



US 36 CORRIDOR
Environmental Impact Statement

US 36 Public Meetings

April 2009

Project No. NH 0361-070(14133)



US 36 CORRIDOR
Environmental Impact Statement

Meeting Agenda

Project No. NH 0361-070(14133)

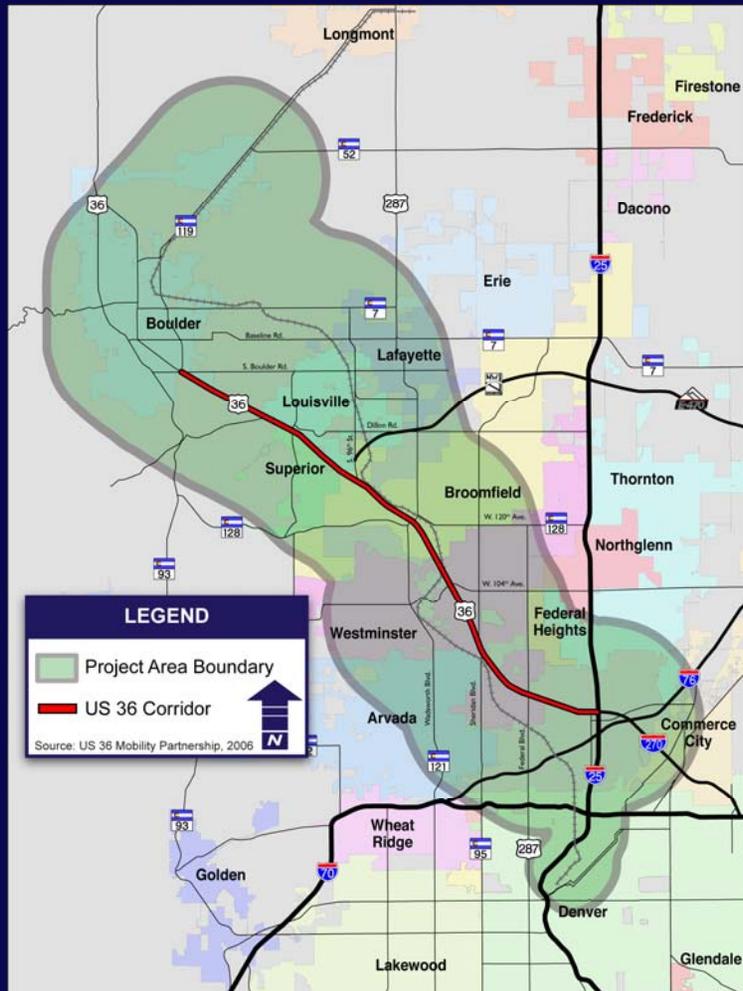
- **Open House 6:00 – 6:30 pm**
- **Presentation 6:30 – 7:00 pm**
- **Facilitated Discussion 7:00 – 7:45 pm**
- **Open House 7:45 – 8:45 pm**



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US 36 Project Area

Project No. NH 0361-070(14133)



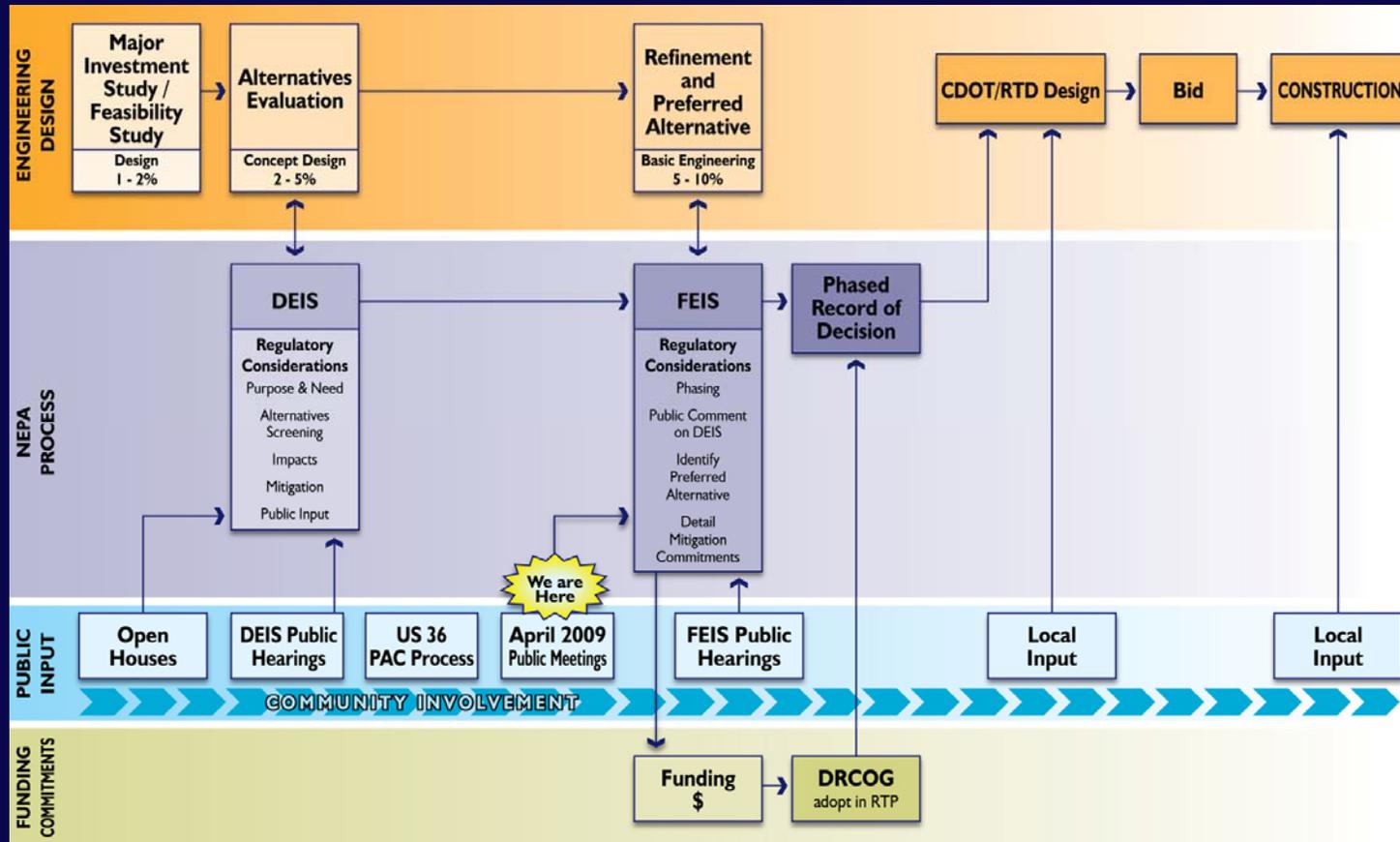
US 36 Project Area

- 18 mile corridor
- Denver to Boulder
- CDOT and RTD lead agencies
- Draft EIS released in August 2007
- Final EIS in progress



US 36 Project Timeline

Project No. NH 0361-070(14133)





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Purpose and Need

Project No. NH 0361-070(14133)

The purpose of the US 36 project is to improve mobility along the US 36 corridor from I-25 in Adams County to Foothills Parkway/Table Mesa Drive in Boulder, and among intermediate destinations





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Purpose and Need

The project needs are:

- **Increase trip capacity**
- **Expand access**
- **Congestion relief**
- **Expand mode of travel options**
- **Efficient transit service**
- **Update outdated highway facilities**





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DEIS Alternatives

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DEIS Alternatives

- **Package 1 – No Action**
- **Package 2 – Managed Lanes/Bus Rapid Transit and Bikeway**
- **Package 4 – General-Purpose Lanes, High-Occupancy Vehicle, Bus Rapid Transit, and Bikeway**



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Draft EIS Comments

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- **Minimize community and environmental impacts**
- **Minimize project costs**
- **Increase mobility improvements**



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Preferred Alternative Committee Process

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- **Twenty-one member group of agencies and jurisdictions**
- **Purpose was to identify a preferred alternative for the US 36 Final EIS**
- **Issues addressed**
 - **Access to Broadway**
 - **Access to Managed Lanes**
 - **Auxiliary Lanes**
 - **Bikeway**
 - **Bus Rapid Transit Operations and Stations**
 - **Travel Demand Management**
 - **West End Design Options**
- **Recommended Combined Alternative Package July 2008**



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Summary of Alternatives

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FEIS Alternatives

- **Package 1**
- **Package 2**
- **Package 4**
- **Combined Alternative Package –
Managed Lanes, Auxiliary Lanes, Bus
Rapid Transit, and Bikeway**



Summary of Alternatives

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Package 1: No Action

- **FasTracks improvements**
 - Northwest Rail
 - Bus park-n-Ride and slip ramp improvements:
 - Relocate Church Ranch p-n-R, new slip ramps
 - Relocate Broomfield p-n-R, new slip ramps, pedestrian bridge
 - New pedestrian bridge at Table Mesa
- **120th Avenue Connection**
- **80th Avenue Bridge**
- **Potential queue jumps at McCaslin, Flatirons, Church Ranch, and Sheridan**



Summary of Alternatives

Project No. NH 0361-070(14133)

Package 2

- **Add two managed lanes in each direction**
 - **Barrier separated with five access points**
- **Median BRT stations**
- **Improvements to all interchanges**
- **Travel Demand Management**
- **Bikeway from Westminster to Boulder**
- **Cost = \$2.4 B (2008 Dollars)**



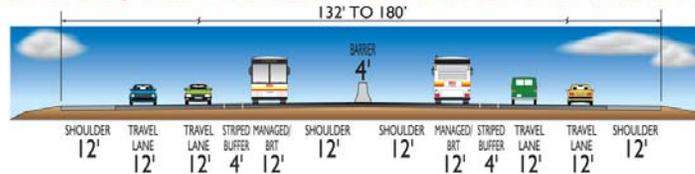
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Summary of Alternatives

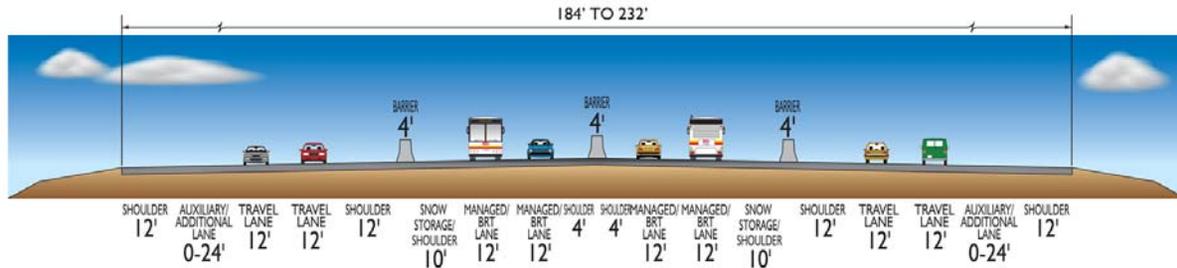
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Package 2

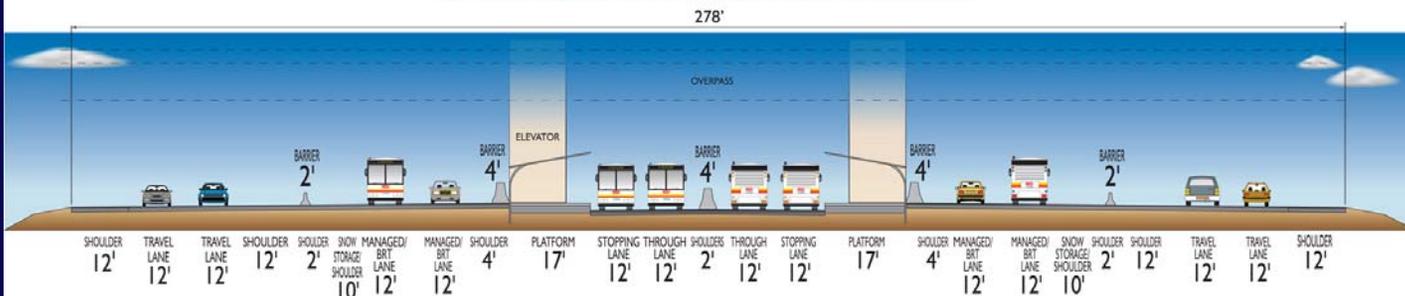
MANAGED / BRT - CHERRYVALE ROAD to S. 88th STREET



MANAGED / BRT - S. 88th STREET to PECOS STREET



MANAGED / BRT with a BRT STATION





Summary of Alternatives

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Package 4

- **One additional general-purpose lane and one additional BRT/HOV lane in each direction**
 - **Buffer separated BRT/HOV lane with seven access points**
- **Median BRT stations**
- **Improvements to all interchanges**
- **Travel Demand Management**
- **Bikeway from Westminster to Boulder**
- **Cost = \$2.1 B (2008 Dollars)**



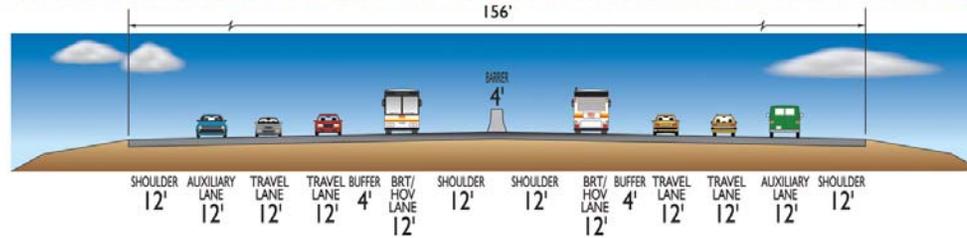
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Summary of Alternatives

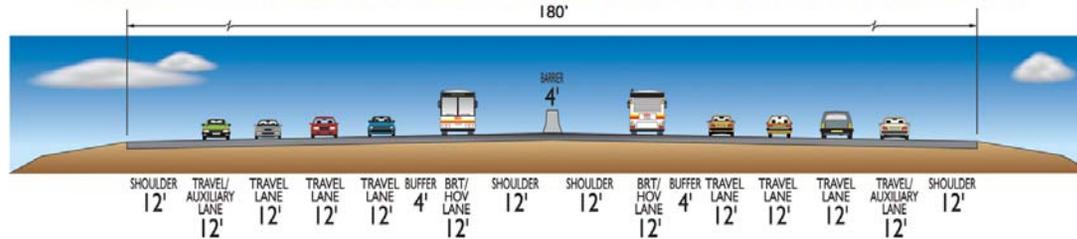
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Package 4

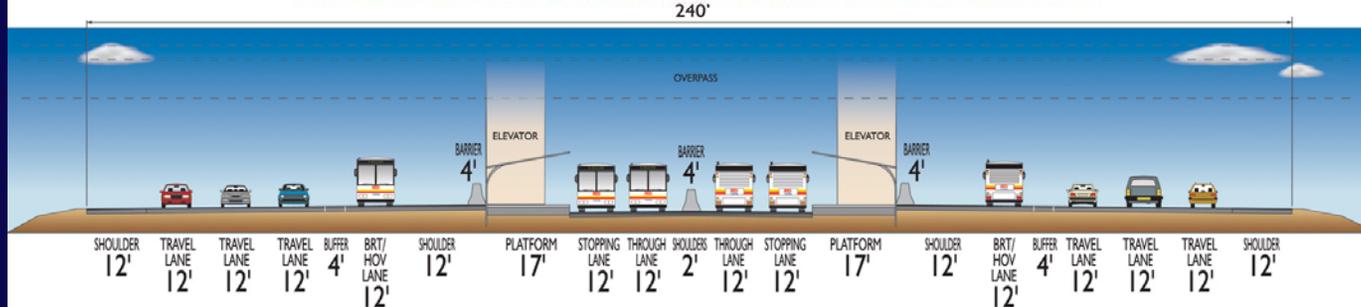
GENERAL PURPOSE LANES, HOV and BRT - FOOTHILLS PARKWAY to EAST FLATIRON CIRCLE



GENERAL PURPOSE LANES, HOV and BRT - EAST FLATIRON CIRCLE to I-25



GENERAL PURPOSE LANES, HOV and BRT with a BRT STATION





Summary of Alternatives

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Combined Alternative Package

- **Add one managed lane in each direction**
 - Buffer separated lane with access points between each interchange
- **Auxiliary lanes between most interchanges**
- **Ramp and side loading BRT stations**
- **Improvements to all interchanges**
- **Travel Demand Management**
- **Bikeway from Westminster to Boulder**
- **Cost = \$1.9 B (2008 Dollars)**

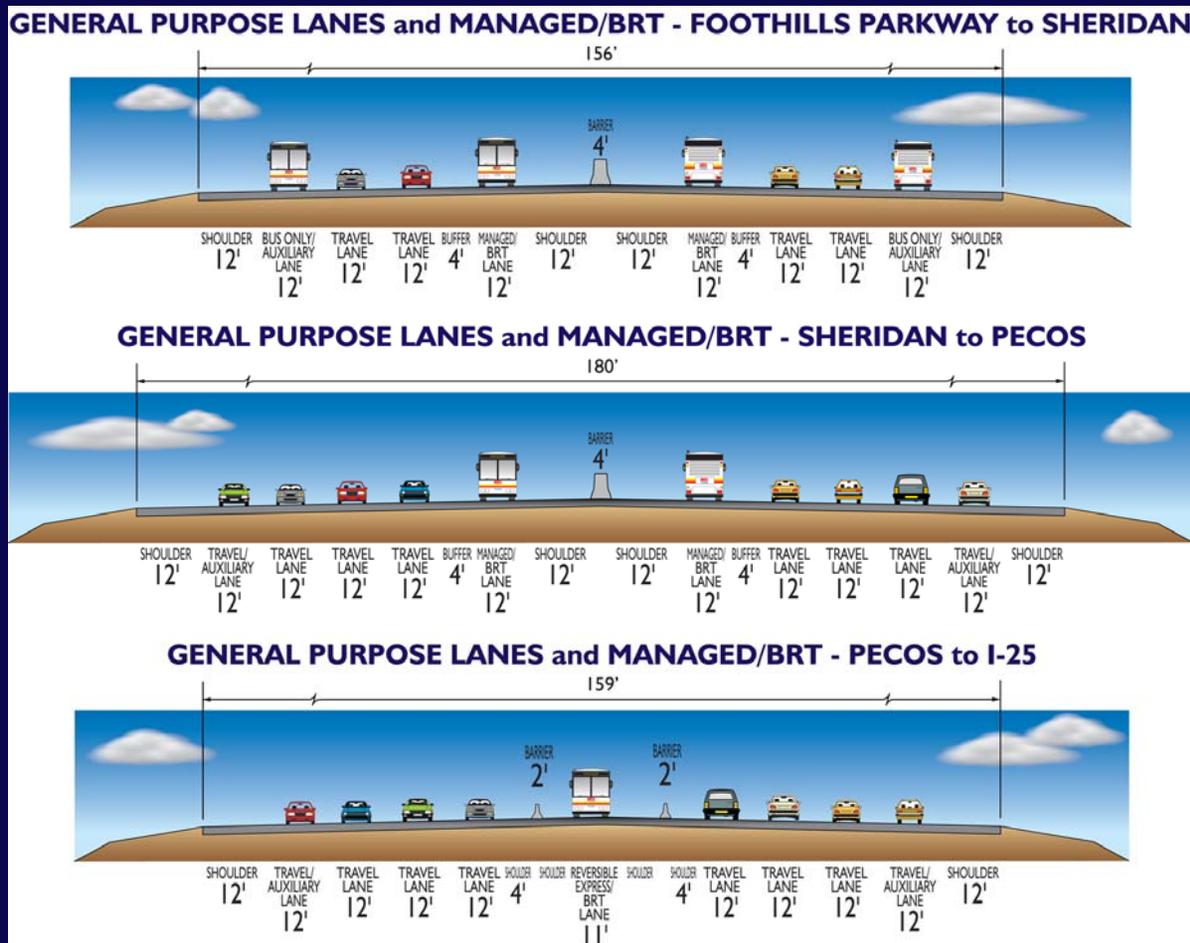


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Summary of Alternatives

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Combined Alternative Package





Combined Alternative

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- **A smaller overall footprint than P2 and P4**
- **Footprint is different due to:**
 - **Median/ramp stations**
 - **Buffer vs. barrier separation**
 - **Keep reversible lane on east end**
 - **Ramp metering**
 - **HOV bypass lanes**
 - **Improved bike path radius at connections**



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Combined Alternative Impacts

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- **ROW impacts reduced approximately 70%**
- **Similar noise impacts and mitigation**
- **Fewer parks and open spaces impacted**
- **Amount of wetland and wildlife impacts reduced**



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- **Due to funding limitations, Record of Decision will be phased**
 - Phase 1 will include funding identified in the Regional Transportation Plan (through 2035)
 - Two other phases to be implemented later
- **Priority is on managed lane between Pecos and Boulder**



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Schedule

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PHASE	2007	2008	2009												FUTURE
			JAN	FEB	MAR	APR	MAY	JUN	JUL	AUG	SEP	OCT	NOV	DEC	
Prepare DEIS	■														
Public Review		■													
Combined Alternative Development		■	■	■	■	■	■	■	■	■	■	■	■	■	■
Prepare FEIS			■	■	■	■	■	■	■	■	■	■	■	■	■
Public Review						■					■	■			
Phase I Record of Decision												■	■	■	■
Identify Funding			■	■	■	■	■	■	■	■	■	■	■	■	■
Final Design & Construction															■



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Access to Broadway

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- **Background**
- **Challenges**
- **Options under consideration**



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Public Meeting Comment

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- **What questions or concerns do you have regarding the Combined Alternative?**
- **What ways do you suggest to improve the project and/or minimize impacts?**
- **Other thoughts?**



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