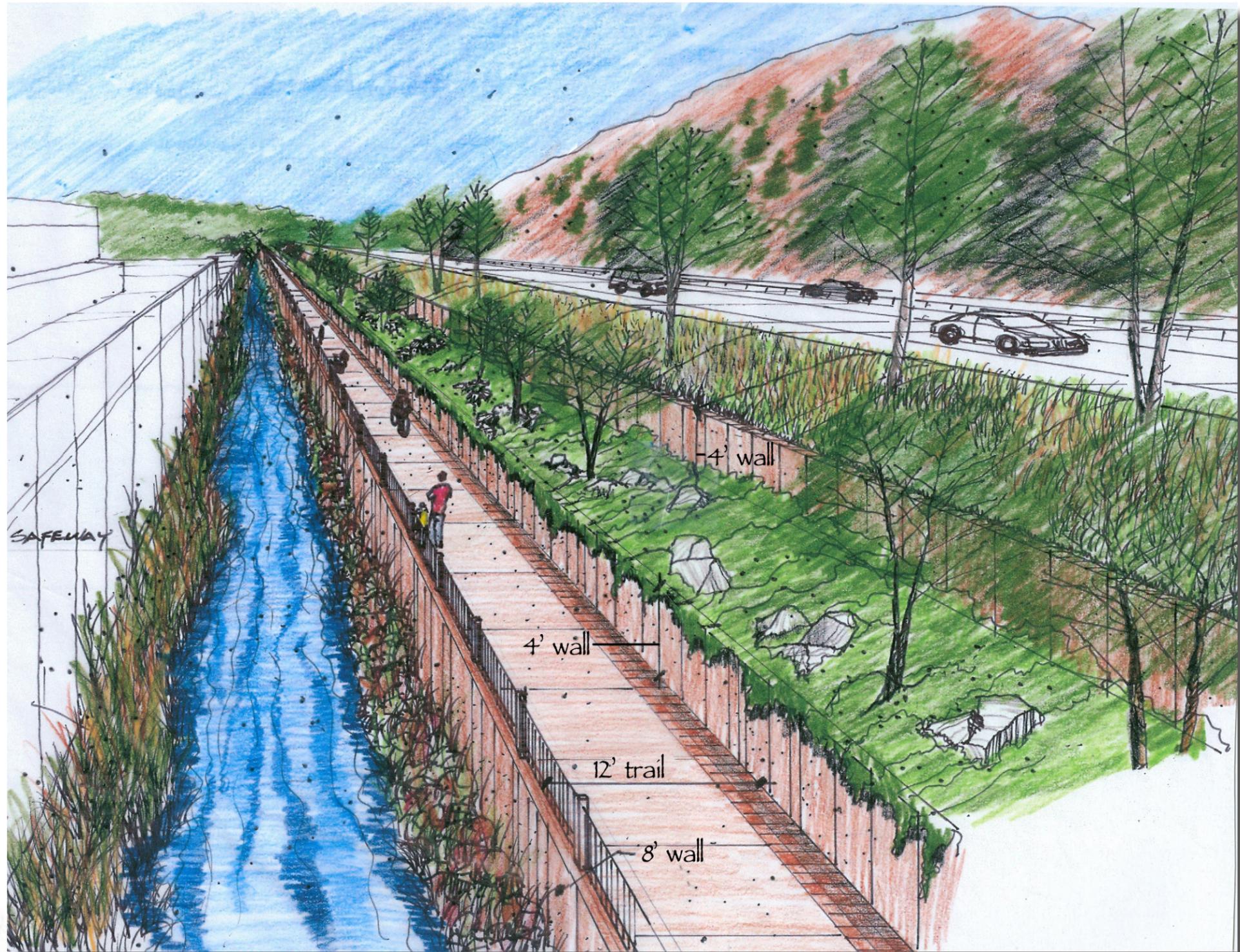
Red Rock Canyon Open Space
Alternative #2 - Pre-cast/cast-in-place concrete wall



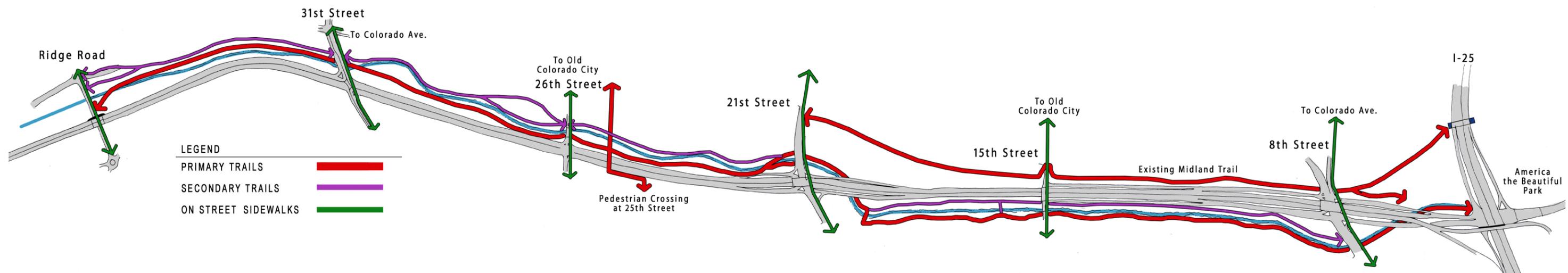
Red Rock Canyon Open Space
Alternative #3 - Block wall with color variations

3. Trail at Safeway

A segment of the Midland Greenway is proposed on the north side of US 24 near the Safeway location west of 31st Street. There is a pinch-point, or narrow section, involving the existing Safeway structure and the rock outcropping on the south side of US 24 adjacent to Red Rock Canyon Open Space. The proposed roadway section needs to fit between these two features. The solution involves cutting into the rock outcropping to the south of the highway and installing retaining walls to hold up the proposed north side of the roadway and allow a trail to be installed. The following perspective sketch shows the aesthetic concept for this pinch-point.



Greenway Trail at Safeway



Unique Elements

1. Trail and Sidewalk Concepts

A hierarchy of trails is envisioned for the park lands adjacent to US 24. The overall goal for this trail system is to link the west side of Colorado Springs with Downtown and America the Beautiful Park. A portion of this trail exists between 8th and 21st Streets. The goal is to connect these existing sections and complete a network of trails that will become the Midland Greenway and complete the regional trail system in the area.

In the Urban Design Segment, trails will be installed on both the north and south sides of US 24. A proposed pedestrian bridge at 25th Street will facilitate a north-south grade separated crossing of US 24. Trails within the Rural Design Segment generally follow Fountain Creek. Sidewalk connections are also part of the planned trail system within both the Urban and Rural Design Segments.

Trail Classifications

Two classifications of trails are envisioned within the Urban and Rural Design Segments. These should be referred to as primary and secondary trails. Their width will vary depending upon their classification. Paved trails are intended to serve pedestrians, cyclists and roller bladers, while unpaved trails are intended to serve hikers and equestrian users.

Primary Trails

Primary trails should be a minimum of 12' in width and should meander to convey a park-like trail. Primary

trails should be concrete with integral earth tone colors to harmonize with the natural environment, as well as the built improvements. The main trail concrete color should be Federal Color #FS23630.



Federal Color #FS23630

Secondary Trails

To accommodate walkers, joggers and equestrian users wherever possible, primary trails should also include an adjacent secondary soft surface trail. This concept of separate hard and soft surface trails adjacent to one another is designed to eliminate conflicts between the different types of trail users. Soft surface trails should be a minimum of four feet wide. Materials such as crusher fines or breeze are the preferred soft surface treatment. These materials compact well and prevent weed growth. The material and color of this type of path should reflect the natural rock colors of the area (reds and browns).



Breeze Color Options

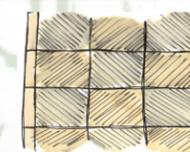


Federal Color #FS30475

Sidewalk areas should provide opportunities for site furniture (i.e., benches, trash receptacles, lights, etc.), as well as, areas for interpretive signs and art displays.

Connections to Old Colorado City

In addition to the pedestrian bridge at 25th Street, several connections to Old Colorado City and Colorado Avenue are planned. These are primarily attached pedestrian-friendly or "living sidewalks" at 8th, 15th, 21st, 26th, and 31st Streets, and Ridge Road. These attached walks should be designed to be at least ten feet wide. To create visual interest and relate to the pedestrian scale, sidewalks should be integrally colored concrete (earth tones) and incorporate score and finishing patterns. Sidewalks that connect Midland Greenway to adjacent neighborhoods should also include integral concrete color. A lighter tone is desired to help to lessen the grey appearance of natural concrete. Federal Color #FS30475 is desired.



Sidewalk Scoring Option

2. Site Amenities

Trail amenities such as benches, signage, lighting and trash receptacles should be included along the length of the trail system. They should be located at key points along the corridor where trails intersect with sidewalks and other trail segments.



Benches



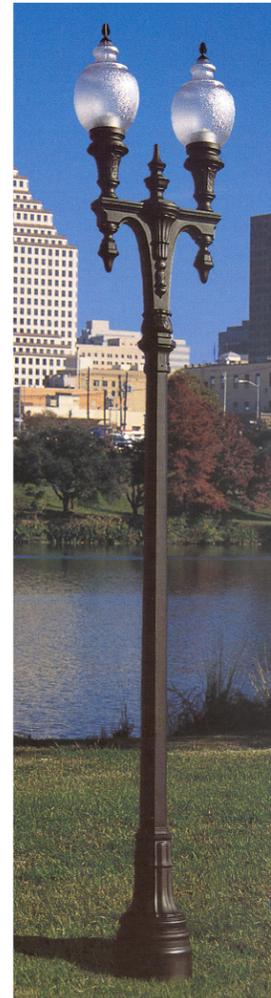
Trash/Recycling Receptacles

Benches may also be located where trail users can enjoy scenic views and can be randomly located along the trail corridor as places for recreationalists to relax, read and take a break from their recreational pursuits.

Lighting fixtures should be placed where security and safety are factors as well as to provide a sense of comfort. Underpasses, trail connections and trail intersections are all examples of potential lighting locations. The use of fixtures in these locations is envisioned to be more aesthetic and should not be confused with the types of lighting normally associated with large parking lots or highway right-of-way areas.

While it is important to preserve and protect the nighttime environment by preventing light pollution, it may not be possible with the use of certain pedestrian scale light fixtures. Light pollution should be avoided where possible with the use of low wattage bulbs and LED fixtures. Light standards should still have a Victorian flavor and blend with the existing community character that is representative of Old Colorado City and Manitou Springs.

As a family of trail amenities, the benches, trash receptacles and lighting should all have a common theme or design to them. There should be continuity between the site furnishings and how they appear. The style should reflect a Victorian/historical theme that is in keeping with the heritage of Old Colorado City and Manitou Springs. It should be carried throughout the entire length of the corridor through both the Urban and Rural Design Segments.



Lighting

3. Roadway Lighting

Roadway lighting should be provided at the on-ramps, off-ramps and interchanges. Lighting should consist of approved FHWA light standards that should be neutral in color (i.e., black and/or grey). So as to not impede or compete with the views and natural scenery within each of the design segments, light pollution should be avoided and, therefore, careful consideration should be given to the number and placement of light poles. Lighting should only be provided for safety, security and comfort. Excessive use of lights impedes night sky access, produces excessive glare, reduces visibility, wastes valuable energy resources and interferes with the natural environment for wildlife.

The actual design and placement of these fixtures will be determined during final design by FHWA standards.